



TOWN OF BRUNSWICK

PLANNING BOARD

85 UNION STREET, BRUNSWICK, ME 04011

**PLANNING BOARD
REVISED AGENDA
BRUNSWICK TOWN HALL
85 UNION STREET
COUNCIL CHAMBERS
TUESDAY, AUGUST 2, 2016; 6:00 P.M.**

1. **Case #16-035 –Cumberland Farms:** The Planning Board will review and take action on a **Sketch Plan** Major Development Review application submitted by authorized representative Sandra L. Guay for a proposed Cumberland Farms retail store with fuel service and associated canopy at 190 Bath Road in the **Cooks Corner Center (CC) Zoning District; Assessors Map CC1, Lot 28.**
2. **Workshop: Draft Zoning Ordinance Review – Chapters 4 and 5**
3. **946 Mere Point Road:** At the request of Town Council, the Planning Board will continue their review and offer recommendation regarding possible public use of the property.
4. **Other Business**
5. **Approval of Minutes**
6. **Next Meeting 9/13/16**

REVISED 7.28.16 TO CHANGE START TIME AND ADD ITEM #3

This agenda is mailed to owners of property within 200 feet of proposed development sites. Please call the Brunswick Department of Planning and Development (725-6660) with questions or comments. Individuals needing auxiliary aids for effective communications please call 725-6659 or TDD 725-5521.

This meeting will be televised.



TOWN OF BRUNSWICK, MAINE

INCORPORATED 1739

DEPARTMENT OF PLANNING AND DEVELOPMENT
85 UNION STREET
BRUNSWICK, ME 04011

ANNA BREINICH, FAICP
DIRECTOR OF PLANNING & DEVELOPMENT

PHONE: 207-725-6660
FAX: 207-725-6663

MEMORANDUM

TO: The Planning Board
FROM: Jared Woolston, Planner
DATE: July 29, 2016
RE: Sketch Plan Review of Cumberland Farms Redevelopment (Case #16-035)

PROJECT SUMMARY

Staff has reviewed the Sketch Plan application and determined that it is complete.

The proposed Sketch Plan Major Development Review application was submitted by authorized representative, Sandra Guay for LHB Enterprises, Inc., (Cumberland Farms) to redevelop an existing parcel with a new Cumberland Farms retail store with gasoline sales at 190 Bath Road (Map CC1, Lot 28). The project involves demolishing an existing 6,883 square foot building that contains a Cumberland Farms Convenience Store, AT&T Retail shop, and Papa John's pizza shop; and constructing a new 4,786 square foot Cumberland Farms Convenience Store building. The existing gasoline pipes, fueling locations, and underground fuel tanks will be removed from the site, and replaced with new underground tanks and pumps. The proposed pumps are proposed in a new configuration on-site with five (5) fuel dispensers that contain two (2) fueling locations per dispenser. The proposed fueling stations will be covered by a new 21-foot wide by 131-foot long canopy structure. The proposed parking lot will be reconfigured to contain 19 parking spaces for patrons, and two (2) additional parking spaces for employees for a total of 21 parking spaces. The proposed project is subject to the requirements of the CC1 Zoning District, including the *Cooks Corner Design Standards* that were adopted by the Brunswick Town Council on May 20, 2002.

The BBPAC reviewed the proposed Sketch Plan at their meeting on July 21, 2016. During the meeting, the applicant requested consideration for the advising the Planning Board to waive the sidewalk requirement within the Cooks Corner Design Standards. The BBPAC indicated that if no other sidewalks were within the project vicinity it would be practical to advise in favor of

waiving sidewalks. Staff advised that the BBPAC would be allowed to consider the project at the time of Final Plan review.

The Staff Review Committee (SRC) reviewed the Sketch Plan application on July 27, 2016. During the SRC meeting, the applicant indicated that the proposed Cumberland Farms Convenience Store would be a standalone facility that no longer contains the existing Papa John's Pizza shop, and AT&T Retail shop. The applicant described the plans that were submitted within the application for the SRC.

During the SRC meeting, the Director of Planning and Development asked the applicant to consider reconfiguring the proposed building, and fueling stations by moving the proposed fuel pumps to the rear of the parcel, and the building towards the front of the parcel. A representative for the applicant, Chris Tymula indicated that the proposed orientation was considered the most attractive for patrons. The applicant's representatives indicated that a 30-foot wide sewer easement at the rear of the parcel renders the area within the sewer easement unbuildable as indicated by the Brunswick Sewer District. The Town Engineer expressed concerns for access to the site from the Bath Road entrance and recommended that a third party engineer review traffic circulation unless the entrance to Bath Road was deleted or the proposed fueling stations were reoriented to avoid congestion within Bath Road should vehicles on-site block the Bath Road entrance as they wait for an unoccupied fuel pump. The traffic engineer for the applicant, Heather Monticup, P.E. responded that the two (2) proposed entrance driveways (Bath Road and Thomas Point Road) are required for delivery trucks entering and exiting the site; and the proposed layout was determined to be acceptable for traffic circulation. Director of Planning and Development, Anna Breinich advised that the Planning Board may consider conditioning a third party review of traffic circulation to satisfy the concerns of the Town Engineer. The Town Engineer noted that the storm drain capacity may be limited at the location where stormwater is proposed to be directed. The applicant indicated that the proposed reduction in impervious area was found to reduce the volume of runoff. The Town Engineer advised that stormwater may need to be redirected to ensure adequate capacity within the public stormwater system at peak flows. The Co-Chair of the Bicycle and Pedestrian Advisory Committee (BBPAC), Will Wilkoff attended the SRC meeting to address the requirement for sidewalks within the Cooks Corner Design Standards. The Co-Chair indicated that due to a lack of information about the project site, the BBPAC was remiss in advising the applicant that sidewalks would likely be waived by the Planning Board at the time of Final Plan review. Mr. Wilkoff indicated that recent findings that sidewalks exist within the project vicinity, and that a nearby business agreed to contribute funds to the Town of Brunswick for sidewalks would likely yield an opinion from the BBPAC that sidewalks should be required by the Planning Board in accordance with the Cooks Corner Design Standards. The applicant indicated that a portion of the frontage on Route 24 is not owned by the applicant and if sidewalks were required by the Planning Board an easement may be required. The SRC requested consideration for providing a sidewalk from the property boundary at Thomas Point Road to the boundary at Route 24 along the abutting properties to the east (Prompto 10 Minute Oil Change, and McDonalds) rather than the frontage to public streets to avoid possible safety issues, and maintain the proposed landscaped areas. The Town Planner advised the applicant to consider providing pedestrian interconnectivity with neighboring businesses (Prompto 10 Minute Oil Change, and McDonalds) with a pedestrian access easement to neighboring lots if allowed by the abutting businesses. The Planner further advised that the

proposed landscaping plan may be enhanced with some strategically placed native deciduous trees to provide tree canopy within an area of town that is primarily unvegetated pavement. The applicant's representatives indicated that consideration would be made for providing large trees within the landscaping plan but any new tree would need to avoid blocking a vehicle operator's line of site to oncoming traffic. The CEO advised that any proposed plantings should be placed in areas that avoid power lines at mature heights since future maintenance would require the removal of hazardous limbs.

The applicant provided a list of waiver requests that would be considered at the time of Final Plan Review as follows: *Corner Lot Treatment, Corner Buildings, Entrance (Corner Lot), Side Lot Parking, Relationship of Building to Thomas Point Road, Relationship of Building to Bath Road.* Reference is made to the Cooks Corner Design Standards for the above described waivers.

A Sketch Plan of the proposed development was prepared by MHF Design Consultants, Inc. entitled, "Site Plan" dated July 11, 2016.

DRAFT MOTIONS
SKETCH PLAN MAJOR REVIEW – CUMBERLAND FARMS
Case # 16-035

Motion 1. That the Board deems the Sketch Plan to be complete.

Motion 2. That the Board approves the Sketch Plan.

WOODMAN EDMANDS DANYLIK AUSTIN
SMITH & JACQUES, P.A.

ATTORNEYS-AT-LAW

ROBERT B. WOODMAN
THOMAS DANYLIK
RALPH W. AUSTIN
JAMES B. SMITH
KEITH R. JACQUES
MICHAEL J. O'TOOLE
HARRY B. CENTER II
SANDRA L. GUAY
AMY McNALLY

234 MAIN STREET
P.O. BOX 468
BIDDEFORD, ME 04005-0468
TELEPHONE: 207-284-4581
FAX: 207-284-2078
E-MAIL: SLG@woodedlaw.com

PETER L. EDMANDS
(Retired)

July 11, 2016

Ms. Anna Breinich
Director of Planning and Development
Town of Brunswick
85 Union Street
Brunswick, ME 04011

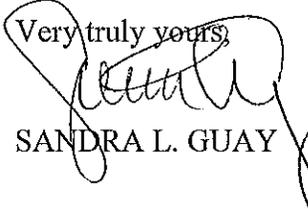
Re: Sketch Plan Application
Cumberland Farms / 190 Bath Road

Dear Ms. Breinich:

Enclosed please find one copy of a Sketch Plan Application submitted by LHB Enterprises, Inc., for the redevelopment of the Cumberland Farms located at 190 Bath Road, Brunswick, Maine (Town of Brunswick as Map CC1 Lot 28). Also enclosed is our check for what we understand to be the requisite filing fee of \$363.58, which includes an additional \$20.00 for the mailing of the abutter's notices and \$200.00 public hearing fee.

I understand that the application will be reviewed for completeness within 5 days of submission, and once deemed complete we will submit 9 copies for staff review.

Kindly let me know when this application will be scheduled for Staff review, and also please confirm that it will appear on the July 21st Bike/Pedestrian Advisory Committee Meeting agenda.

Very truly yours,

SANDRA L. GUAY

SLG/lb
Enclosures
Copy to:

Kathleen Sousa, Cumberland Farms, Inc.
Christopher Tymula, MHF Design Consultants, Inc.

TOWN OF BRUNSWICK PLANNING BOARD

SKETCH PLAN APPLICATION

APPLICANT: LHB Enterprises, Inc.

AGENT: Sandra L. Guay, Esquire
Woodman Edmands Danylik Austin Smith & Jacques, P.A.
234 Main Street, P.O. Box 468
Biddeford, Maine 04005

ENGINEERS: MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, NH 03079

**Sketch Plan Application of
LHB Enterprises, Inc.**

**Cumberland Farms Project
190 Bath Road**

TABLE OF CONTENTS

1. Sketch Plan Application
2. Authorization
3. Right Title and Interest
4. Location Maps
5. Abutter's List
6. Project Description
7. Trip-Generation and Collision Summary Letter
8. Waiver Requests
9. Plans (submitted separately)

1

**MAJOR DEVELOPMENT REVIEW
SKETCH PLAN APPLICATION**

1. Project Name: Cumberland Farms Project

2. Project Applicant
 Name: LHB Enterprises, Inc.
 Address: 3 Battery Wharf, #3411
Boston, MA 02109
 Phone Number: _____

3. Authorized Representative
 Name: Sandra L. Guay, Esq.
 Address: 234 Main Street
Biddeford, ME 04005
 Phone Number: (207) 284-4581

3. List of Design Consultants. Indicate the registration number, address and phone number Of any engineer, surveyor, architect, landscape architect or planner used:

1. MHF Design Consultants, Inc., 44 Stiles Road, Ste One, Salem, NH 03079 (603)893-0720
2. _____
3. _____

5. Physical location of property being affected: 190 Bath Road

6. Lot Size: 50,960 sq. ft.

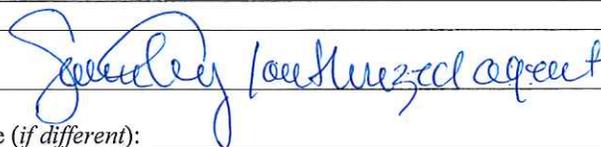
7. Zoning District: CC

8. Indicate the interest of the applicant in the property and abutting property. For example, is the applicant the owner of the property and abutting property? If not, who owns the property subject to this application? Applicant is the owner of the property

9. Assessor's Tax Map CC1 Lot Number 28 of subject property.

10. Brief description of proposed use: Redevelopment of the Cumberland Farms retail store with fuel service.

11. Describe specific physical improvements to be done: Please see attached

Owner Signature: 

Applicant Signature (if different): _____

Required Attachments (by Applicant):

- Sketch Plan Check List
- Sketch Plan Requirements for Open Space Developments (if applicable)
- Request for Waivers (if applicable)
- Required Copies of Sketch Plan

Required Attachment (by Planning and Development Department):

- Listing of all owners of property within 200-foot radius of property under review.

SKETCH PLAN REQUIREMENTS

Key: "O"= omit; "S"=submit; "NA"=not applicable; "W" = waiver; "P"=pending

Item	O	S	NA	W	P	Comments
Indicate Variances Granted			X			
Indicate Special Permits			X			
Indicate Special Exceptions			X			
Date, north point, scale		X				
Land area, existing use of the property, location of proposed development, locations reserved for future development		X				
Tentative rights-of-way locations, lot lines, lot numbers, lot areas		X				
Estimated soil boundary locations from the Soil Conservation Service Medium Intensity Soil Survey noting areas of severe and very severe soil limitations			X			
Existing natural, topographical, and cultural features including areas of steep slopes, bedrock outcrops, ponds, streams, aquifers, and other water bodies, wetlands, groundwater recharge areas, slumps, flood hazard areas, trees, and other vegetation, excavation sites, stone walls, net site area, historic and archeological sites, structures, or districts, and any other pertinent features.			X			
Tentative locations of proposed structures, owners of existing structures, and neighboring land uses		X				
Special conservation and recreation areas			X			
Location map		X				
Zoning information, including the zoning district(s) in which the property is located and the location of any overlay zones depicted on the plan.		X				
Any conditions imposed by previous development on the site.			X			
Other information Planning Board/Staff Review Committee deems necessary to conduct an informed review.		X				Traffic Report
Letter of consent signed by property owner authorizing the development review application in cases where applicant is not the owner of the property.		X				
Application Fee		X				
For Open Space Developments, sketch plan design review requirements indicated in Section 308.1			X			
Open Space Development: Request for Bonus Density			X			

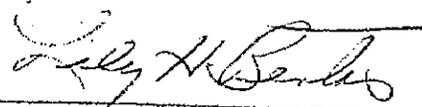
2

AUTHORIZATION

I, Lily H. Bentas, on behalf of myself and my company LHB Enterprises, Inc., hereby authorize our attorneys, Woodman Edmands Danylik Austin Smith & Jacques, P.A., Cumberland Farms, Inc., and our engineers, MHF Design Consultants, Inc., to sign any and all State of Maine or Town of Brunswick permit and appeal applications on our behalf with regard to the proposed Cumberland Farms development located at 190 Bath Road, Brunswick, Maine. I further authorize any of the attorneys or engineers in the above referenced firms to appear on our behalf and to represent the company before any department, board, committee or agency of said State or Town, including, but not limited to, the Maine Department of Environmental Protection, Planning Board and the Zoning Board of Appeals.

DATED: July 6, 2016

LHB Enterprises, Inc.



By: Lily H. Bentas
Its: President

3

021569

DEEDB95.2

QUITCLAIM DEED

Lily H. Bentas, of 431 Lewis Wharf, Boston, MA 02110,

for consideration paid of One Dollar (\$1.00) and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged,

grants to L. H. B. Enterprises, Inc., a Florida corporation with offices at 431 Lewis Wharf, Boston, MA 02110,

the real property located at Route 1 and 24, Brunswick, Cumberland County, Maine, more particularly described in Exhibit A attached hereto and made a part hereof (the "Property") together with any and all servitudes, easements, rights-of-way, licenses and other rights in real property appurtenant thereto, and all improvements located thereon.

Said Property is conveyed subject to any liens for taxes not yet due and payable, provisions of any ordinance, municipal regulation and public or private law, all matters of the public record and any state of facts which an accurate survey of the premises would disclose.

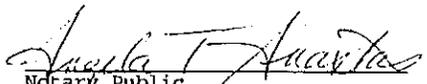
IN WITNESS WHEREOF, Grantor has executed this deed this
29th day of April, 1997.

By: 
Lily H. Bentas,

THE COMMONWEALTH OF MASSACHUSETTS

Norfolk SS. April 29, 1997

Then personally appeared the above Lily H. Bentas, to me known and known by me to be the person executing the foregoing instrument, and she acknowledged said instrument by her executed to be her free act and deed, before me.


Notary Public

My Commission Expires: 3/17/00

SEAL

EXHIBIT A

Certain lots or parcels of land, together with buildings and improvements thereon, situated on the southerly side of Old Route #1 (also being known as the Bath Road), in the Town of Brunswick, County of Cumberland, and State of Maine, bounded and described as follows:

Parcel I. Beginning at an iron pipe situated on the Westerly side of Thomas Point Road, which iron pipe marks the most Northerly corner of land now or formerly owned by one Evrard; thence proceeding South 49° 21' 0" West along the Northwesterly side of said Evrard Land, a distance of 150.02 feet to an iron pin at the most Westerly corner of said Evrard land, said iron pin also marking the Northeasterly corner of land of McDonald's Corporation, as conveyed to said McDonald's Corporation by Bo-Ed Inc. by deed dated April 30, 1969 and recorded in the Cumberland County Registry of Deeds in Book 3083, Page 284; thence proceeding North 84° 57' 34" West along the Northerly side of said McDonald's Corporation land a distance of 115 feet to an iron pipe; thence proceeding North 87° 46' 39" West along the Northerly side of said McDonald's Corporation land a distance of 54.82 feet to an iron pipe situated at the Southeasterly corner of a 20 foot right-of-way; thence proceeding North 2° 53' 11" West along the Easterly side of said right-of-way, a distance of 192.07 feet to an iron pipe situated on the Southerly side of Old Route #1, so-called also being known as the Bath Road; thence proceeding North 89° 19' 26" East along the Southerly side of Old Route #1, a distance of 149.57 feet to a granite monument; thence proceeding South 80° 53' 37" East along the Southerly side of Old Route #1, a distance of 29.43 feet to an iron pipe situated on the westerly side of said Thomas Point Road; thence proceeding South 47° 49' 53" East along the Westerly side of said Thomas Point Road, a distance of 154.07 feet to an iron pipe in the point of beginning.

Parcel II. Beginning at an iron pipe situated on the Westerly side of Thomas Point Road, which iron pipe marks the most Northerly corner of land now or formerly owned by one Evrard; thence proceeding South 49° 21' 0" West along the Northwesterly side of said Evrard land, a distance of 150.02 feet to an iron pin at the most Westerly corner of said Evrard land, said iron pin also marking the Northeasterly corner of land of McDonald's Corporation, as conveyed to said McDonald's Corporation by Bo-Ed Inc. by deed dated April 30, 1969 and recorded in the Cumberland County Registry of Deeds in Book 3083, Page 284; thence proceeding North 84° 57' 34" West along the Northerly side of said McDonald's Corporation land a distance of 115 feet to an iron pipe; thence proceeding North 87° 46' 39" West along the Northerly side of said McDonald's Corporation land a distance of 54.82 feet to an iron pipe situated at the Southeasterly corner of a twenty foot right-of-way and the point of beginning; thence proceeding North 2° 53' 11" West 192.07 feet to an iron pipe situated on the Southerly side of Old Route #1, so-called, also being known as the Bath Road; thence proceeding South 89° 19' 26" West along the Southerly side of Old Route #1, a distance of 20.01 feet to an iron pipe; thence South 2° 53' 11" East a distance of 191.05 feet to an iron pipe; thence proceeding South 87° 46' 39" East 20.08 feet to the point of beginning.

Meaning and intending to describe the same premises described in the deed recorded in Book 4183, Page 22.

RECEIVED
RECORDED REGISTRY OF DEEDS

97 MAY -2 PH 12: 59

CUMBERLAND COUNTY

John B. Curran

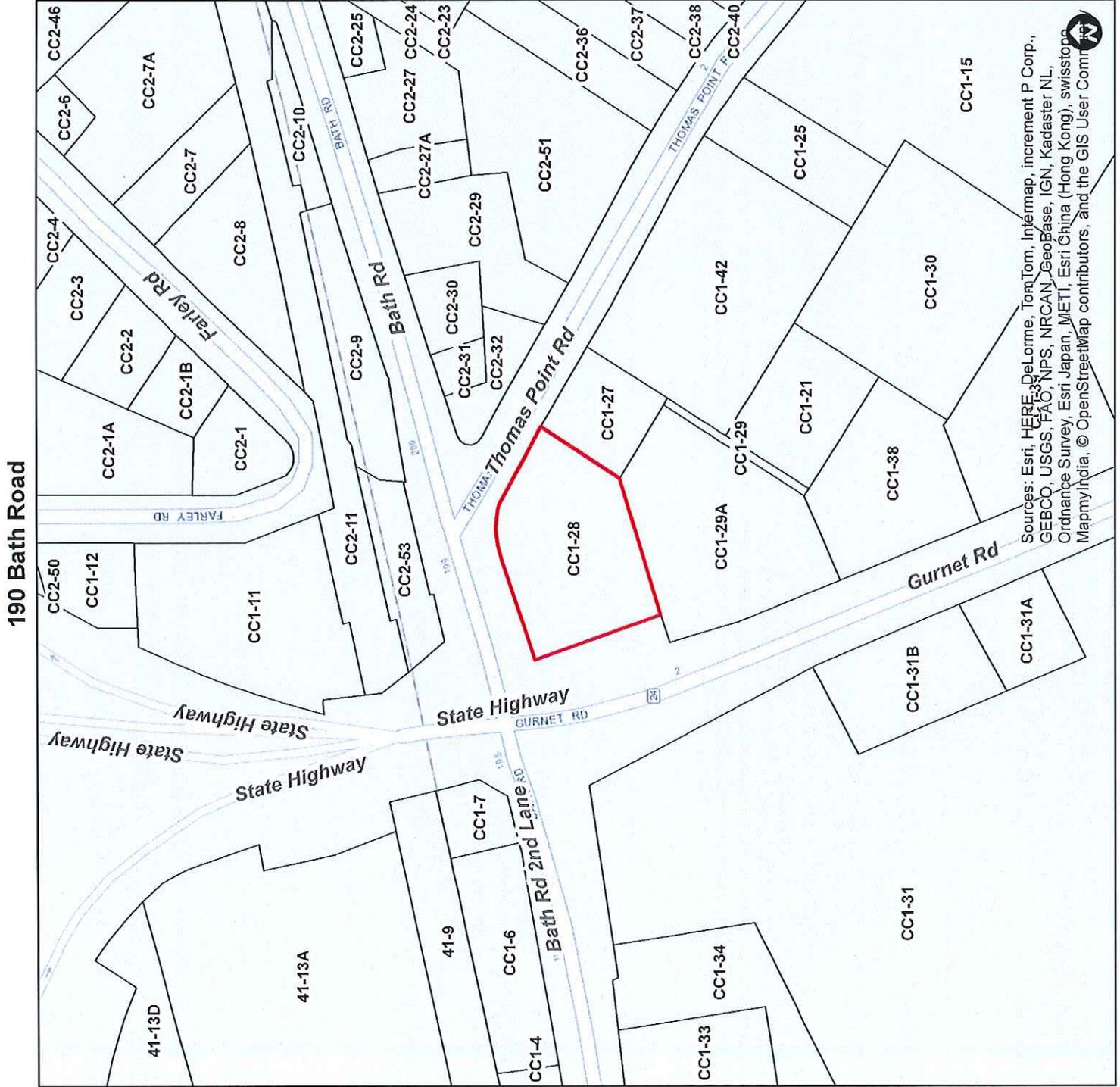
4

Brunswick Maine



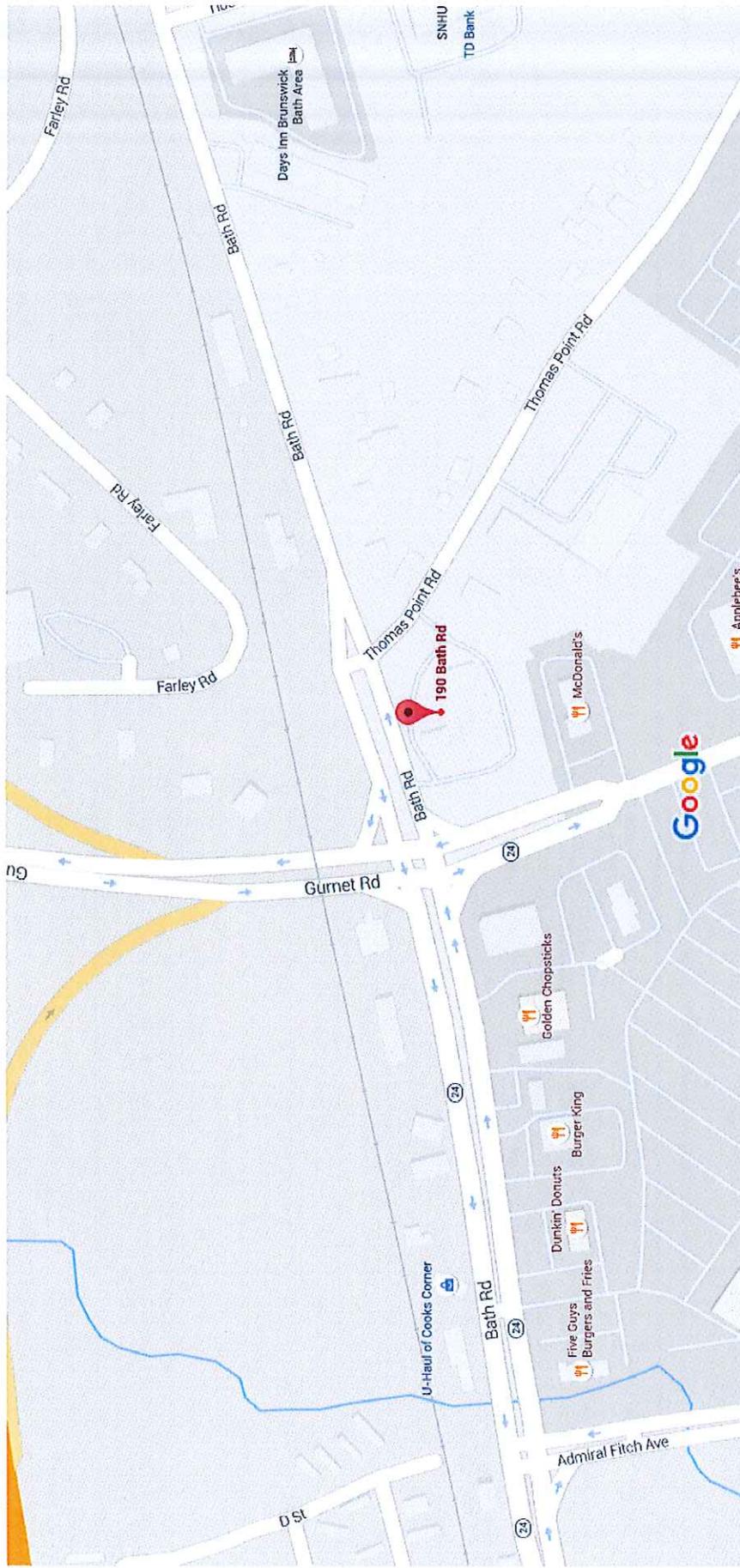
This map was generated by the Town of Brunswick's online GIS. This information has been compiled from various public and private sources. While every attempt has been made to provide accurate information, neither the municipality nor the service host guarantee the accuracy of information provided herein.

Map generated on: 6/28/2016



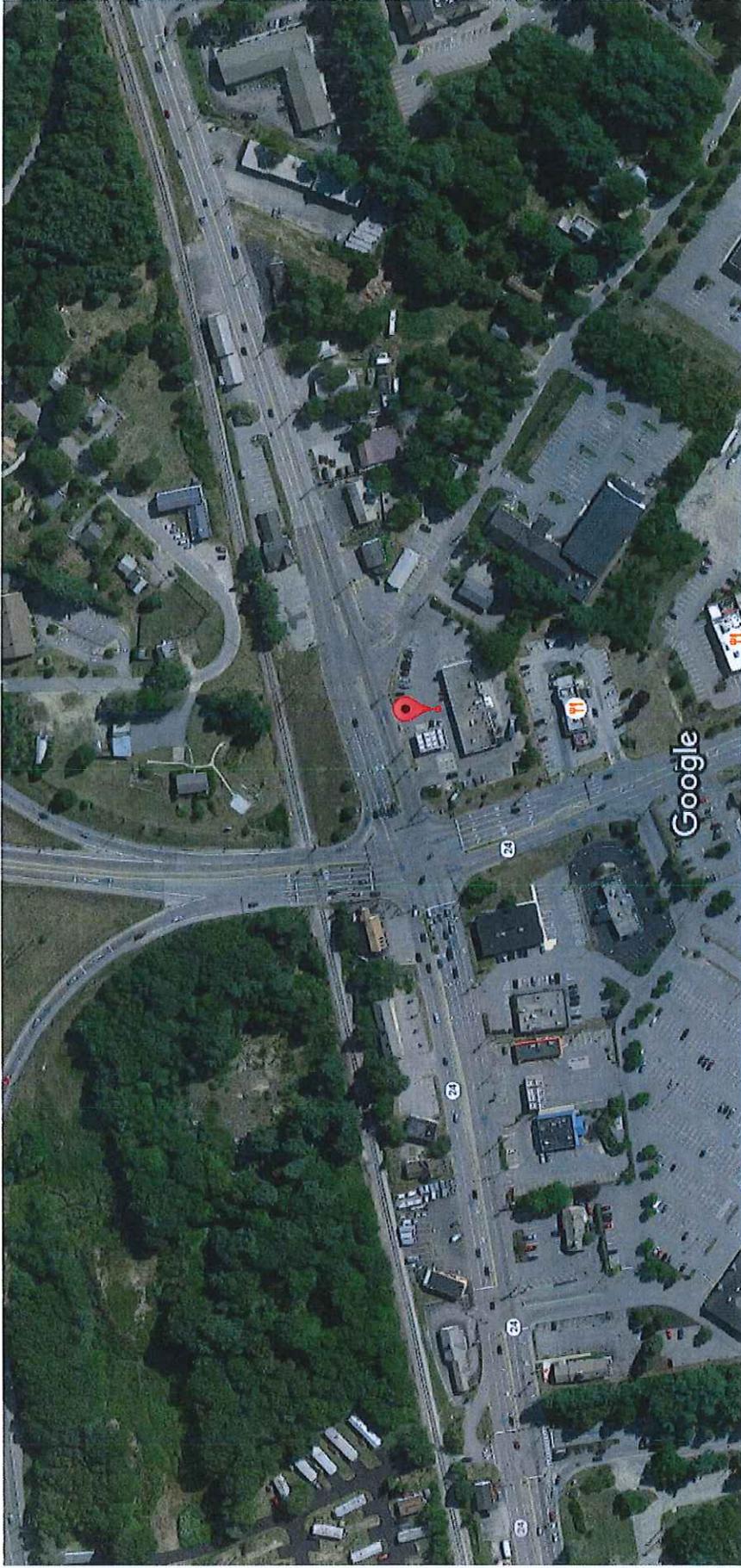
Sources: Esri, HERE, DeLorme, TomTom, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), swisstopo, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

Google Maps 190 Bath Rd



Map data ©2016 Google 200 ft

Google Maps 190 Bath Rd



Map data ©2016 Google 200 ft

5

**Sketch Plan Application of
LHB Enterprises, Inc.**

**Cumberland Farms Project
19 Bath Road**

LIST OF ABUTTERS

Map	Lot	Owner	Mailing Address
CC1	7	Ricky and Monique Lachapelle	379 Lisbon Street Lewiston, ME 04240
CC1	21	Just Because, LLC	P.O. Box 9340 Auburn, ME 04210
CC1	27	Prompto, Inc.	70 Scott Drive Westbrook, ME 04092
CC1	29A	McDonalds Corporation c/o Joe Breisacher	P.O. Box 653 Brunswick, ME 04011
CC1	31	Developers Diversified Cooks Corner LP	P.O. Box 228042 Beachwood, OH 44122
CC2	9	Crooker Enterprises, Inc. c/o Estes Lobster House	1909 Harpswell Neck Road Harpswell, ME 04079
CC2	11	State of Maine c/o MDOT	16 State House Station Augusta, ME 04333
CC2	29	Linda A. Johnson	206 Bath Road Brunswick, ME 04011
CC2	30	Cook's Corner Group, LLC	204 Bath Road Brunswick, ME 04011
CC2	31	Edward and Samantha Hannan	169 Cedar Pond Road Durham, ME 04222
CC2	32	Sunshine Too, Inc.	200 Bath Road Brunswick, ME 04011
CC2	42	Brunswick Landing Development Group, LLC	2 Main Street, Suite 200 Topsham, ME 04086
CC2	53	State of Maine c/o MDOT	16 State House Station Augusta, ME 04333

6

July 11, 2016

Ms. Anna Breinich
Director of Planning and Development
Town of Brunswick
85 Union Street
Brunswick, ME 04011

Re: Cumberland Farms, Inc.
Map CC1 Lot 28
190 Bath Road
Brunswick, ME 04011

Sub: Drainage Memorandum

Dear Ms. Breinich,

MHF Design Consultants, Inc., on behalf of our client, Cumberland Farms Inc., is pleased to provide the following information summarizing the proposed site re-development project relative to the effects it will have on stormwater management on the surrounding areas at the above referenced site. Please refer to the attached Proposed Site Re-Development Plans, dated July 11, 2016, as prepared by this office.

The property is located on a parcel of land that is approximately 1.17 Ac in size and identified as Tax Map CC1, Lot 28 and is located in the Cooks Corner Center zoning district at the southeast intersection of Bath Road and Route 24. The lot is currently occupied by a 6,883 sf Cumberland Farms Convenience Store which includes an AT&T Retail shop and Papa John's Pizza shop. The site also includes a fuel dispensing area with 4 dispensers (8 fueling locations) and an overhead canopy, 16 striped parking spaces and many additional un-striped paved areas accessed by driveways located along both Bath Road and Thomas Point Road.

Cumberland Farms Inc. proposes to raze the existing building and fuel dispensing area and construct a new standalone Cumberland Farms convenience store (4,786 sf), a new fuel canopy with five (5) dispensers (10 fueling locations), new fuel piping and two (2) new 20,000 gallon double wall fiberglass underground fuel storage tanks. Site improvements will also include a reconfigured parking area, new trash enclosure, bike rack and modified entrance driveways. New utility connections including water, gas, electric and sewer are proposed along with new landscaping to enhance the site.

Re-development of this parcel will result in a decrease in onsite impervious coverage of approximately 11,372 sf which will in turn reduce both the peak rate and volume of runoff discharging into the Bath Road and Thomas Point Road closed drainage systems. The proposed on-site stormwater management system includes installing several deep sump, hooded catch basins and the installation of two (2) hydrodynamic separators prior to discharge into the existing closed drainage systems in Bath Road and Thomas Point Road which will significantly improve



the stormwater quality. While there is an existing closed drainage system currently onsite, it appears that the pipes are undersized and that little or no pretreatment of the stormwater is provided before leaving the site.

Based on the reduction in impervious coverage of over 1/4-ac and the implementation of a new stormwater management system, we believe that the owner is improving the current conditions while reducing peak rates of runoff and volume discharging from the site and that there will be no negative impact on the abutting properties as a result of the proposed redevelopment project.

Please review the attached information and should you have any questions, please feel free to call our office at your convenience.

Sincerely,
MHF Design Consultants, Inc.

A handwritten signature in blue ink, appearing to read 'Chris Tymula', is written over a faint, larger version of the signature.

Chris Tymula
Project Manager

F:\Projects\Eng\375115\3751-Drainage Memo.doc

Cc: Dalina Boryszewski - Cumberland Farms, Inc. (EB Upload)
Sandra Guay - Woodman Edmands Danylik Austin Smith & Jacques, P.A.

7

GPI Greenman-Pedersen, Inc.

Engineering and Construction Services

REF.: MAX-2016046

July 7, 2016

Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

SUBJECT: Trip-Generation and Collision Summary Letter
Cumberland Farms Redevelopment
190 Bath Road
Brunswick, Maine

Dear Ms. Sousa:

Greenman-Pedersen Inc. (GPI) has prepared this letter to evaluate the expected trips associated with the proposed Cumberland Farms redevelopment located at 190 Bath Road in Brunswick, Maine. The subject sites consists of a Cumberland Farms facility that includes a $\pm 3,000$ square-foot (sf) convenience store with 4 Multi-Product Dispensers (MPDs) having eight (8) vehicle fueling positions (vfps) as well as $\pm 3,870$ sf of retail space which is occupied by an AT&T store ($\pm 2,250$ sf) and a Papa John's restaurant ($\pm 1,350$ sf). The development consists of razing the existing structures on the site and constructing a 4,786 sf convenience store with 5 MPDs having ten (10) vfps. Access and egress is currently provided via two driveways; one right-in/right-out only driveway on Bath Road and one full access/egress driveway on Thomas Point Road. Access and egress to the site will remain the same as part of the redevelopment, however, the Bath Road driveway will be modified/widened slightly to allow an easier right-turn movement onto Bath Road from the site. The subject site is located on the southeast quadrant of the signalized intersection of Bath Street (Route 24) at Gurnet Road (Route 24), bounded by Bath Road to the north, Thomas Point Road to the east, Gurnet Road (Route 24) to the west and a McDonald's restaurant to the south. The site location in relation to the surrounding roadways is shown on the map on Figure 1.

TRIP-GENERATION AND COLLISION SUMMARY LETTER

Cumberland Farms Redevelopment – Brunswick, Maine

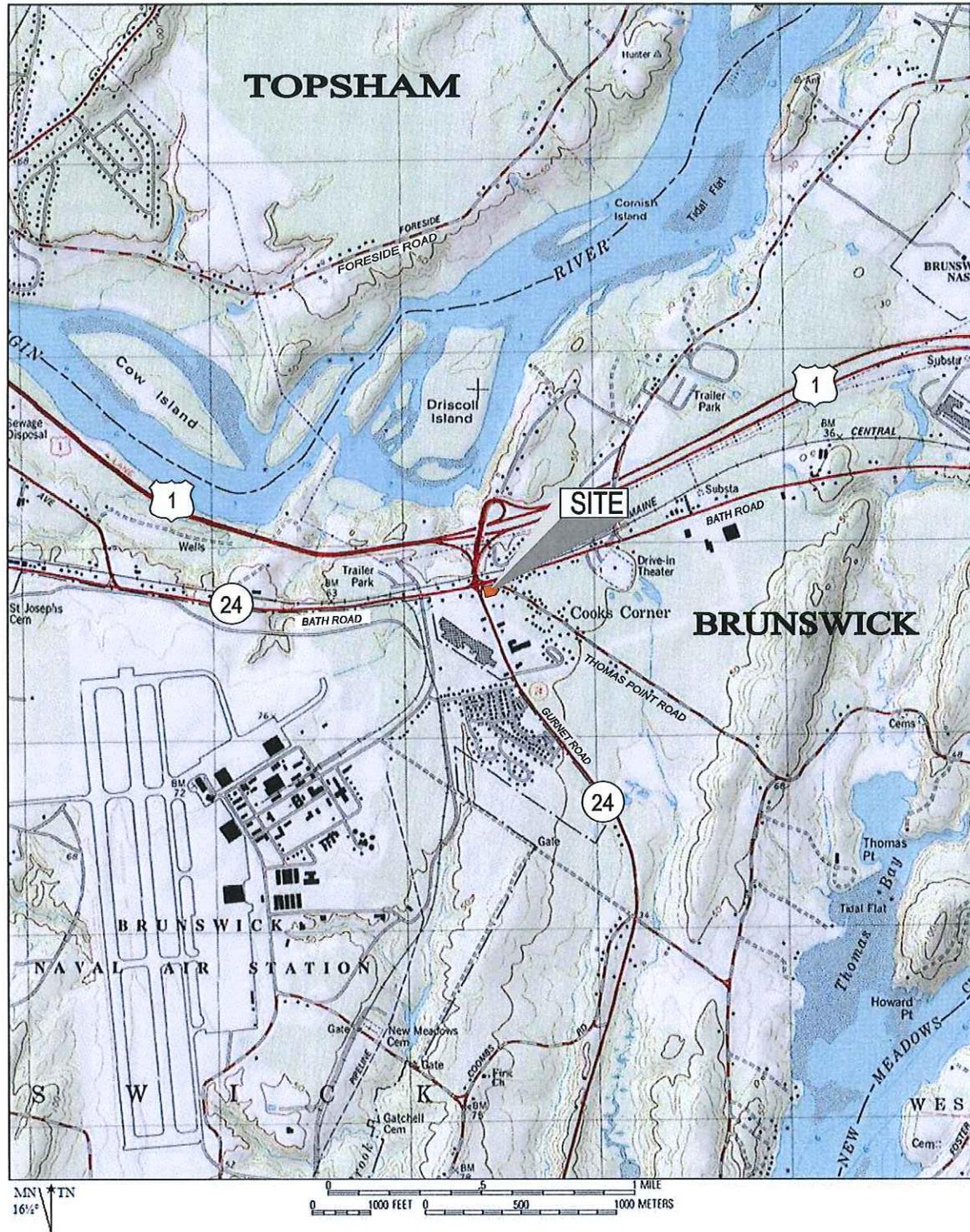


Figure 1
Site Location Map

Existing Conditions

Geometry

Adjacent to the site, Bath Road consists of two general-purpose travel lanes in each direction and one general-purpose lane in each direction beyond the vicinity, with exclusive turn lanes at major intersections. Gurnet Road consists of one general purpose lane in the northbound direction and two general-purpose lanes in the southbound direction with a two-way left-turn lane along the roadway and additional exclusive turn lanes at major intersections. Thomas Point Road consists of one general-purpose lane in each direction with no pavements markings. The posted speed limit along Bath Road and Gurnet Road is 35 miles per hour (mph) and the posted speed limit along Thomas Point Road is 25 mph. Currently, the Cumberland Farms site contains two curb cuts; one on Bath Road and one on Thomas Point Road. Access will remain the same but the Bath Road curb-cut will be modified to provide both right-in and right-out access.

Collision History

Collision data for the study area intersections were obtained from Maine Department of Transportation (MaineDOT) for the most recent three-year period available (2013 through 2015). A summary of the collision data at the intersections is provided in Table 1.

The signalized intersection of Bath Road (Route 24) at Gurnet Road (Route 24) has experienced on average 14.6 reported collisions per year. Of the 44 collisions, eight have resulted in injury and thirty-six resulted in property damage only. Thirty-two of the crashes were rear end collisions, nine were angle collisions, two were reported as other and one was a collision with the curb. Forty-eight percent of the reported crashes occurred during the weekday AM or weekday PM commuter peak periods and fourteen percent occurred during wet conditions.

The intersection of Bath Road at the Cumberland Farms driveway has experienced on average 1 reported collision per year. None of the three collisions resulted in personal injury. All three of the crashes were rear end collisions. None of the reported crashes occurred during the weekday AM or weekday PM commuter peak periods and thirty-three percent occurred during snowy conditions.

The intersection of Thomas Point Road at Cumberland Farms driveway has experienced one collision over the three-year period. The collision did not result in personal injury. The crash was an angle collision and occurred during the AM commuter peak hour. The collision occurred during icy conditions.

GPI

Ms. Kathleen A. Sousa
 July 7, 2016
 Page 4 of 7

Table 1
COLLISION SUMMARY

Location	Number of Collisions		Severity ^a			Collision Type ^b						Percent During	
	Total	Average per Year	PD	PI	F	CM	RE	HO	FO	Cyc	U	Commuter Peak ^c	Wet/Icy Conditions ^d
Bath Road (Rte 24) at Gurnet Road (Rte 24)	44	14.6	36	8	--	9	32	--	1	--	2	48%	14%
Bath Road at Cumberland Farms Dwy	3	1.00	3	--	--	--	3	--	--	--	--	0%	33%
Thomas Point Rd at Cumberland Farms Dwy	1	0.33	1	--	--	1	--	--	--	--	--	100%	100%

Source: MaineDOT (2013-2015).

^a PD = property damage only; PI = personal injury; F = fatality.

^b CM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; Cyc = cyclist; U = unknown.

^c Percent of vehicle incidents that occurred during the weekday AM and weekday PM commuter peak periods.

^d Represents the percentage of only "known" collisions occurring during inclement weather conditions.



Ms. Kathleen A. Sousa
July 7, 2016
Page 5 of 7

Design Conditions

The subject sites consists of a Cumberland Farms facility that includes a $\pm 3,000$ sf convenience store with 4 MPDs having eight (8) vfps as well as $\pm 3,870$ sf of retail space which is occupied by an AT&T store ($\pm 2,250$ sf) and a Papa John's Pizza restaurant ($\pm 1,350$ sf). The development consists of razing the existing structures on the site and constructing a 4,786 sf convenience store with 5 MPDs having ten (10) vfps.

Trip-Generation

For trip-generation purposes, traffic anticipated to be generated by the proposed Cumberland Farms redevelopment project was forecast using the trip rates contained in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*¹ utilizing Land Use Code (LUC) 853 (Convenience Market with Gasoline Pumps). Table 2 provides a trip-generation summary for the traffic anticipated to be generated by the proposed project. All trip-generation data are attached to this letter.

Not all of the vehicle trips expected to be generated by the proposed project represent *new* trips on the study area roadway system. Studies have shown that for developments such as the one proposed, a substantial portion of the site-generated vehicle trips are already present in the adjacent passing stream of traffic or are diverted from another route to the proposed site. Based on information published in the ITE *Trip Generation Handbook*, the average *pass-by* trip percentage is 63 percent during the weekday AM peak hour and 66 percent during the weekday PM peak hour for Convenience Markets with Gasoline Pumps (Land Use Code 853).² Table 2 summarizes the additional peak-hour trips expected to be generated as compared to the existing uses. All trip-generation data are attached to this letter.

¹ *Trip Generation Manual*, 9th Edition; Institute of Transportation Engineers; Washington, DC; 2012.

² *Trip Generation Handbook*; 3rd Edition; Institute of Transportation Engineers; Washington, DC; August 2014.



Ms. Kathleen A. Sousa
 July 7, 2016
 Page 6 of 7

Table 2
TRIP-GENERATION SUMMARY

Time Period/Direction	Existing Trips			Proposed Trips	Additional Trips		
	Retail ^a	Restaurant ^b	Total ^c	Convenience / Gasoline ^d	Total Trips ^e	Pass-By Trips ^f	New Trips ^g
Weekday Daily	4,100	630	4,730	5,430	700	440	260
Weekday AM Peak Hour:							
<i>Enter</i>	65	26	91	83	(8)	(1)	(7)
<i>Exit</i>	<u>58</u>	<u>21</u>	<u>79</u>	<u>83</u>	<u>4</u>	<u>(1)</u>	<u>5</u>
<i>Total</i>	123	47	170	166	(4)	(2)	(2)
Weekday PM Peak Hour:							
<i>Enter</i>	82	13	95	96	1	0	1
<i>Exit</i>	<u>86</u>	<u>10</u>	<u>96</u>	<u>95</u>	<u>(1)</u>	<u>0</u>	<u>(1)</u>
<i>Total</i>	168	23	191	191	0	0	0
Saturday Daily	1,400	610	2,010	2,040	30	20	10
Saturday Midday Peak Hour:							
<i>Enter</i>	31	25	56	51	(5)	(4)	(1)
<i>Exit</i>	<u>32</u>	<u>23</u>	<u>55</u>	<u>49</u>	<u>(6)</u>	<u>(4)</u>	<u>(2)</u>
<i>Total</i>	63	48	111	100	(11)	(8)	(3)

^a ITE *Trip Generation Handbook*, Procedure for Estimating Multi-Use Trip Generation for ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for 8 vfps and ITE Land Use Code 826 (Specialty Retail Center) for 2,250 sf.

^b ITE *Trip Generation Handbook*, Procedure for Estimating Multi-Use Trip Generation for ITE Land Use Code 933 (Fast-Food Restaurant without Drive-Through Window) for 1,350 sf.

^c Existing Retail Trips (which includes the Convenience/Gasoline Trips) plus Restaurant Trips.

^d ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for 10 vfps.

^e Proposed Trips minus Total Existing Trips.

^f 63 percent of Total Additional Trips during the Weekday, AM peak hour, Saturday, and Saturday midday peak hour; 66 percent of Total Additional Trips during the weekday PM peak hour.

^g Additional Total Trips minus Pass-By Trips.

As shown in Table 2, the proposed Cumberland Farms redevelopment project as compared to the existing uses is expected to generate 2 less vehicle trips (7 less entering and 5 more exiting) during the weekday AM peak hour, the same vehicle trips (1 more entering and 1 less exiting) during the weekday PM peak hour, and 3 less vehicle trips (1 less entering and 2 less exiting) during the Saturday

GPI

Ms. Kathleen A. Sousa

July 7, 2016

Page 7 of 7

midday peak hour. Traffic decreases as a result of the redevelopment are expected to be approximately 1 less vehicle every 20 to 30 minutes during the peak hours.

Since the section of Bath Road where the Cumberland Farms driveway exists is not under state jurisdiction, a MaineDOT Traffic Movement Permit (TMP) and Application for Driveway/Entrance Permit is not expected to be required. Should you have any questions, or require additional information, please contact me at (978) 570-2968.

Sincerely,

GREENMAN – PEDERSEN, INC.



for Heather L. Monticup, P.E.
Senior Project Manager

Attachment(s)

TRIP-GENERATION AND COLLISION SUMMARY

Cumberland Farms Redevelopment – Brunswick, Maine

ATTACHMENTS

TRIP-GENERATION WORKSHEETS

Trip Generation Summary

Existing Gas Station = 8 Vehicle Fueling Positions
 Existing Retail = 2,250 Square Feet
 Existing Restaurant = 1,350 Square Feet
 Proposed Gas Station = 10 Vehicle Fueling Positions

	Existing Trips (External)			Proposed Trips	Additional Trips		
	Retail	Restaurant	Total Trips	Retail	Total Trips	Pass-By Trips	New Trips
Weekday Daily	4,102	630	4,732	5,426	694	437	257
Weekday AM Peak Hour	65	26	91	83	-8	-1	-7
	<u>58</u>	<u>21</u>	<u>78</u>	<u>83</u>	<u>5</u>	<u>-1</u>	<u>6</u>
	123	47	170	166	-4	-2	-1
Weekday PM Peak Hour	82	13	95	96	1	0	1
	<u>86</u>	<u>10</u>	<u>96</u>	<u>95</u>	<u>-1</u>	<u>0</u>	<u>-1</u>
	168	23	191	191	0	0	0
Saturday Daily	1,401	611	2,012	2,044	32	20	12
Saturday Midday Peak Hour	31	25	56	51	-5	-4	-2
	<u>32</u>	<u>23</u>	<u>55</u>	<u>49</u>	<u>-6</u>	<u>-4</u>	<u>-2</u>
	63	48	111	100	-11	-7	-4

Pass-By Percentages
 LUC 853
 AM 63%
 PM 66%
 SAT 63%

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 853 - Convenience Market with Gasoline Pumps

Average Vehicle Trips Ends vs: Vehicle Fueling Positions

Independent Variable (X): 8

AVERAGE WEEKDAY DAILY

$$T = 542.60 * (X)$$

$$T = 542.60 * 8$$

$$T = 4340.80$$

T = 4,340 vehicle trips

with 50% (2,170 vpd) entering and 50% (2,170 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 16.57 * (X)$$

$$T = 16.57 * 8$$

$$T = 132.56$$

T = 133 vehicle trips

with 50% (67 vph) entering and 50% (66 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 19.07 * (X)$$

$$T = 19.07 * 8$$

$$T = 152.56$$

T = 153 vehicle trips

with 50% (77 vph) entering and 50% (76 vph) exiting.

SATURDAY DAILY

$$T = 204.47 * (X)$$

$$T = 204.47 * 8$$

$$T = 1635.76$$

T = 1,636 vehicle trips

with 50% (818 vpd) entering and 50% (818 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

$$T = 10.00 * (X)$$

$$T = 10.00 * 8$$

$$T = 80.00$$

T = 80 vehicle trips

with 51% (41 vph) entering and 49% (39 vph) exiting.

Institute of Transportation Engineers (ITE)
Land Use Code (LUC) 826 - Specialty Retail Center

Average Vehicle Trips Ends vs: 1,000 Sq. Feet Gross Leasable Area
 Independent Variable (X): 2.250

AVERAGE WEEKDAY DAILY

T = 44.32 * (X)
 T = 44.32 * 2.250
 T = 99.72
 T = 100 vehicle trips
 with 50% (50 vpd) entering and 50% (50 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$\frac{\text{ITE LUC 820 Weekday Morning Trip Rate}}{\text{ITE LUC 820 Weekday Evening Trip Rate}} = \frac{\text{ITE LUC 826 Weekday Morning Trip Rate}}{\text{ITE LUC 826 Weekday Evening Trip Rate}}$$

$$\frac{0.96}{3.71} = \frac{(Y)}{2.71} \quad Y = 0.70123989$$

T = Y * 2.250
 T = 1.578
 T = 2 vehicle trips
 with 62% (1 vph) entering and 38% (1 vph) exiting.

(same distribution split as ITE LUC 820 during the weekday morning peak hour of adjacent street traffic)

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

T = 2.40 * (X) + 21.48
 T = 2.40 * 2.250 + (21.48)
 T = 26.88
 T = 27 vehicle trips
 with 44% (12 vph) entering and 56% (15 vph) exiting.

SATURDAY DAILY

T = 42.04 * (X)
 T = 42.04 * 2.250
 T = 94.59
 T = 94 vehicle trips
 with 50% (47 vpd) entering and 50% (47 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR

$$\frac{\text{ITE LUC 820 Saturday Midday Trip Rate}}{\text{ITE LUC 820 Saturday Daily Trip Rate}} = \frac{\text{ITE LUC 826 Saturday Midday Trip Rate}}{\text{ITE LUC 826 Saturday Daily Trip Rate}}$$

$$\frac{4.82}{49.97} = \frac{(Y)}{42.04} \quad Y = 4.05508905$$

T = Y * 2.250
 T = 9.124
 T = 9 vehicle trips
 with 52% (5 vph) entering and 48% (4 vph) exiting.

(same distribution split as ITE LUC 820 during the Saturday midday peak hour of generator)

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 933 - Fast-Food Restaurant without Drive-Through Window

Average Vehicle Trips Ends vs: 1,000 Sq. Feet Gross Floor Area
Independent Variable (X): 1.350

AVERAGE WEEKDAY DAILY

$T = 716.00 * (X)$
 $T = 716.00 * 1.350$
 $T = 966.60$
 $T = 968$ vehicle trips
with 50% (484 vpd) entering and 50% (484 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$T = 43.87 * (X)$
 $T = 43.87 * 1.350$
 $T = 59.22$
 $T = 59$ vehicle trips
with 60% (35 vph) entering and 40% (24 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$T = 26.15 * (X)$
 $T = 26.15 * 1.350$
 $T = 35.30$
 $T = 35$ vehicle trips
with 51% (18 vph) entering and 49% (17 vph) exiting.

SATURDAY DAILY

$T = 696.00 * (X)$
 $T = 696.00 * 1.350$
 $T = 939.60$
 $T = 940$ vehicle trips
with 50% (470 vpd) entering and 50% (470 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

$T = 54.55 * (X)$
 $T = 54.55 * 1.350$
 $T = 73.64$
 $T = 74$ vehicle trips
with 49% (36 vph) entering and 51% (38 vph) exiting.

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 853 - Convenience Market with Gasoline Pumps

Average Vehicle Trips Ends vs: Vehicle Fueling Positions
Independent Variable (X): 10

AVERAGE WEEKDAY DAILY

$$T = 542.60 * (X)$$

$$T = 542.60 * 10$$

$$T = 5426.00$$

T = 5,426 vehicle trips
with 50% (2,713 vpd) entering and 50% (2,713 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 16.57 * (X)$$

$$T = 16.57 * 10$$

$$T = 165.70$$

T = 166 vehicle trips
with 50% (83 vph) entering and 50% (83 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 19.07 * (X)$$

$$T = 19.07 * 10$$

$$T = 190.70$$

T = 191 vehicle trips
with 50% (96 vph) entering and 50% (95 vph) exiting.

SATURDAY DAILY

$$T = 204.47 * (X)$$

$$T = 204.47 * 10$$

$$T = 2044.70$$

T = 2,044 vehicle trips
with 50% (1,022 vpd) entering and 50% (1,022 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

$$T = 10.00 * (X)$$

$$T = 10.00 * 10$$

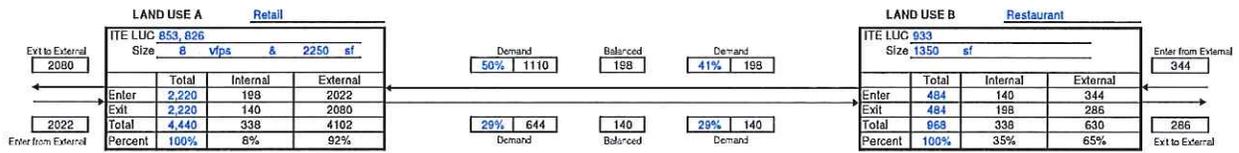
$$T = 100.00$$

T = 100 vehicle trips
with 51% (51 vph) entering and 49% (49 vph) exiting.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Weekday Daily



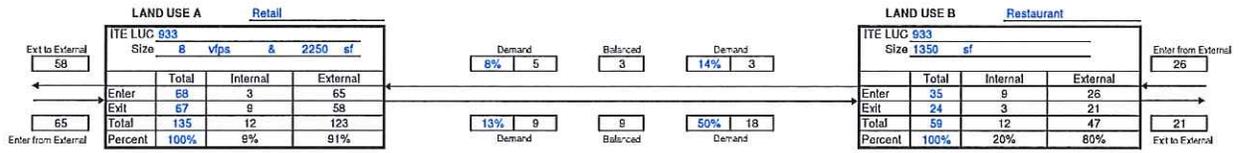
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	2022	2080	4102	4440	
Land Use B	344	286	630	968	
TOTAL	2366	2366	4732	5408	13%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlp: Cumberland Farms
 Time Period: Weekday AM Peak Hour



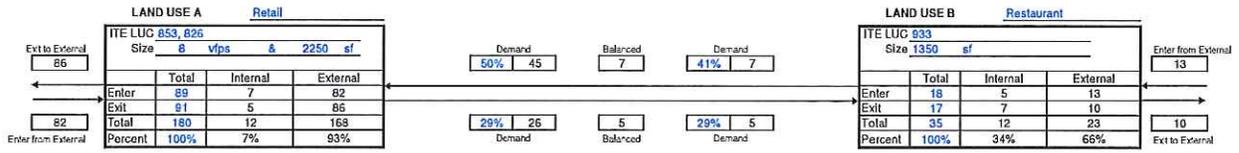
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	65	58	123	135	
Land Use B	26	21	47	59	
TOTAL	91	78	170	194	13%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Weekday PM Peak Hour



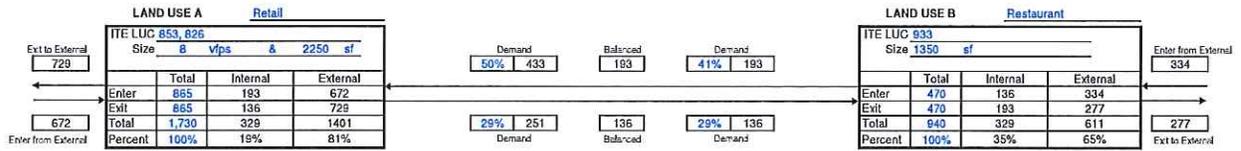
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	82	86	168	180	
Land Use B	13	10	23	35	
TOTAL	95	96	191	215	11%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Saturday Daily



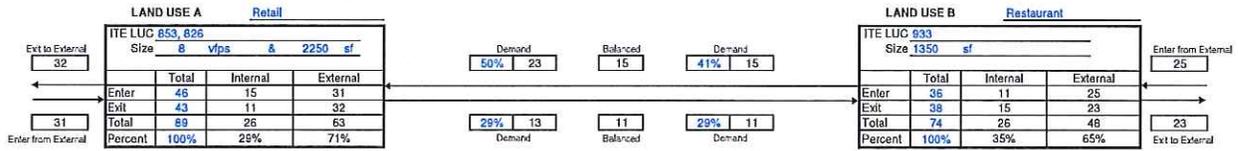
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	672	729	1401	1730	
Land Use B	334	277	611	940	
TOTAL	1006	1006	2012	2570	25%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlp: Cumberland Farms
 Time Period: Saturday Midday Peak Hour



	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	31	32	63	89	
Land Use B	25	23	48	74	
TOTAL	56	55	111	163	32%

Based on ITE Trip Generation Handbook, June 2004.

**Table F.14 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period
Land Use Code 851—Convenience Market (Open 24 Hours)**

SIZE (1,000 SQ. FT. (GFA))	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
3	Overland Park, KS	Aug. 1987	68	4:30-5:30 p.m.	34	53	13	66	—	—
3	Overland Park, KS	July 1987	68	4:30-5:30 p.m.	28	50	22	72	—	—
~1.9	Billings, MT	1987	461	4:00-6:00 p.m.	62	13	25	30	—	ITE Montana Section Tech Comm
<50.0	Chicago suburbs, IL	1987	72	3:00-6:00 p.m.	28	—	—	72	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	54	3:00-6:00 p.m.	78	—	—	22	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	34	3:00-6:00 p.m.	69	—	—	31	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	100	3:00-6:00 p.m.	63	—	—	37	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	43	3:00-6:00 p.m.	43	—	—	57	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	135	3:00-6:00 p.m.	39	—	—	61	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	74	3:00-6:00 p.m.	53	—	—	47	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	80	3:00-6:00 p.m.	64	—	—	36	—	Kenig, O'Hara, Humes, Flock

Average Pass-By Trip Percentage: 51

"—" means no data were provided

**Table F.15 Pass-By and Non-Pass-By Trips Weekday, AM Peak Period
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. (GFA))	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	7:00-9:00 a.m.	54	11	35	46	1,240	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	7:00-9:00 a.m.	48	17	35	52	1,210	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	47	7:00-9:00 a.m.	62	19	19	38	1,705	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	—	7:00-9:00 a.m.	72	15	13	28	940	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	49	7:00-9:00 a.m.	66	16	18	34	990	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	62	7:00-9:00 a.m.	74	10	16	26	790	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	58	7:00-9:00 a.m.	64	5	31	36	1,255	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	79	7:00-9:00 a.m.	56	6	38	44	635	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	49	7:00-9:00 a.m.	67	4	29	33	1,985	Barton-Aschman Assoc.

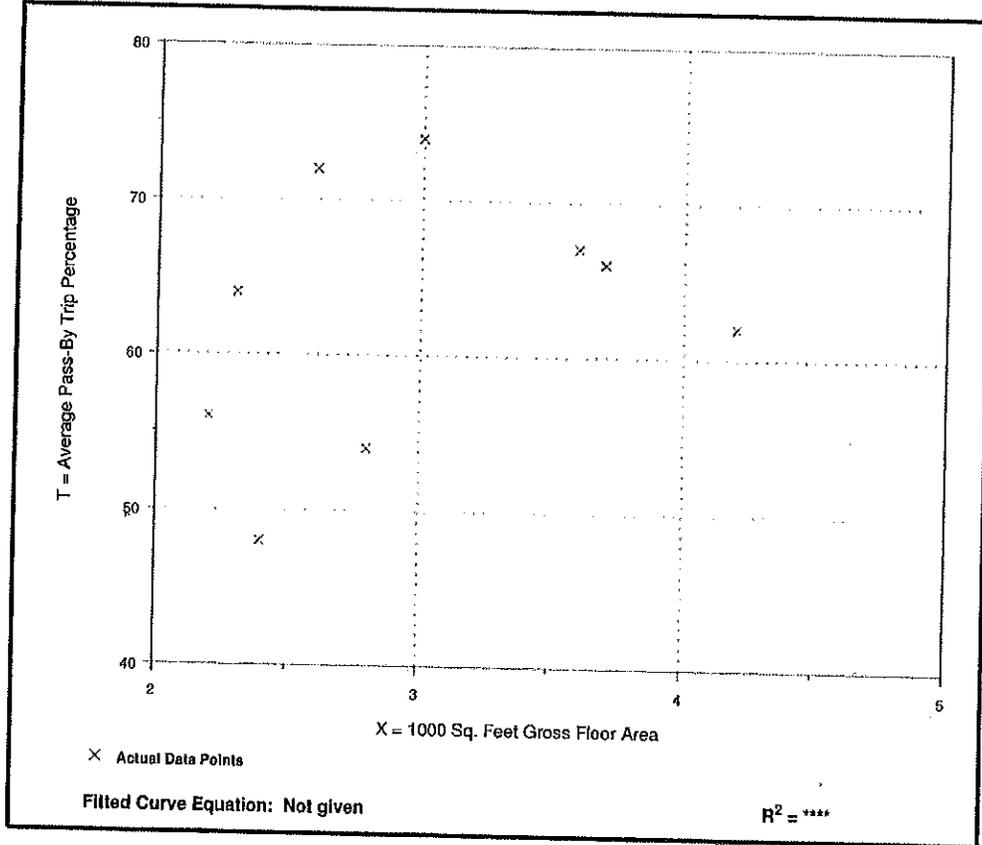
Average Pass-By Trip Percentage: 63

"—" means no data were provided

Figure F.11 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: 1000 Sq. Feet Gross Floor Area
On a: Weekday, A.M. Peak Period
Number of Studies: 9
Average 1000 Sq. Feet GFA: 3

Data Plot



**Table F.16 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. GSA)	LOCATION	WEEKDAY SURVEY DATE	No. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET CAPACITY US/CONF.	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	4:00–6:00 p.m.	62	11	27	38	2,875	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	4:00–6:00 p.m.	58	13	29	42	2,655	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	61	4:00–6:00 p.m.	58	26	16	42	2,300	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	68	4:00–6:00 p.m.	67	15	18	33	950	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	70	4:00–6:00 p.m.	61	16	23	39	2,175	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	80	4:00–6:00 p.m.	65	15	20	35	1,165	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	67	4:00–6:00 p.m.	57	16	27	43	1,954	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	115	4:00–6:00 p.m.	48	16	36	52	820	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	60	4:00–6:00 p.m.	56	17	27	44	2,505	Barton-Aschman Assoc.
2.6	Seminole Co., FL	1989	82	4:00–6:00 p.m.	73	20	7	27	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	98	4:00–6:00 p.m.	81	15	4	19	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	115	4:00–6:00 p.m.	69	16	15	31	—	Tipton Associates Inc.
2.6	Volusia Co., FL	1989	98	4:00–6:00 p.m.	74	15	11	26	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	4:00–6:00 p.m.	74	24	2	26	—	Tipton Associates Inc.
2.7	Volusia Co., FL	1989	82	4:00–6:00 p.m.	87	8	5	13	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	99	2:00–4:00 p.m.	64	28	8	36	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	2:00–4:00 p.m.	68	21	11	32	—	Tipton Associates Inc.

Average Pass-By Trip Percentage: 66

"—" means no data were provided

Figure F.12 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: 1000 Sq. Feet Gross Floor Area
On a: Weekday, P.M. Peak Period
Number of Studies: 17
Average 1000 Sq. Feet GFA: 3

Data Plot

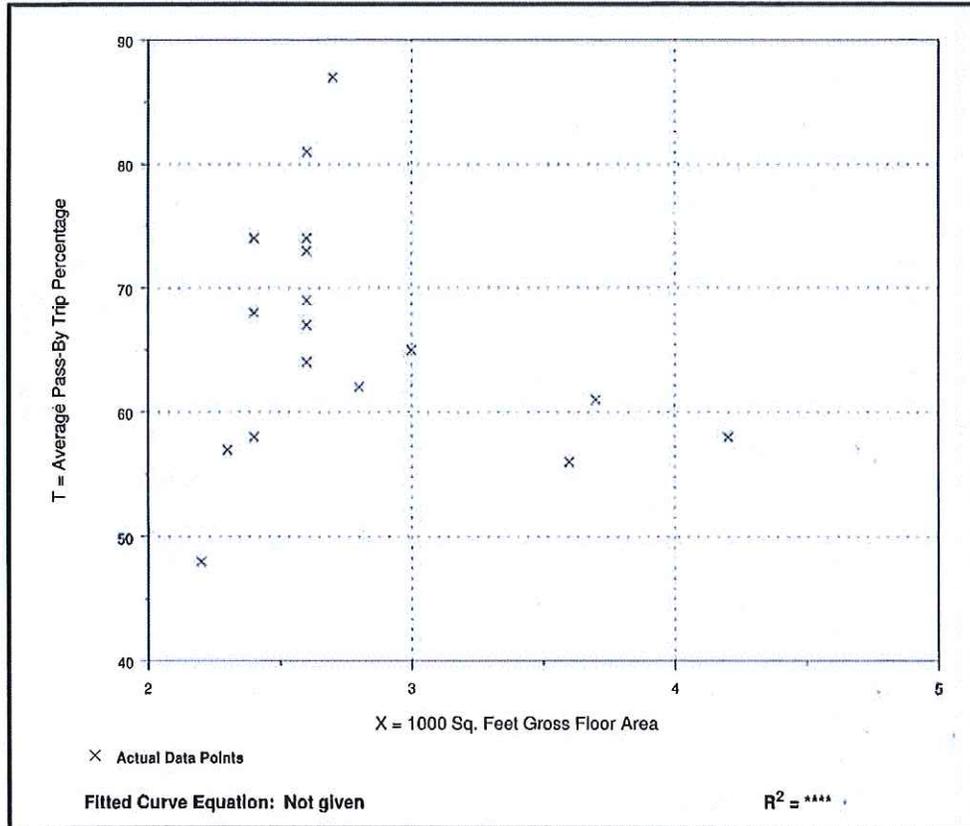
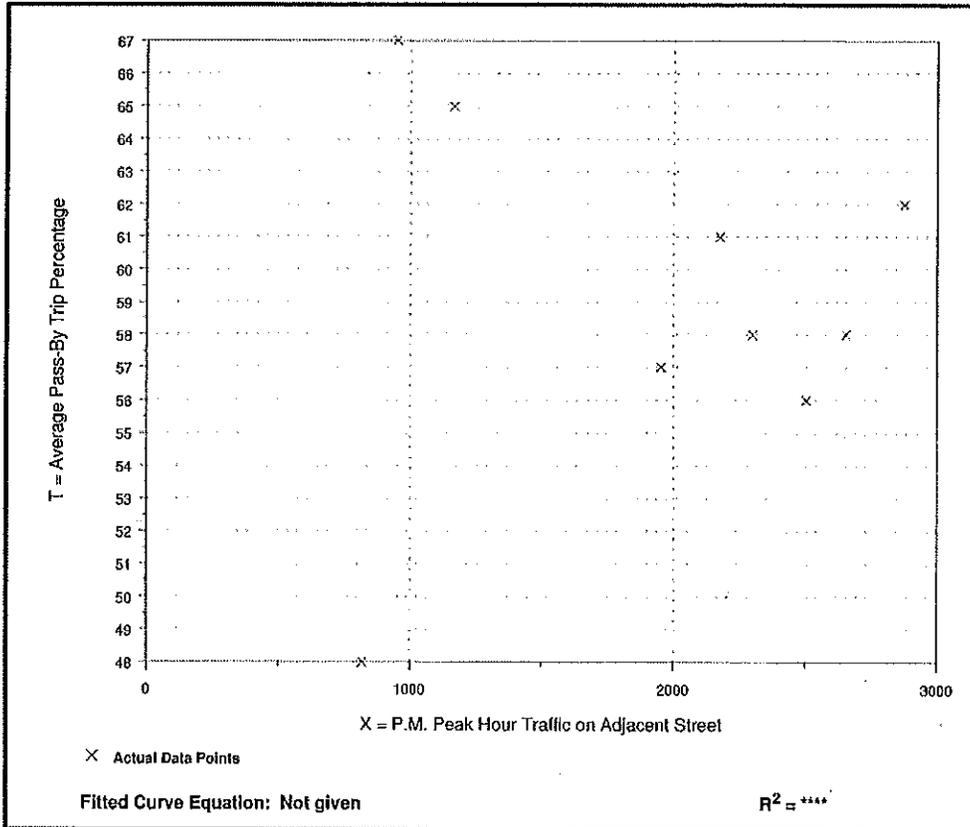


Figure F.13 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: P.M. Peak Hour Traffic on Adjacent Street
 On a: Weekday, P.M. Peak Period
 Number of Studies: 9
 Avg. P.M. Peak Hr. Traf. on Adj. Street: 1,933

Data Plot



8

**CUMBERLAND FARMS SITE PLAN APPLICATION
190 BATH ROAD, BRUNSWICK, MAINE**

WAIVER REQUESTS

Pursuant to Section 205.3.A. of the Brunswick Zoning Ordinance, and due to the unique shape of the subject parcel, the required location of the fuel service area, and the location of the sewer easement on the south side of the parcel, Cumberland Farms respectfully requests the following waivers from the Cook's Corner Design Standards:

1. Corner Lot Treatment: The proposed redesign meets the required minimum setbacks from all street frontages. Due to the unique shape of the parcel, the location of the sewer easement, and for safety reasons, the fuel service area must however be located in the front of the store with clear visibility from the retail cashier location. As such, Cumberland Farms requests a waiver from the design standard requiring no parking, vehicular travel, or service areas to be located between the building and property lines on both streets.
2. Corner Buildings: The design of the redeveloped structure is two-story with a pitched roof and dormers. Due to the nature of the business as well as required parking, the second story does not have useable floor area and Cumberland Farms requests a waiver from this design standard.
3. Entrance (Corner Lot): Due to the unique shape of the parcel and the necessary location of the placement of the fuel service area within a straight visual line from the retail cashier, Cumberland Farms requests a waiver from the design requirement that the entrance be located on the corner.
4. Side Lot Parking: Due to the unique shape of the parcel, some of the parking spaces along the sideline will extend closer to Thomas Point Road than the front façade of the building, although not closer than the canopy structure for the fuel service area. To the extent that a waiver is required, Cumberland Farms requests a waiver from this design standard.
5. Relationship of Building to Thomas Point Road: Due to the unique shape of the subject parcel, the need to orient the retail building facing Bath Road, and the location of the underground fuel storage tanks, the side of the structure facing Thomas Point Road is further than 25 feet from the property line. The Design Guidelines reference the "front lot line" and Thomas Road is the side lot line for this project. To the extent that a waiver is required, Cumberland Farms request such waiver.
6. Relationship of Building to Bath Road: Due to the unique shape of the parcel, the required location of the fuel service area, and the need for vehicles to safely access the fuel service area, the building is located 108.7 feet from the front lot line which is further than the maximum 100-foot setback in the Design Guidelines. Cumberland Farms therefore requests a waiver from this Design Standard.

PROPOSED SITE RE-DEVELOPMENT PLANS

for

ASSESSORS MAP CC1 LOT 28

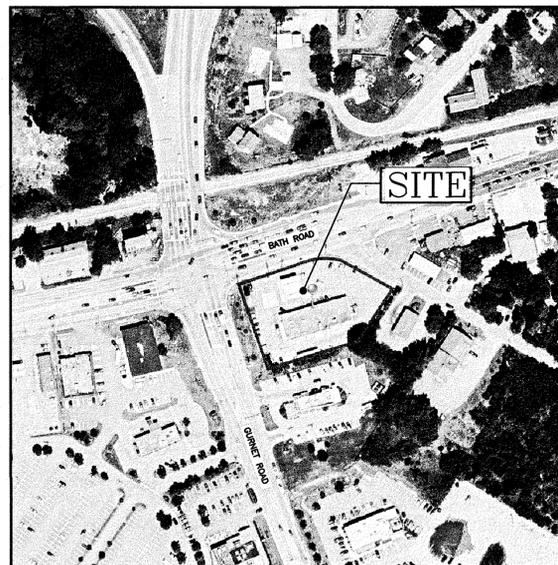
190 BATH ROAD

BRUNSWICK, ME 04011

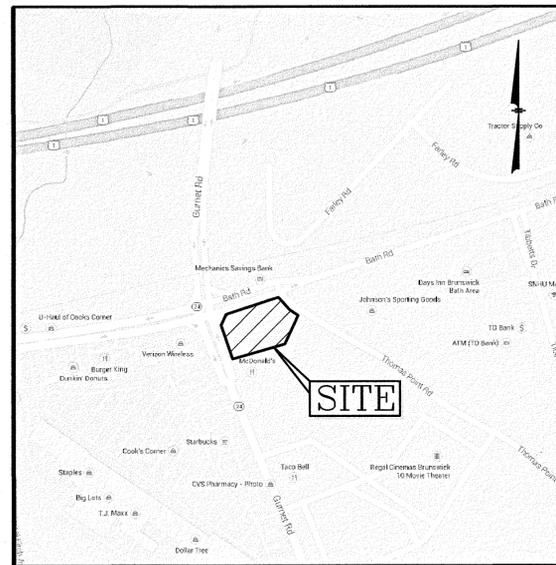
Prepared for:



100 CROSSING BLVD, FRAMINGHAM, MASSACHUSETTS 01702



AERIAL IMAGE
(NOT TO SCALE)



LOCATION MAP
(NOT TO SCALE)

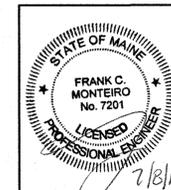
INDEX TO DRAWINGS

- CFG01.0 TITLE SHEET
- CFG02.0 ALTA/ACSM LAND TITLE SURVEY
- CFG03.0 DEMOLITION PLAN
- CFG04.0 SITE PLAN
- CFG05.0 GRADING & DRAINAGE PLAN
- CFG06.0 EROSION & SEDIMENTATION CONTROL PLAN
- CFG07.0 UTILITY PLAN
- CFG08.0 LANDSCAPE PLAN
- CFG09.0 DETAIL SHEET
- CFG09.1 DETAIL SHEET
- CFG09.2 DETAIL SHEET
- CFG09.3 DETAIL SHEET
- CFG10.0 LIGHTING PLAN
- CFG10.1 FIXTURE SPECIFICATION SHEET
- CFG11.0 FIRE SUPPRESSION PLAN (BY OTHERS)
- CFG12.0 PROPOSED CANOPY PLAN & ELEVATIONS
- CFG13.0 SIGN PLAN 1
- CFG13.1 SIGN PLAN 2
- CFG16.0 TRUCK TURN PLAN (CFG TANKER)
- A1.1 FLOOR PLAN & PARTITION TYPES *
- A3.1 EXTERIOR ELEVATIONS *
- A3.2 EXTERIOR ELEVATIONS *
- DS1.1 DOWNSPOUT LOCATION PLAN *
- L1.1 EXTERIOR BUILDING LIGHTING PLAN *

* ARCHITECTURAL PLANS FOR PERMITTING USE ONLY.
CONTRACTOR TO REFER TO STAMPED, SIGNED, SEALED
PLANS LABELED "FOR CONSTRUCTION".



44 Silles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

190 BATH ROAD
BRUNSWICK, MAINE 04011

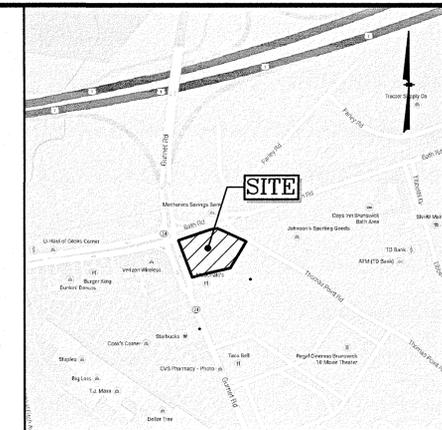
SCALE: AS NOTED
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

Cumberland FARMS

TITLE SHEET
CFG01.0

F:\Projects\CA0A\375115\3751SP.dwg Model 7/07/16 11:02am dmp



LOCATION MAP
(NOT TO SCALE)

MAP CC1 LOT 31
N/F DEVELOPERS DIVERSIFIED
COOKS CORNER LP
3300 ENTERPRISE PKWY.
PO BOX 228042
BEACHWOOD, OHIO 44122
BOOK 13256 PAGE 134

MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1 LOT 27
N/F PROMPTO, INC.
70 SCOTT DRIVE
WESTBROOK, ME 04092
BOOK 8252 PAGE 135

MAP CC2 LOT 32
N/F SUNSHINE TOO, INC.
200 BATH ROAD
BRUNSWICK, ME 04011
BOOK 17214 PAGE 199

MAP CC2 LOT 53
N/F STATE OF MAINE C/O
DEPT OF TRANSPORTATION
16 STATE STATION HOUSE
AUGUSTA, ME 04333
BOOK PAGE

NOTES:

- 1) LOCATION OF UNDERGROUND UTILITIES IS APPROXIMATE ONLY. ADDITIONAL UNDERGROUND UTILITIES OTHER THAN THOSE SHOWN MAY BE ENCOUNTERED.
- 2) ELEVATIONS SHOWN HEREON ARE ON NGVD 1929. CURB ELEVATIONS SHOWN ARE AT THE "TOE" OF CURB. CURBS ARE 0.50'± HIGH.
- 3) THE SURVEY TRACT IS NOT LOCATED WITHIN A FLOOD HAZARD ZONE AS SHOWN ON THE FLOOD INSURANCE RATE MAP FOR THE TOWN OF BRUNSWICK, MAINE CUMBERLAND COUNTY, PANEL 15 OF 35, COMMUNITY PANEL NUMBER 230042 0015 B; EFFECTIVE DATE: JANUARY 3, 1986.
- 4) REFER TO FIRST AMERICAN TITLE INSURANCE COMPANY COMMITMENT FOR TITLE INSURANCE #NCS-727776-HOU1, EFFECTIVE: APRIL 23 2015.
- 5) NONE OF THE SURVEY TRACT LIES WITHIN THE BOUNDS OF ANY ADJACENT STREETS, ROADS OR WAYS.

PLAN REFERENCES:

- 1) CUMBERLAND COUNTY REGISTRY OF DEEDS (C.C.R.D.) PLAN BOOK 197 PAGE 350.
- 2) C.C.R.D. PLAN BOOK 195 PAGE 161.
- 3) C.C.R.D. PLAN BOOK 118 PAGE 28.
- 4) C.C.R.D. PLAN BOOK 66 PAGE 68.
- 5) C.C.R.D. PLAN BOOK 66 PAGE 58.
- 6) STATE OF MAINE DEPARTMENT OF TRANSPORTATION RIGHT OF WAY MAP. D.O.T. FILE NO. 3-350 DATED APRIL, 1987.
- 7) STANDARD BOUNDARY SURVEY ON ROUTE 24, BRUNSWICK, MAINE MADE FOR McDONALD'S CORPORATION; SCALE: 1"=20'; DATE: APRIL 2, 1993 BY OWEN HASKELL, INC. COPY OBTAINED FROM THE TOWN OF BRUNSWICK PLANNING DEPARTMENT.

CERTIFICATION:

TO: CUMBERLAND FARMS, INC., HINCKLEY, ALLEN & SNYDER AND FIRST AMERICAN TITLE INSURANCE COMPANY

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS IN 2011, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 7A, 7B, 8, 9, 10, 11B, 12, 13 AND 19 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON APRIL 7, 2015.



Gordon D. Hislop Jr. 7/2/16
DATE OF PLAT OR MAP:

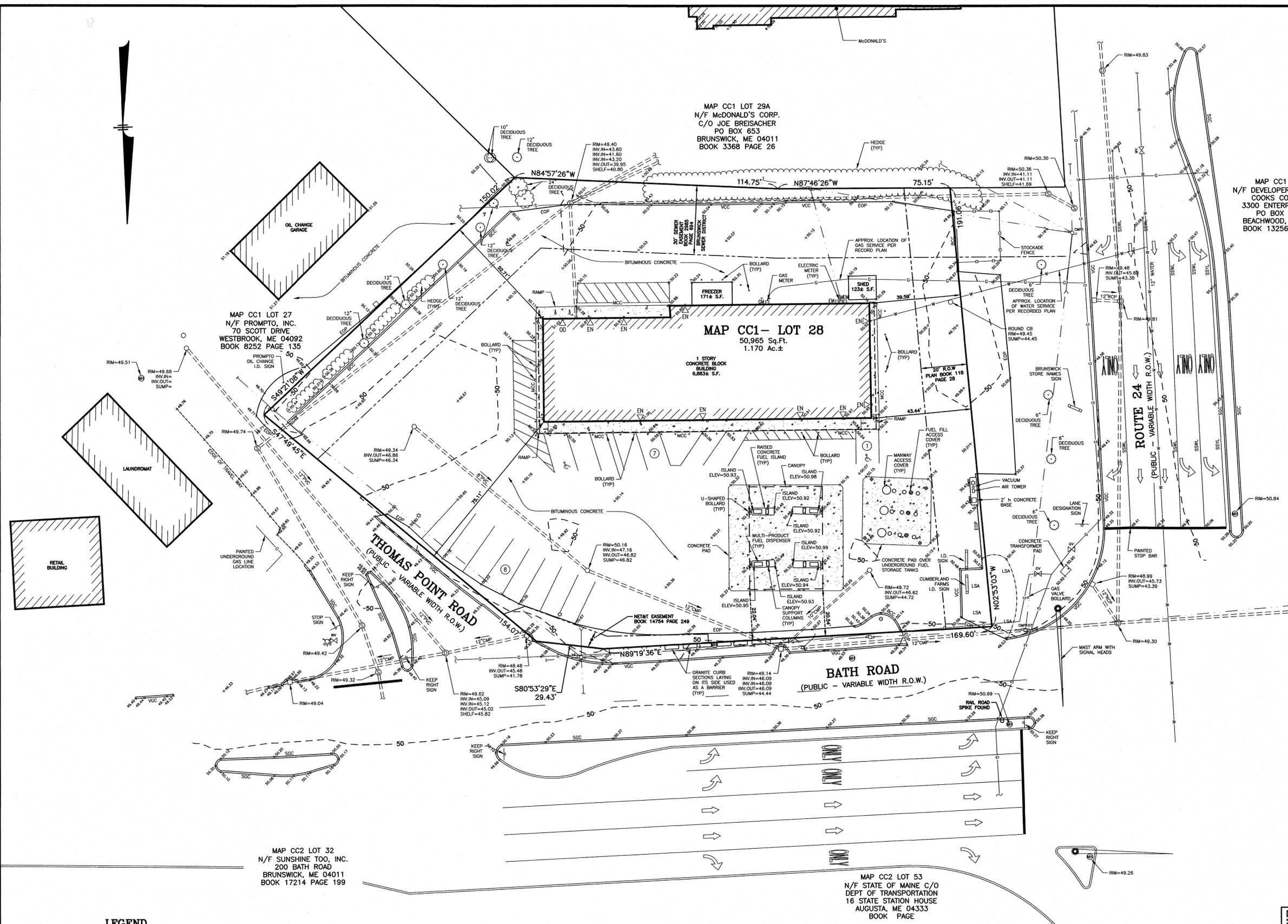


MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

REVISIONS		
NO.	DATE	REV. BY. DESCRIPTION

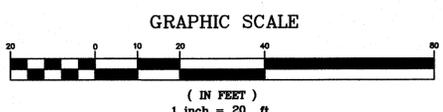
50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

190 BATH ROAD
BRUNSWICK, MAINE 04011
CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702
SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751TWS.dwg
DRAWN BY: MJK
CHECKED BY: FCM
ALTA/ACSM LAND TITLE SURVEY CFG02.0



LEGEND

	VERTICAL GRANITE CURB		UTILITY POLE
	SLOPED GRANITE CURB		DRAIN MANHOLE
	BITUMINOUS CONCRETE LIP CURBING		SEWER MANHOLE
	OVERHEAD SERVICE WIRES		TELEPHONE MANHOLE
	SINGLE SOLID WHITE LINE		CATCH BASIN
	BROKEN WHITE LINE		WATER LINE
	SIGN		WATER VALVE
	TEST BORING		FIRE HYDRANT
	TREELINE		GAS VALVE
	CONTOUR ELEVATION		GAS LINE
	SPOT ELEVATION		

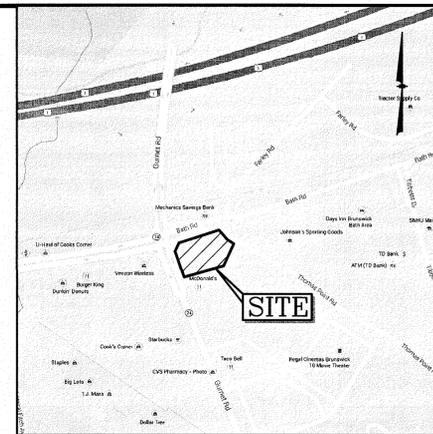


OWNER OF RECORD:
LHB ENTERPRISES
3 BATTERY WHARF UNIT 3411
BOSTON, MASSACHUSETTS 02109
BOOK 13060 PAGE 174



F:\Projects\375115\3751TWS.dwg ALTA 7/08/16 8:56am.cmf

SEE EROSION & SEDIMENTATION CONTROL PLAN FOR CONSTRUCTION SEQUENCE, TEMPORARY EROSION CONTROL MEASURES, AND LOCATION OF EROSION CONTROL DEVICES.

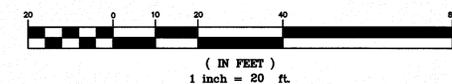


LOCATION MAP
(NOT TO SCALE)

NOTES:

- 1) A DEMOLITION PERMIT MUST BE OBTAINED FROM THE TOWN OF BRUNSWICK PRIOR TO COMMENCEMENT OF WORK. ALL EXISTING UTILITY DISCONNECTIONS MUST BE COORDINATED WITH RESPECTIVE UTILITY COMPANIES.
- 2) ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN STRICT ADHERENCE TO ALL FEDERAL, STATE AND LOCAL REGULATIONS. CONTRACTOR TO INSTALL EROSION CONTROL DEVICES IN ACCORDANCE WITH GRADING & DRAINAGE PLAN PRIOR TO BEGINNING DEMOLITION ACTIVITIES.
- 3) PROCEED WITH DEMOLITION IN A SYSTEMATIC MANNER, FROM THE TOP OF THE STRUCTURE(S) TO THE GROUND.
- 4) DEMOLISH CONCRETE IN ALL SECTIONS.
- 5) BREAK UP CONCRETE SLABS-ON-GRADE, UNLESS OTHERWISE DIRECTED BY THE CONSTRUCTION MANAGER.
- 6) CONDUCT ALL DEMOLITION OPERATIONS IN A MANNER THAT WILL PREVENT INJURY, DAMAGE TO STRUCTURES, ADJACENT BUILDINGS AND ALL PERSONS.
- 7) REFRAIN FROM USING EXPLOSIVES WITHOUT PRIOR WRITTEN CONSENT OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES.
- 8) CONDUCT DEMOLITION SERVICES IN SUCH A MANNER TO INSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS AND OTHER ADJACENT FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS OR OTHER OCCUPIED FACILITIES WITHOUT PRIOR WRITTEN PERMISSION OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES. PROVIDE ALTERNATIVE ROUTES AROUND CLOSED OR OBSTRUCTED TRAFFIC WAYS IF REQUIRED BY APPLICABLE GOVERNMENTAL REGULATIONS.
- 9) USE WATERING, TEMPORARY ENCLOSURES AND OTHER SUITABLE METHODS, AS NECESSARY TO LIMIT THE AMOUNT OF DUST AND DIRT RISING AND SCATTERING IN THE AIR. CLEAN ADJACENT STRUCTURES AND IMPROVEMENTS OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. RETURN ALL ADJACENT AREAS TO THE CONDITIONS EXISTING PRIOR TO THE START OF WORK.
- 10) ACCOMPLISH AND PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE UNAUTHORIZED ENTRY OF PERSONS AT ANY TIME.
- 11) COMPLETELY FILL BELOW GRADE AREAS AND VOIDS RESULTING FROM THE DEMOLITION OF STRUCTURES AND FOUNDATIONS WITH SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. STONES USED WILL NOT BE LARGER THAN 6 INCHES IN DIMENSION. MATERIAL FROM DEMOLITION MAY NOT BE USED AS FILL. PRIOR TO PLACEMENT OF FILL MATERIALS, UNDERTAKE ALL NECESSARY ACTION IN ORDER TO INSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROZEN MATERIAL, TRASH, DEBRIS. PLACE FILL MATERIALS LAYERS NOT EXCEEDING 6 INCHES IN LOOSE DEPTH AND COMPACT EACH LAYER AT PLACEMENT TO 95% OPTIMUM DENSITY, GRADE SURFACE TO MEET ADJACENT CONTOURS AND TO PROVIDE SURFACE DRAINAGE.
- 12) REMOVE FROM THE DESIGNATED SITE, AT THE EARLIEST POSSIBLE TIME, ALL DEBRIS, RUBBISH, SALVAGEABLE ITEMS, HAZARDOUS AND COMBUSTIBLE SERVICES. REMOVED MATERIALS MAY NOT BE STORED, SOLD OR BURNED ON SITE. REMOVAL OF HAZARDOUS AND COMBUSTIBLE MATERIALS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROCEDURES AS AUTHORIZED BY THE FIRE DEPARTMENT OR OTHER APPROPRIATE REGULATORY AGENCIES AND DEPARTMENTS.
- 13) DISCONNECT, SHUT OFF AND SEAL ALL UTILITIES SERVING THE STRUCTURE(S) TO BE DEMOLISHED BEFORE THE COMMENCEMENT OF THE DESIGNATED DEMOLITION. MARK FOR POSITION ALL UTILITY DRAINAGE AND SANITARY LINES AND PROTECT ALL ACTIVE LINES. CLEARLY IDENTIFY BEFORE THE COMMENCEMENT OF DEMOLITION SERVICES THE REQUIRED INTERRUPTION OF ACTIVE SYSTEMS THAT MAY AFFECT OTHER PARTIES, AND NOTIFY ALL APPLICABLE UTILITY COMPANIES TO INSURE THE CONTINUATION OF SERVICE.
- 14) PROTECT EXISTING DRAINAGE SYSTEM(S) AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING DURING CONSTRUCTION. SEE DETAIL SHEETS FOR EROSION CONTROL DEVICES.
- 15) ALL WORK WITHIN ROADWAY RIGHT-OF-WAYS TO CONFORM TO TOWN OF BRUNSWICK AND MDOT STANDARDS.
- 16) THE LIMITS OF WORK SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO THE START OF CONSTRUCTION OR SITE CLEARING.
- 17) IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO NOTIFY "DIG SAFE" (1-888-344-7233) 72 HOURS PRIOR TO ANY EXCAVATION ON THIS SITE.
- 18) NOTES ON THIS PLAN THAT READ "TBR" REPRESENT FEATURES TO BE REMOVED. ANY FEATURES NOT LABELED "TBR" OR "TO BE REMOVED" SHALL BE CONSIDERED EXISTING TO REMAIN.

GRAPHIC SCALE

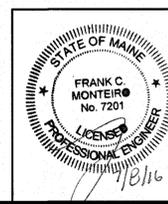


LEGEND

○	UTILITY POLE	—	OVERHEAD SERVICE WIRES
⊙	DRAIN MANHOLE	—	DOUBLE SOLID YELLOW LINE
⊙	SEWER MANHOLE	—	SINGLE SOLID WHITE LINE
⊙	TELEPHONE MANHOLE	—	SIGN
⊙	CATCH BASIN	⊕	OBSERVATION WELL
—	WATER LINE	—	TREELINE
—	WATER VALVE	—	CONTOUR ELEVATION
—	FIRE HYDRANT	—	GAS LINE
—	GAS VALVE	(TBR)	TO BE REMOVED
		—	TO BE REMOVED



PRIOR TO CONSTRUCTION CONTRACTOR TO INSTALL TEMPORARY CONSTRUCTION FENCING AROUND PERIMETER OF SITE



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

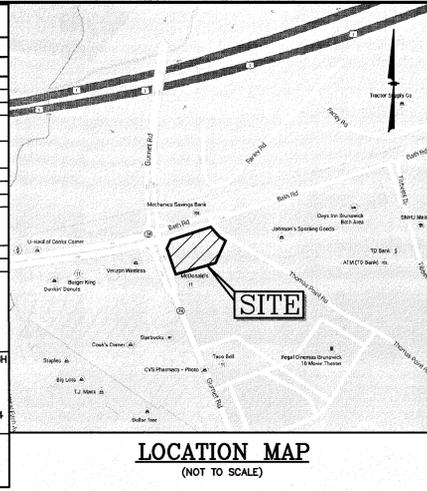
50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

190 BATH ROAD
BRUNSWICK, MAINE 04011
SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM
DEMOLITION PLAN
CFG03.0

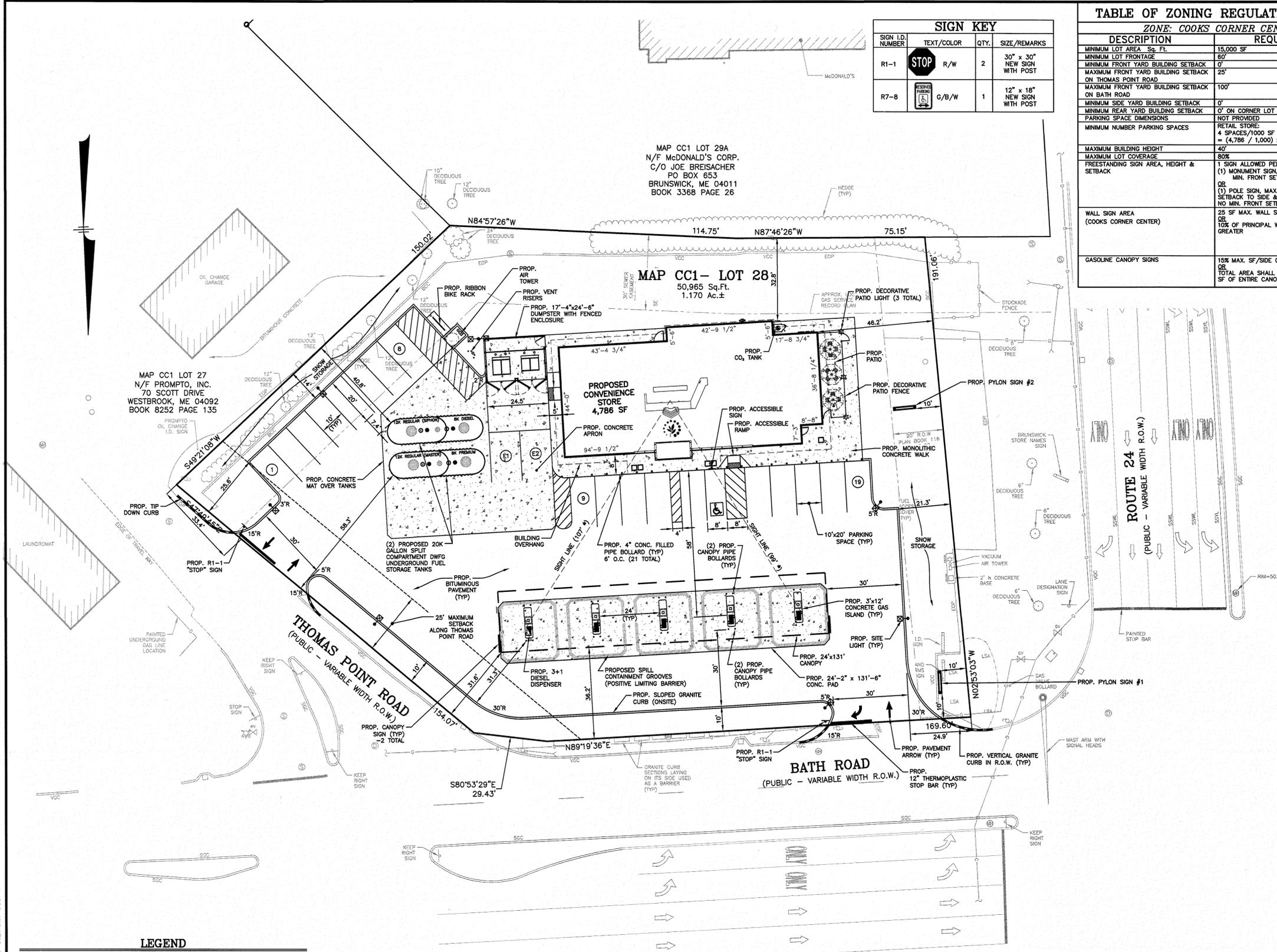
SIGN KEY			
SIGN I.D. NUMBER	TEXT/COLOR	QTY.	SIZE/REMARKS
R1-1	STOP R/W	2	30" x 30" NEW SIGN WITH POST
R7-8	G/B/W	1	12" x 18" NEW SIGN WITH POST

TABLE OF ZONING REGULATIONS - BRUNSWICK, ME		
ZONE: COOKS CORNER CENTER DISTRICT (CCD)		
DESCRIPTION	REQUIRED	PROVIDED
MINIMUM LOT AREA	15,000 SF	50,965 SF
MINIMUM LOT FRONTAGE	60'	189.03' (ALONG BATH ROAD)
MINIMUM FRONT YARD BUILDING SETBACK	0'	31.6' (CANOPY)
MAXIMUM FRONT YARD BUILDING SETBACK ON THOMAS POINT ROAD	25'	MIN. = 31.6' (CANOPY)
MAXIMUM FRONT YARD BUILDING SETBACK ON BATH ROAD	100'	(CANOPY) MIN = 36.2' (C-STORE) MIN = 108.7'
MINIMUM SIDE YARD BUILDING SETBACK	0'	46.2' (BUILDING)
MINIMUM REAR YARD BUILDING SETBACK	0' ON CORNER LOT	32.8' (BUILDING)
PARKING SPACE DIMENSIONS	NOT PROVIDED	10' x 20'
MINIMUM NUMBER PARKING SPACES	4 SPACES/1000 SF FLOOR AREA = (4,786 / 1,000) x 4 = 21 SPACES REQUIRED	21 SPACES (INCLUDES 2 EMPLOYEE SPACES)
MAXIMUM BUILDING HEIGHT	40'	32'-10"
MAXIMUM LOT COVERAGE	80%	35,111 SF ± (68.8%)
FREESTANDING SIGN AREA, HEIGHT & SETBACK	1 SIGN ALLOWED PER 250 LF OF LOT FRONTAGE: (1) MONUMENT SIGN, MAX. 32 SF, 10' HIGH, NO MIN. FRONT SETBACK OR (1) POLE SIGN, MAX. 25 SF, 15' HIGH, 5' SETBACK TO SIDE & REAR PROPERTY LINES & NO MIN. FRONT SETBACK	54416 LF FRONTAGE (2) 10' HIGH, 32 SF MONUMENT SIGNS PROPOSED
WALL SIGN AREA (COOKS CORNER CENTER)	25 SF MAX. WALL SIGNAGE OR 10% OF PRINCIPAL WALL FACADE, WHICHEVER IS GREATER	103.5' WALL FACADE x 13'-10" HIGH = 1,431 SF 10% = 143 SF ALLOWED (1) 37.8 SF SIGN FACING BATH ROAD (1) 24.5 SF SIGN FACING ROUTE 24 TOTAL WALL SIGNS = 62.1 SF
GASOLINE CANOPY SIGNS	15% MAX. SF/SIDE OF CANOPY OR TOTAL AREA SHALL NOT EXCEED 8% OF TOTAL SF OF ENTIRE CANOPY	24' WIDE CANOPY x 3'-8" HIGH = 88 SF 15% = 13.2 SF ALLOWED/SIDE (3) 11 SF SIGNS PROPOSED



MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1 - LOT 28
50,965 Sq.Ft.
1.170 Ac.±

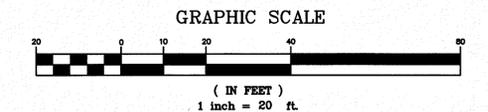


NOTES:

- THE BOUNDARY INFORMATION SHOWN HEREON FOR MAP CC1 LOT 28 IS THE RESULT OF A STANDARD BOUNDARY SURVEY BY THIS OFFICE.
- ZONING DISTRICT: COOKS CORNER CENTER DISTRICT (CCD)
- LOT AREA = 50,965 Sq.Ft.± = 1.170 Ac.±
- EXISTING USE: RETAIL MOTOR FUEL OUTLET WHICH INCLUDES A 6,883 SF CONVENIENCE STORE, A FUEL DISPENSING AREA WITH 4 DISPENSERS (8 FUELING POSITIONS), AN OVERHEAD CANOPY, AN AT&T RETAIL STORE AND A PAPA JOHN'S PIZZA.
PROPOSED USE: RETAIL MOTOR FUEL OUTLET WHICH INCLUDES A 4,786 SF CONVENIENCE STORE AND A FUEL DISPENSING AREA WITH 5 DISPENSERS (10 FUELING POSITIONS) AND AN OVERHEAD CANOPY.
- ALL BUILDINGS AND SITE CONSTRUCTION SHALL COMPLY WITH THE RULES AND REGULATIONS OF THE AMERICANS WITH DISABILITIES ACT (ADA) AS PUBLISHED IN THE FEDERAL REGISTER, VOL. 56, NO. 144, DATED JULY 26, 1991, REVISED JULY 23, 2004.
- THE LOCATIONS OF EXISTING SUBSURFACE UTILITIES SHOWN ON THIS PLAN WERE COMPILED FROM AVAILABLE RECORD DRAWINGS AND ARE NOT WARRANTED TO BE CORRECT. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING SUBSURFACE UTILITIES PRIOR TO PERFORMING ANY WORK.
- WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIGSAFE 1-888-344-7233 PRIOR TO ANY EXCAVATION.
- ALL CONSTRUCTION SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF BRUNSWICK AND THE STATE OF MAINE.
- THE SURVEY TRACT IS LOCATED IN ZONE "X", AREA OF MINIMAL FLOODING, AS SHOWN ON THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL #230042 0015 B FOR THE TOWN OF BRUNSWICK, MAINE, CUMBERLAND COUNTY. EFFECTIVE DATE: JANUARY 3, 1986.
- ALL CONSTRUCTION SHALL CONFORM TO THESE PLANS AND THE STANDARD CONSTRUCTION DRAWINGS AS SUPPLIED BY CUMBERLAND FARMS.
- A SIGN PERMIT SHALL BE OBTAINED PRIOR TO INSTALLATION. SIGNAGE SHALL COMPLY WITH THE LAND USE CODE.
- THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THESE PLANS, PREPARED BY MHF DESIGN CONSULTANTS, INC. DO NOT EXTEND TO OR INCLUDE SYSTEMS PERTAINING TO THE PERFORMANCE OF THE WORK. THE SEAL OF THE SURVEYOR AND/OR ENGINEER AS INCLUDED IN THE PLAN SET DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONTRACTOR SHALL PREPARE AND/OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND/OR LOCAL REGULATIONS.
- ALL UNDERGROUND STORAGE TANKS, PRODUCT PIPING AND VENT LINES SHALL COMPLY WITH CURRENT STATE AND E.P.A. REGULATIONS.
- ALL SNOW SHALL BE STORED IN THE AREA(S) DEPICTED ON THIS PLAN AS SNOW STORAGE AREAS. IN THE EVENT THAT THE AREA(S) APPROVED FOR SNOW STORAGE BECOME FULL, THE OWNER SHALL REASONABLY REMOVE EXCESS SNOW FROM THE SITE, AND SHALL NOT ALLOW SNOW TO BE STORED WITHIN PARKING LOTS OR TRAVEL AISLES.
- ALL WASTE MATERIALS AND RECYCLABLES SHALL BE CONTAINED WITHIN THE BUILDING(S) OR APPROVED STORAGE FACILITIES AND SHALL NOT BE OTHERWISE STORED ON PROPERTY.
- EXISTING IMPERVIOUS COVERAGE = 46,521 SF ± (91.3%)
PROPOSED IMPERVIOUS COVERAGE = 35,149 SF ± (68.0%)

LEGEND

○ IRON PIN FOUND	○ TREELINE
□ CONCRETE BOUND FOUND	○ UTILITY POLE
△ RAILROAD SPIKE FOUND	○ DRAIN MANHOLE
○ DRILL HOLE FOUND	○ SEWER MANHOLE
— VGC	○ TELEPHONE MANHOLE
— VCG	□ CATCH BASIN
— VCB	— WATER LINE
— VCB	— WATER VALVE
— VCB	— FIRE HYDRANT
— VCB	— GAS VALVE
— VCB	— GAS LINE
— VCB	— UNDERGROUND TELEPHONE LINE
— VCB	— UNDERGROUND ELECTRIC AND TELEPHONE
— VCB	— SIGN
— VCB	— OBSERVATION WELL
— VCB	— TEST PIT
— VCB	— TEST BORING



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

Cumberland FARMS
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

**190 BATH ROAD
BRUNSWICK, MAINE 04011**

SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM

SITE PLAN
CFG04.0

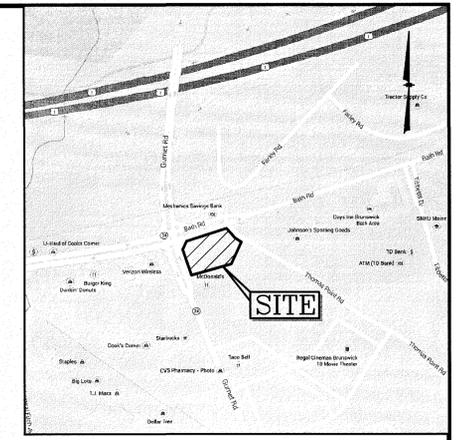
F:\Projects\CAD\375115\3751SP.dwg SP 7/08/16 9:27am ecc

FROM STRUCTURE NUMBER	PIPE SIZE (inches)	TYPE OF PIPE	APPROX. PIPE LENGTH (feet)	SLOPE OF PIPE (ft./ft.)	TO STRUCTURE NUMBER
CB-2	12	HDPE	58	0.005	CB-1
CB-1	12	HDPE	68	0.005	DMH-1
CB-3	12	HDPE	65	0.008	DMH-1
CB-5	12	HDPE	58	0.011	DMH-2
DMH-2	12	HDPE	90	0.007	CB-4
CB-4	12	HDPE	45	0.007	EX. CB-B
DMH-1	12	HDPE	15	0.005	EX. CB-A

DRAINAGE STRUCTURES

CB-1* RIM=49.30 INV.IN=46.60 INV.OUT=46.50	DMH-1 (FD) RIM=50.20 INV.IN=46.67 INV.S IN=46.17 (CB'S) INV.OUT=46.17
CB-2 RIM=49.85 INV.IN=47.35(RD) INV.OUT=46.85	DMH-2 RIM=51.20 INV.IN=46.70 INV.OUT=46.80
CB-3 RIM=49.70 INV.IN=46.70	
CB-4 (FD) RIM=49.00 INV.IN=45.00 INV.OUT=45.90	
CB-5 RIM=50.70 INV.IN=47.70(RD) INV.OUT=47.20	

FD = FIRST DEFENSE UNIT
* = LOW PROFILE FRAME GRATE & TOP SLAB



LOCATION MAP
(NOT TO SCALE)

NOTES:

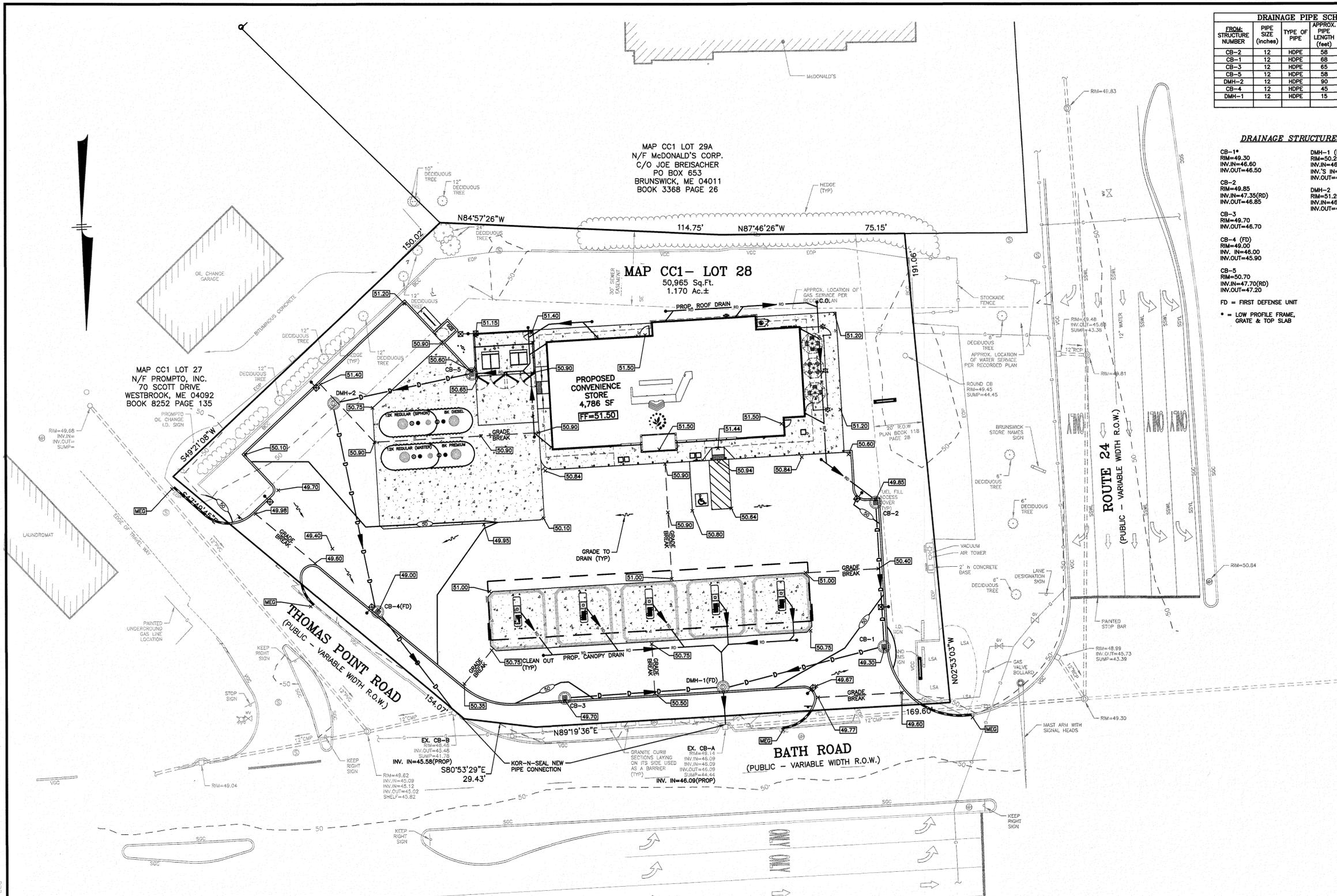
- ALL SITE DRAINAGE PIPE SHALL BE CORRUGATED HIGH-DENSITY POLYETHYLENE PIPE WITH STANDARD JOINTS, DUAL-WALL, SMOOTH INTERIOR, AS MANUFACTURED BY ADS, INC., OR APPROVED EQUAL, UNLESS OTHERWISE NOTED ON PLAN.
- ALL ROOF AND CANOPY DRAIN PIPE SHALL BE 6" PVC (SDR-35).
- ELEVATIONS ARE BASED ON NAVD 1988 DATUM.
- ALL PROPOSED ELEVATIONS AS SHOWN ARE BOTTOM OF CURB ELEVATIONS, UNLESS OTHERWISE NOTED.
- ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER OF ANY DISCREPANCIES. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND INSTALLATIONS SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (811) PRIOR TO COMMENCING ANY EXCAVATION.
- ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO MDT AND THE TOWN OF BRUNSWICK, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- ALL ADA ACCESSIBLE WALKWAYS CANNOT EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE. RAMPS CANNOT EXCEED 8.33% RUNNING SLOPE AND 2% CROSS SLOPE, AND HC PARKING STALLS AND ACCESS AISLES CANNOT EXCEED 2% SLOPE IN ANY DIRECTION. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.
- SEE UTILITY PLAN FOR DETAILED UTILITY LAYOUT.
- CONTRACTOR IS TO FIELD ADJUST GRADES FOR THE TANK PAD TO SHED WATER.
- CONTRACTOR TO INCORPORATE RECOMMENDATIONS MADE IN THE GEOTECHNICAL ENGINEERING EXPLORATION EVALUATION AS PREPARED BY ENVIRONMENTAL COMPLIANCE SERVICES, INC. (ECS) DATED JULY 16, 2015.

MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1 - LOT 28
50,965 Sq.Ft.
1.170 Ac.±

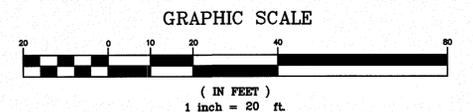
PROPOSED CONVENIENCE STORE
4,786 SF
FF=51.50

MAP CC1 LOT 27
N/F PROMPTO, INC.
70 SCOTT DRIVE
WESTBROOK, ME 04092
BOOK 8252 PAGE 135

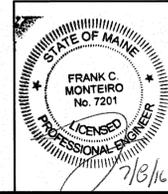


LEGEND

- | | | | |
|---|---------------------------------|---|------------------------------------|
| ○ | IRON PIN FOUND | — | TREELINE |
| □ | CONCRETE BOUND FOUND | — | UTILITY POLE |
| △ | RAILROAD SPIKE FOUND | — | DRAIN MANHOLE |
| ⊙ | DRILL HOLE FOUND | — | SEWER MANHOLE |
| — | VERTICAL GRANITE CURB | — | TELEPHONE MANHOLE |
| — | SLOPED GRANITE CURB | — | CATCH BASIN |
| — | BITUMINOUS CONCRETE LIP CURBING | — | WATER LINE |
| — | BITUMINOUS CONCRETE BERM | — | WATER VALVE |
| — | OVERHEAD SERVICE WIRES | — | FIRE HYDRANT |
| — | DOUBLE SOLID YELLOW LINE | — | GAS VALVE |
| — | SINGLE SOLID WHITE LINE | — | GAS LINE |
| — | BROKEN WHITE LINE | — | UNDERGROUND TELEPHONE LINE |
| — | SIGN | — | UNDERGROUND ELECTRIC AND TELEPHONE |
| — | OBSERVATION WELL | — | WETLAND LINE |
| — | TEST PIT | — | SPOT ELEVATION |
| — | TEST BORING | — | CONTOUR ELEVATION |



MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com



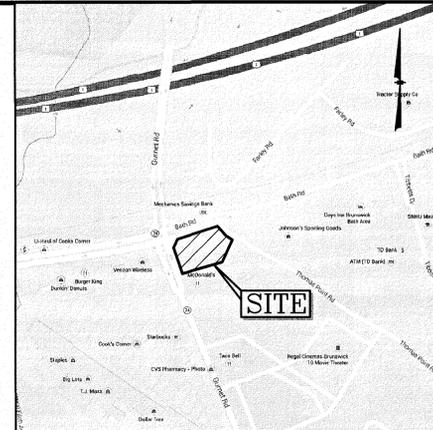
REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

190 BATH ROAD
BRUNSWICK, MAINE 04011
SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM
GRADING & DRAINAGE PLAN
CFG05.0

CONSTRUCTION SEQUENCE:

- CUT AND STUMP AREAS OF PROPOSED CONSTRUCTION.
- INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS REQUIRED. CONSTRUCT CONSTRUCTION ENTRANCE AS SHOWN ON PLAN.
- REMOVE AND STOCKPILE TOPSOIL. STOCKPILE SHALL BE SEED TO PREVENT EROSION. THE DOWN GRADIENT PORTION OF THE STOCKPILE SHALL BE ENCIRCLED WITH EITHER SILT FENCE OR A STRAW OR HAYBALE BARRIER INSTALLED ACCORDING TO DETAILS SHOWN ON THIS PLAN.
- CONSTRUCT DETENTION BASIN AND CLOSED DRAINAGE SYSTEM. PROTECT CULVERT INLETS AND CATCH BASINS WITH SEDIMENTATION BARRIERS.
- PERFORM SITE GRADING, PLACING HAY BALES AND SILTATION FENCES AS REQUIRED TO CONTROL SOIL EROSION.
- INSTALL UNDERGROUND UTILITIES.
- BEGIN TEMPORARY AND PERMANENT SEEDING AND MULCHING. ALL CUT AND FILL SLOPES SHALL BE SEED OR MULCHED IMMEDIATELY AFTER THEIR CONSTRUCTION.
- DAILY, OR AS REQUIRED, CONSTRUCT, INSPECT, AND IF NECESSARY, RECONSTRUCT TEMPORARY BERRIS, GRASS, DITCHES, SILT FENCES AND SEDIMENT TRAPS INCLUDING MULCHING AND SEEDING. SEE LANDSCAPE PLAN FOR MULCHING SPECIFICATIONS.
- BEGIN EXCAVATION FOR AND CONSTRUCTION OF BUILDINGS.
- FINISH PAVING ALL DRIVES AND PARKING AREAS.
- COMPLETE PERMANENT SEEDING AND LANDSCAPING.
- NO FLOW SHALL BE DIVERTED TO ANY WETLANDS UNTIL A HEALTHY STAND OF GRASS HAS BEEN ESTABLISHED IN REGRADED AREAS.
- AFTER GRASS HAS BEEN FULLY GERMINATED IN ALL SEED AREAS, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.

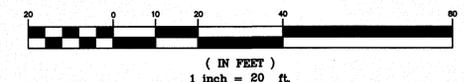


LOCATION MAP
(NOT TO SCALE)

EROSION CONTROL MEASURES & MAINTENANCE

- DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED:
- INSTALLATION OF STONE CHECK DAMS, SILTATION FENCES AND ORANGE CONSTRUCTION FENCE SHALL BE COMPLETED PRIOR TO THE START OF THE SITE WORK IN ANY GIVEN AREA. PREFABRICATED SILTATION FENCES SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
- STONE CHECK DAMS, SILTATION FENCES, AND ORANGE CONSTRUCTION FENCE SHALL BE KEPT CLEAN DURING CONSTRUCTION AND REMOVED WHEN ALL SLOPES HAVE A HEALTHY STAND OF VEGETATIVE COVER. EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EVERY 0.5" OF RAINFALL.
- EXISTING VEGETATION IS TO REMAIN UNDISTURBED WHEREVER POSSIBLE.
- THE AREA OF LAND EXPOSED AND THE TIME OF EXPOSURE SHALL BE MINIMIZED. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 72 HOURS AFTER FINAL GRADING OR OTHER TEMPORARY MEASURES OR TEMPORARY MEASURES. IN NO CASE SHALL ANY DISTURBED AREAS BE LEFT UNSTABILIZED WITH EITHER PERMANENT OR TEMPORARY EROSION CONTROL MEASURES FOR MORE THAN 72 HOURS, OR AS NEEDED TO ENSURE SUFFICIENT STABILIZATION DUE TO WEATHER OR OTHER CONDITIONS. SIMULTANEOUS WORK IN MULTIPLE AREAS MAY BE PERMITTED AS NEEDED, SUBJECT TO THE ABOVE. HOWEVER THE CONTRACTOR SHALL NOT DISTURB AREAS THAT CANNOT REASONABLY BE PROPERLY STABILIZED AND MAINTAINED WITHIN 72 HOURS.
- ALL DISTURBED AREAS SHALL HAVE A MINIMUM OF 6" OF LOAM INSTALLED WITH MIX. SEED MIXTURE SHALL BE:
 - A. SLOPE AND DETENTION MIX - 1.5LBS/1000SF (TYPE 2):
 - PERENNIAL SWEET PEA - 20%
 - WILD LUPINE - 15%
 - SNOWY TACK TREFLOID - 15%
 - WHITE CLOVER - 15%
 - B. MIX LAWN MIX - 3LBS/1000SF (TYPE 1):
 - PERENNIAL RYEGRASS - 10%
 - ANNUAL PERGRASS - 10%
 - KENTUCKY BLUEGRASS - 25%
 - CREeping RED FESCUE - 45%
 - CHEWINGS RYEGRASS - 15%
- LIME AND FERTILIZER SHALL BE INCORPORATED INTO THE SOIL PRIOR TO OR AT THE TIME OF SEEDING. A MINIMUM OF 2 TONS PER ACRE OF AGRICULTURAL LIMESTONE AND 300 LBS. PER ACRE OF 10-10-10 FERTILIZER SHALL BE APPLIED. SEEDING PRACTICES SHALL COMPLY WITH LOCAL USDA SOIL CONSERVATION SERVICES RECOMMENDATIONS.
- HAY MULCH AND SLOPE PROTECTION BLANKET SHALL BE USED WHERE INDICATED ON THE PLANS. A MINIMUM OF 1.5 TONS OF MULCH PER ACRE SHALL BE APPLIED. MULCH SHALL BE ANCHORED IN PLACE WHERE NECESSARY. SLOPE PROTECTION BLANKET SHALL BE LAID IN THE DIRECTION OF RUNOFF FLOW AND APPLIED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS AND/OR THE DETAILS.
- PERMANENT OR TEMPORARY COVER MUST BE IN PLACE BEFORE THE GROWING SEASON ENDS. WHEN SEEDING AREAS ARE MULCHED, PLANTINGS MAY BE MADE FROM EARLY SPRING TO EARLY OCTOBER. WHEN SEEDING AREAS ARE NOT MULCHED, PLANTINGS SHOULD BE MADE FROM APRIL 15 TO MAY 20 OR FROM AUGUST 15 TO SEPTEMBER 15. NO DISTURBED AREA SHALL BE LEFT EXPOSED DURING WINTER MONTHS.
- PRIOR TO CONSTRUCTION OF IMPERVIOUS AREAS, ALL DRAINAGE STRUCTURES AND PIPES SHALL BE INSTALLED AND INSPECTED FOR PROPER FUNCTION. DURING CONSTRUCTION OF OTHER SITE FEATURES, ALL DRAINAGE FACILITIES SHALL BE INSPECTED ON A DAILY BASIS AND CLEANED/PREPARED IMMEDIATELY UPON DISCOVERY OF SEDIMENT BUILD-UP OR DAMAGE.
- EROSION CONTROLS ARE TO BE INSPECTED AND MAINTAINED ON A DAILY BASIS. DISCOVERY OF SILT BUILD-UP IN CATCH BASIN SUMPS OR ANY OTHER STRUCTURE SHALL REQUIRE CLEANING.
- ALL EXPOSED SOILS SHALL BE IMMEDIATELY STABILIZED WITH A LAYER OF MULCH HAY.
- UPON INSTALLATION OF CATCH BASINS, INLET PROTECTION - AS DESCRIBED ON AFOREMENTIONED PLAN - SHALL BE INSTALLED AND MAINTAINED UNTIL READY FOR PAVING.
- CALCIUM CHLORIDE AND/OR WATER SHALL BE USED FOR DUST CONTROL IN APPROPRIATE AREAS.
- IF, DURING CONSTRUCTION, IT COMES APPARENT THAT ADDITIONAL EROSION CONTROL MEASURES ARE REQUIRED TO STOP ANY EROSION ON THE CONSTRUCTION SITE DUE TO ACTUAL SITE CONDITIONS, THE CONTRACTOR SHALL BE REQUIRED TO INSTALL THE NECESSARY EROSION PROTECTION AT NO EXPENSE TO THE CITY OR OWNER.
- OVER WINTER STABILIZATION (IF NEEDED):
 - A. PERMANENT STABILIZATION CONSISTS OF AT LEAST 85% VEGETATION, PAVEMENT/GRAVEL BASE OR RIPRAP
 - B. DO NOT EXPOSE SLOPES OR LEAVE SLOPES EXPOSED OVER THE WINTER OR FOR ANY OTHER EXTENDED TIME OF WORK SUSPENSION UNLESS FULLY PROTECTED WITH MULCH.
 - C. APPLY HAY MULCH AT TWICE THE STANDARD RATE (150 LBS. PER 1,000 SF). THE MULCH MUST BE THICK ENOUGH SUCH THAT THE GROUND SURFACE WILL NOT BE VISIBLE AND MUST BE ANCHORED.
 - D. USE MULCH AND MULCH NETTING OR AN EROSION CONTROL BLANKET OR MIX FOR ALL SLOPES GREATER THAN 5% OR OTHER AREAS EXPOSED TO DIRECT WIND.
 - E. INSTALL EROSION CONTROL BLANKETS IN ALL DRAINAGE WAYS (BOTTOM AND SIDES) WITH A SLOPE GREATER THAN 3%.
 - F. SEE THE VEGETATION MEASURES FOR MORE INFORMATION ON SEEDING DATES AND TYPES.
- WINTER CONSTRUCTION (IF NEEDED):
 - A. WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED AS SUCH NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME.
 - B. AN AREA WITHIN 100 FEET OF A PROTECTED NATURAL RESOURCE MUST BE PROTECTED WITH A DOUBLE ROW OF SEDIMENT BARRIER.
 - C. TEMPORARY MULCH MUST BE APPLIED WITHIN 7 DAYS OF SOIL EXPOSE OR PRIOR TO ANY STORM EVENT, BUT AFTER EVERY WORKDAY IN AREAS WITHIN 100 FEET FROM A PROTECTED NATURAL RESOURCE.
 - D. AREAS THAT HAVE BEEN BROUGHT TO FINAL GRADE MUST BE PERMANENTLY MULCHED THE SAME DAY.
 - E. IN THE EVENT OF A SNOWFALL GREATER THAN 1 INCH (FRESH OR CUMULATIVE), THE SNOW SHALL BE REMOVED FROM AREAS DUE TO BE SEEDING AND MULCHED.
 - F. LOAM SHALL BE FREE OF FROZEN CLUMPS BEFORE IT IS APPLIED.
 - G. A DITCH THAT WILL BE CONSTRUCTED DURING THE WINTER MUST BE STABILIZED WITH RIPRAP.
 - H. ALL DITCHES OR SWALES WHICH DO NOT SHOW A MIN. OF 80% VEGETATION BY OCT. 15 OR WHICH ARE DISTURBED AFTER OCT. 15 SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW.
 - I. AFTER NOV. 15 INCOMPLETE ROAD OR PARKING SURFACES WHERE WORK HAS STOPPED FOR THE WINTER SEASON SHALL BE PROTECTED WITH A MINIMUM OF 3" OF CRUSHED GRAVEL PER FOOT.

GRAPHIC SCALE



LEGEND

○	IRON PIN FOUND	—	TREELINE
□	CONCRETE BOUND FOUND	—	UTILITY POLE
△	RAILROAD SPIKE FOUND	—	DRAIN MANHOLE
○	DRILL HOLE FOUND	—	SEWER MANHOLE
—	VERTICAL GRANITE CURB	—	TELEPHONE MANHOLE
—	SLOPED GRANITE CURB	—	CATCH BASIN
—	BITUMINOUS CONCRETE LIP CURBING	—	WATER LINE
—	BITUMINOUS CONCRETE BERM	—	WATER VALVE
—	OVERHEAD SERVICE WIRES	—	FIRE HYDRANT
—	DOUBLE SOLID YELLOW LINE	—	GAS VALVE
—	SINGLE SOLID WHITE LINE	—	GAS LINE
—	BROKEN WHITE LINE	—	UNDERGROUND TELEPHONE LINE
—	SIGN	—	UNDERGROUND ELECTRIC AND TELEPHONE
—	OBSERVATION WELL	—	WETLAND LINE
—	TEST PIT	—	SPOT ELEVATION
—	TEST BORING	—	CONTOUR ELEVATION



MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 28

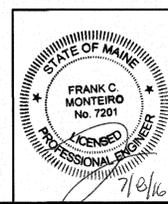
MAP CC1 - LOT 28
50,965 Sq.Ft.
1.170 Ac.±

PROPOSED CONVENIENCE STORE
4,786 SF
FF=51.50

THOMAS POINT ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

BATH ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

ROUTE 24
(PUBLIC - VARIABLE WIDTH R.O.W.)



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

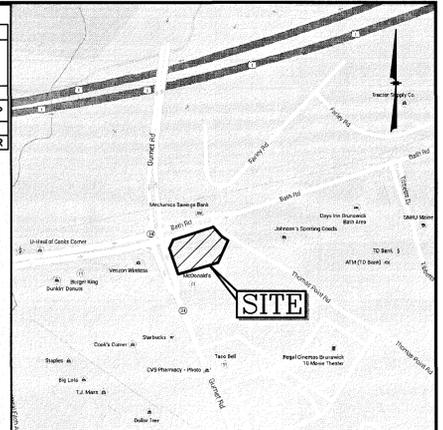
50,965 FEET	<p>MHF Design Consultants, Inc. 190 BATH ROAD BRUNSWICK, MAINE 04011</p>	SCALE: 1" = 20'
1.170 ACRES		DATE: JULY 11, 2016
V# L0589		FILE: 3751SP.dwg
Store# 5585		DRAWN BY: CMT
Gas Station# 1818	<p>CUMBERLAND FARMS INC. 100 CROSSING BLVD. FRAMINGHAM, MA 01702</p>	CHECKED BY: FCM
EROSION & SEDIMENTATION CONTROL PLAN		CFG06.0

FROM: STRUCTURE NUMBER	PIPE SIZE (inches)	TYPE OF PIPE	APPROX. PIPE LENGTH (feet)	SLOPE OF PIPE (ft./ft.)	TO: STRUCTURE NUMBER
BLDG. SEWER TRAP	6	SDR 35 PVC	42	0.020	WYE #1
GR. TRAP	6	SDR 35 PVC	8	0.020	WYE #1
WYE #1	6	SDR 35 PVC	41	0.1540	EXIST. SEWER

SEWER STRUCTURES

PROPOSED 1,500 GALLON GREASE TRAP
 RIM=51.00
 INV.IN=47.50
 INV.OUT=47.25

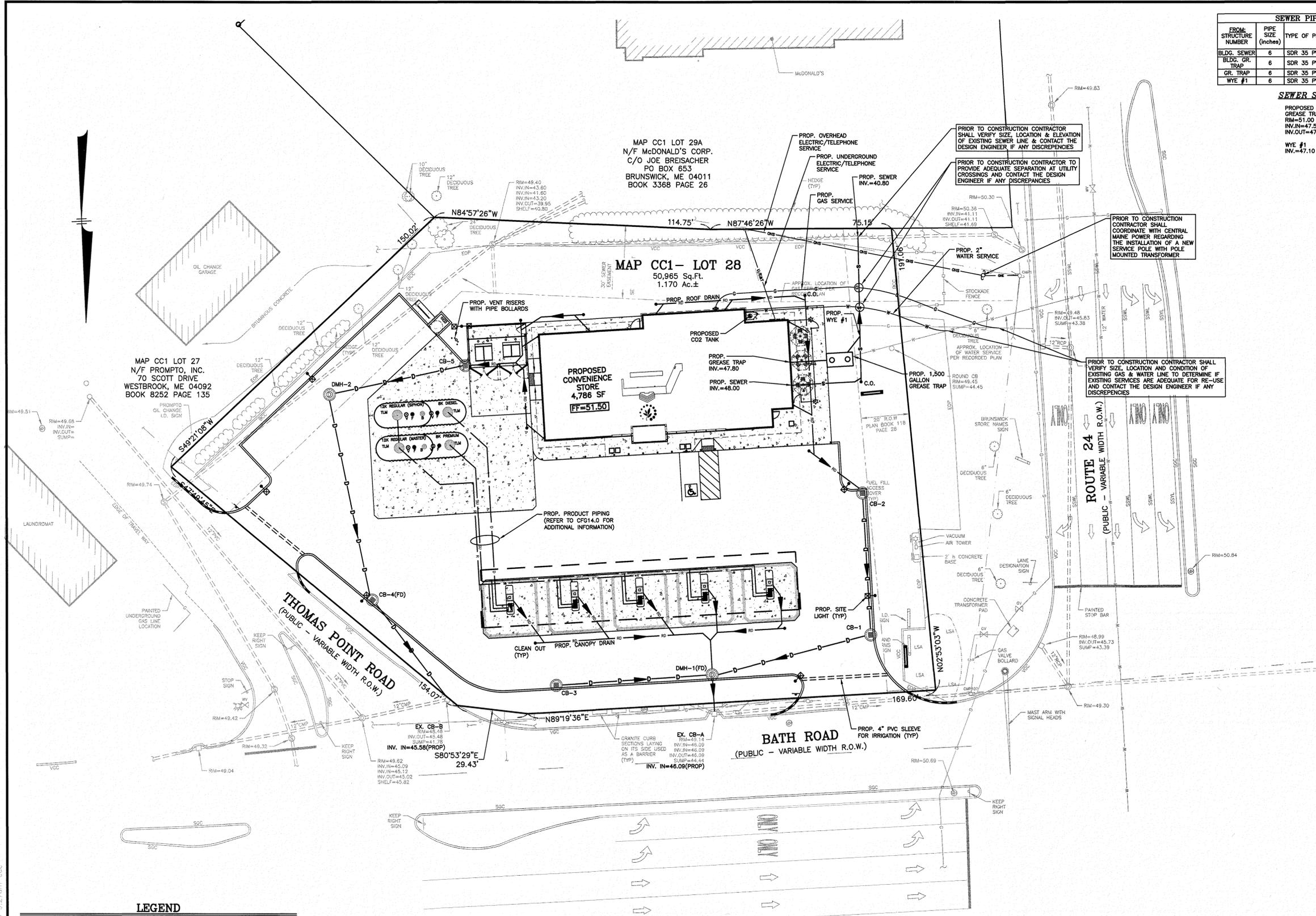
WYE #1
 INV.=47.10



LOCATION MAP
(NOT TO SCALE)

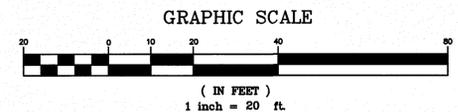
NOTES:

- 1) ALL SANITARY SEWER PIPE SHALL BE PVC (SDR-35), UNLESS OTHERWISE NOTED.
- 2) ALL WATER PIPE SHALL BE COPPER (TYPE K), UNLESS OTHERWISE NOTED.
- 3) ELEVATIONS ARE BASED ON NAVD 1988 DATUM.
- 4) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- 5) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER IF ANY DISCREPANCIES.
- 6) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- 7) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
- 8) ALL WATER, SEWER AND DRAINAGE CONSTRUCTION SHALL CONFORM TO THE TOWN OF BRUNSWICK STANDARDS AND SPECIFICATIONS.
- 9) ALL ELECTRIC, TELEPHONE AND CABLE TV LINES ARE TO BE UNDERGROUND AND INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY CO. SPECIFICATIONS.
- 10) ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY UTILITY COMPANY AND LOCAL DPW.
- 11) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO MAINE DOT AND BRUNSWICK DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- 12) SEE GRADING & DRAINAGE PLAN FOR DETAILED DRAINAGE INFORMATION.
- 13) ELECTRICAL CONDUIT WITHIN 20' OF TANKS OR DISPENSERS MAY NEED TO BE RIGID METAL CONDUIT WITH CONCRETE ENCASUREMENT. CONTRACTOR TO COORDINATE WITH UTILITY COMPANY AND/OR TOWN ELECTRICAL INSPECTOR AS REQUIRED.



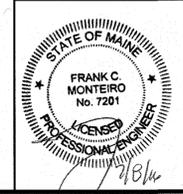
LEGEND

○	IRON PIN FOUND	—	TREELINE
□	CONCRETE BOUND FOUND	—	UTILITY POLE
△	RAILROAD SPIKE FOUND	—	DRAIN MANHOLE
○	DRILL HOLE FOUND	—	SEWER MANHOLE
—	VERTICAL GRANITE CURB	—	TELEPHONE MANHOLE
—	SLOPED GRANITE CURB	—	CATCH BASIN
—	BITUMINOUS CONCRETE LIP CURBING	—	WATER LINE
—	BITUMINOUS CONCRETE BERM	—	WATER VALVE
—	OVERHEAD SERVICE WIRES	—	FIRE HYDRANT
—	DOUBLE SOLID YELLOW LINE	—	GAS VALVE
—	SINGLE SOLID WHITE LINE	—	GAS LINE
—	BROKEN WHITE LINE	—	UNDERGROUND TELEPHONE LINE
—	SIGN	—	UNDERGROUND ELECTRIC AND TELEPHONE
—	OBSERVATION WELL		
—	TEST PIT		
—	TEST BORING		



MHF Design Consultants, Inc.

44 Stiles Road, Suite One
 Salem, New Hampshire 03079
 (603) 893-0720
 ENGINEERS • PLANNERS • SURVEYORS
 www.mhfdesign.com



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
 1.170 ACRES
 V# L0589
 Store# 5585
 Gas Station# 1818

190 BATH ROAD
 BRUNSWICK, MAINE 04011

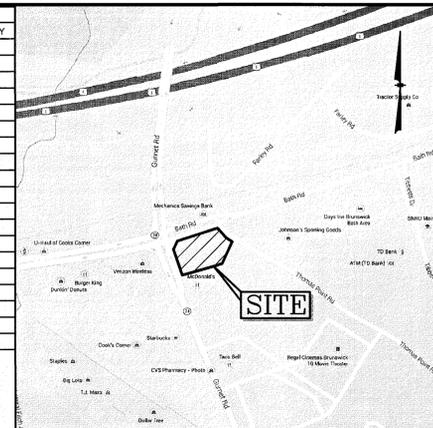
CUMBERLAND FARMS INC.
 100 CROSSING BLVD.
 FRAMINGHAM, MA 01702

SCALE: 1" = 20'
 DATE: JULY 11, 2016
 FILE: 3751SP.dwg
 DRAWN BY: CMT
 CHECKED BY: FCM

UTILITY PLAN
 CFG07.0

F:\Projects\CAD\375115\3751SP.dwg UT 7/08/16 9:27am ccc

Plant Schedule					
SYMB.	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS	QUANTITY
Shrubs					
CNP	Chamaecyparis nootkanensis "Pendula"	Weeping Alaskan cedar	6' - 8' ht	B & B	4
SBG	Spiraea bumaldii "Goldmound"	Goldmound Spirea	18" - 24" spr	3 gal.	29
TMD	Taxus media "Densiflora"	Dense Spreading Yew	24" - 30" ht	B & B or 5 gal.	4
VFS	Viburnum plicatum tomentosum "Shasta"	Shasta Doublefile Viburnum	24" - 30" ht	B & B or 5 gal.	10
Shade & Ornamental Trees					
MLB	Magnolia stellata loebneri "Ballerina"	Ballerina Star Magnolia	2.5' cal.	B & B	3
MSS	Malus var. "Spring Snow"	Spring Snow Crabapple	2.5' cal.	B & B fullleaf	6
SRI	Syringa reticulata "Ivory Silk"	Ivory Silk Japanese Tree Lilac	3" cal.	B & B	3
ZSV	Zelkova serrata "Village Green"	Village Green Zelkova	3" cal.	B & B	1
Ornamental Grasses & Perennials					
EP	Echinacea purpurea	Purple Cone Flower	1 gal	24" o.c.	10
FEB	Felucca orene "Elijah Blue"	Elijah Blue Fescue	1 gal	12" o.c.	150
HHR	Hemerocallis "Happy Returns"	Happy Returns daylily	1 gal	18" o.c.	175
PAH	Perennisetum alopecuroides "Hamelii"	Dwarf Fountain Grass	1 gal	18" o.c.	100
RFG	Rudbeckia fulgida "Little Gold Star"	Little Gold Star Black-eyed Susan	1 gal	23" o.c.	40



LOCATION MAP
(NOT TO SCALE)

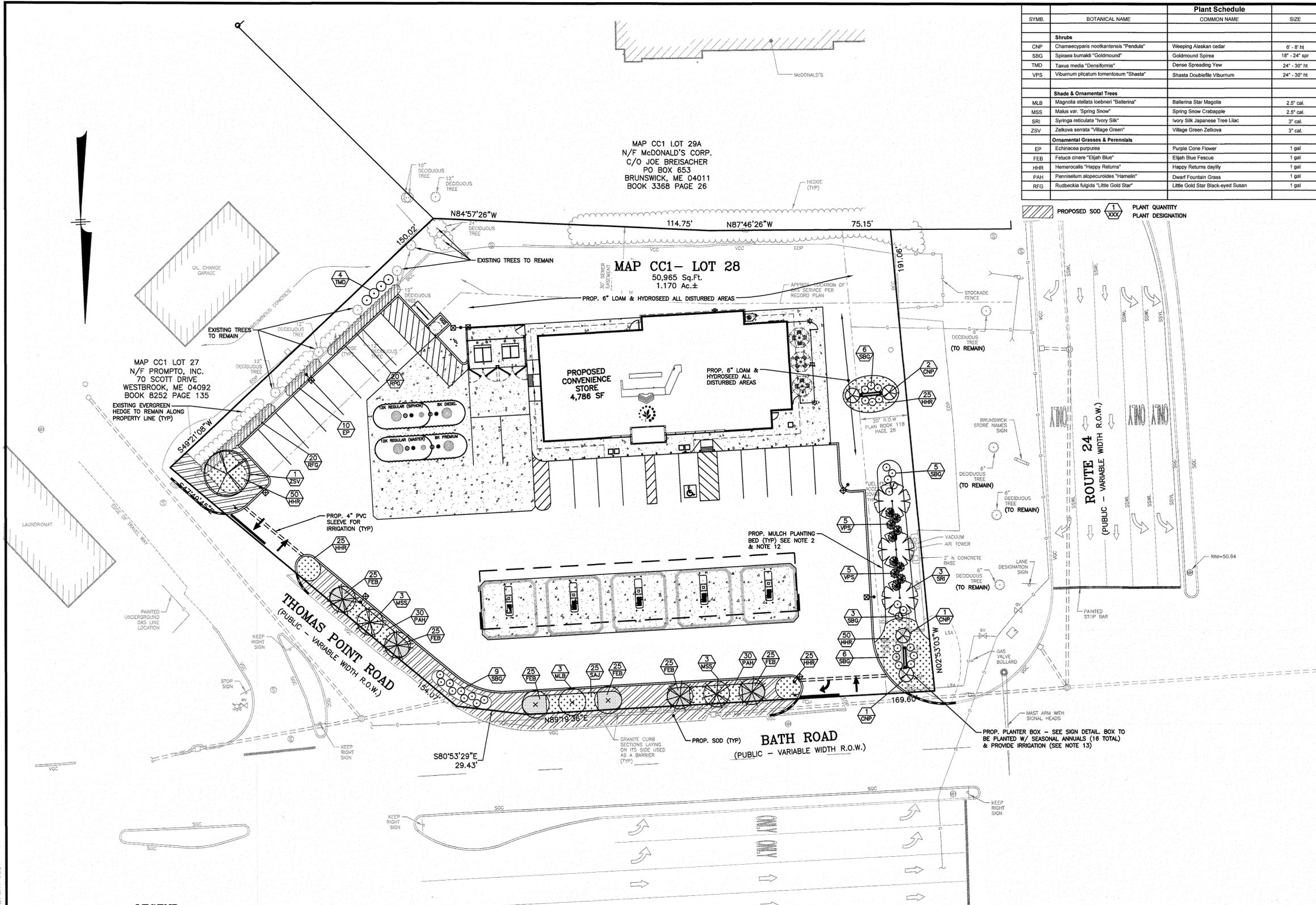
NOTES:

- ALL PLANT STOCK SHALL CONFORM TO ANSI Z260.1 - NURSERY STOCK, LATEST EDITION (AMERICAN ASSOCIATION OF NURSERYMEN, INC.).
- 4" AGED PINEBARK MULCH AND A WEED BARRIER (TY-PAR FABRIC OR APPROVED EQUAL) SHALL BE APPLIED TO ALL SHRUB AND GROUND COVER BEDS. INSTALL WEED BARRIER AS PER MANUFACTURERS RECOMMENDATIONS.
- PLANT PIT BACKFILL SHALL BE MIXED AT A RATE OF 7 PARTS OF TOPSOIL TO 2 PART OF DEHYDRATED COW MANURE. SLOW RELEASE FERTILIZER SHALL BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS. USE EXISTING ON-SITE TOPSOIL AS PART OF BACKFILL WHEN AVAILABLE.
- ALL LANDSCAPED AREAS NOT PLANTED WITH TREES, SHRUBS OR GROUND COVER SHALL BE RESTORED WITH SEED OR SOD AS INDICATED ON PLANS.
- ALL SOD, SEED, SHRUB AND TREE AREAS SHALL RECEIVE 6" PH CORRECTED TOPSOIL AFTER TOPSOIL IS SPREAD EVENLY OVER ENTIRE AREA. ALL CLODS, LUMPS, STONES AND OTHER DELETERIOUS MATERIAL SHALL BE RAKED UP AND REMOVED.
- APPLICATION OF GRASS SEED, FERTILIZERS AND MULCH SHALL BE ACCOMPLISHED BY BROADCAST SEEDING OR HYDROSEEDING AT THE RATES OUTLINED BELOW:
LIMESTONE: 100 LBS./1,000 SQUARE FEET.
FERTILIZER: 500 LBS./ACRE OF 10-20-20 OR 1000 LBS./ACRE OF 5-10-10.
MULCH: HAY MULCH APPROXIMATELY 3 TONS/ACRE

SEED MIX (SLOPES LESS THAN 4:1)	LBS./ACRE
CREeping RED FESCUE	20
TALL FESCUE	15
PERENNIAL RYEGRASS	5
	42

SLOPE MIX (SLOPES GREATER THAN 4:1)	LBS./ACRE
CREeping RED FESCUE	20
TALL FESCUE	20
BIRDFOOT TREEFOIL	8
	48

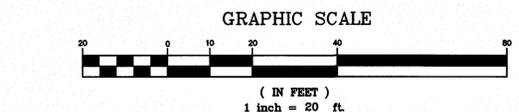
- FOR TEMPORARY EROSION CONTROL NOTES, SEE EROSION & SEDIMENT CONTROL PLAN.
- NEWLY GRADED AREAS REQUIRING SLOPE PROTECTION OUTSIDE OF NORMAL SEEDING SEASON SHALL RECEIVE STRAW MULCH AT THE APPROXIMATE RATE OF NO MORE THAN 3 TONS PER ACRE.
- ANY CHANGES IN PLANT LOCATIONS OR TYPES SHALL BE APPROVED BY THE DEVELOPER AND TOWN PRIOR TO INSTALLATION.
- PLANTINGS SHALL BE GUARANTEED BY THE CONTRACTOR FOR ONE YEAR AFTER WRITTEN ACCEPTANCE OF THE DEVELOPER.
- EXPOSED SOILS SHALL BE SEEDED OR HAY MULCHED WITHIN 72 HOURS OF FINAL GRADING.
- THE CONTRACTOR SHALL INSTALL AN IRRIGATION SYSTEM TO PROVIDE COMPLETE COVERAGE OF ALL SEED, SOD AREAS AND SHRUB BEDS. THE SYSTEM SHALL INCLUDE A TIMER WITH RAIN SENSOR AND SHALL BE INSTALLED IN ACCORDANCE WITH LOCAL CODES. IRRIGATION SYSTEM SHALL BE ROUTED TO PYLON SIGN PLANTER BED AND PLANTER BED ALONG BUILDING.



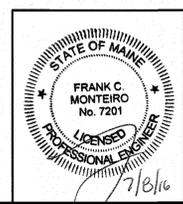
MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1 - LOT 28
50,965 Sq.Ft.
1.170 Ac.±

MAP CC1 LOT 27
N/F PROMPTO, INC.
70 SCOTT DRIVE
WESTBROOK, ME 04092
BOOK 8252 PAGE 135



LEGEND	
○	IRON PIN FOUND
□	CONCRETE BOUND FOUND
△	RAILROAD SPIKE FOUND
○	DRILL HOLE FOUND
—	VERTICAL GRANITE CURB
—	SLOPED GRANITE CURB
—	BITUMINOUS CONCRETE LIP CURBING
—	BITUMINOUS CONCRETE BERM
—	OVERHEAD SERVICE WIRES
—	DOUBLE SOLID YELLOW LINE
—	SINGLE SOLID WHITE LINE
—	BROKEN WHITE LINE
—	SIGN
—	OBSERVATION WELL
—	TEST PIT
—	TEST BORING
—	TREELINE
—	UTILITY POLE
—	DRAIN MANHOLE
—	SEWER MANHOLE
—	TELEPHONE MANHOLE
—	CATCH BASIN
—	WATER LINE
—	WATER VALVE
—	FIRE HYDRANT
—	GAS VALVE
—	GAS LINE
—	UNDERGROUND TELEPHONE LINE
—	UNDERGROUND ELECTRIC AND TELEPHONE
—	WETLAND LINE
—	SPOT ELEVATION
—	CONTOUR ELEVATION



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

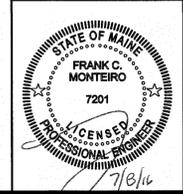
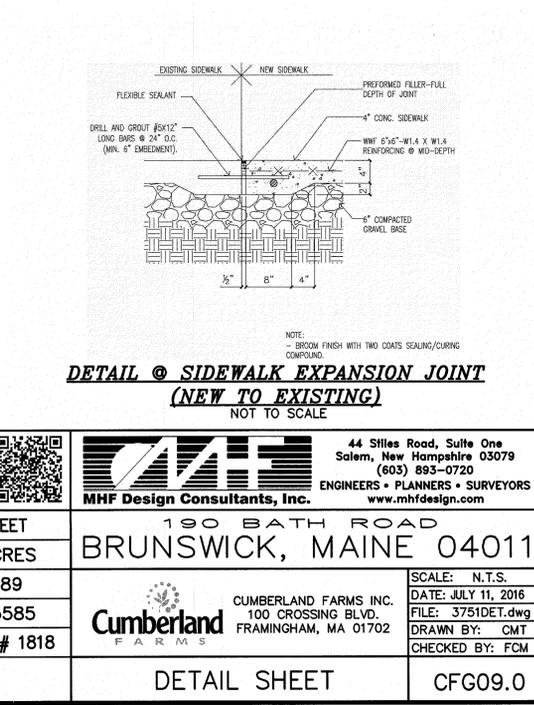
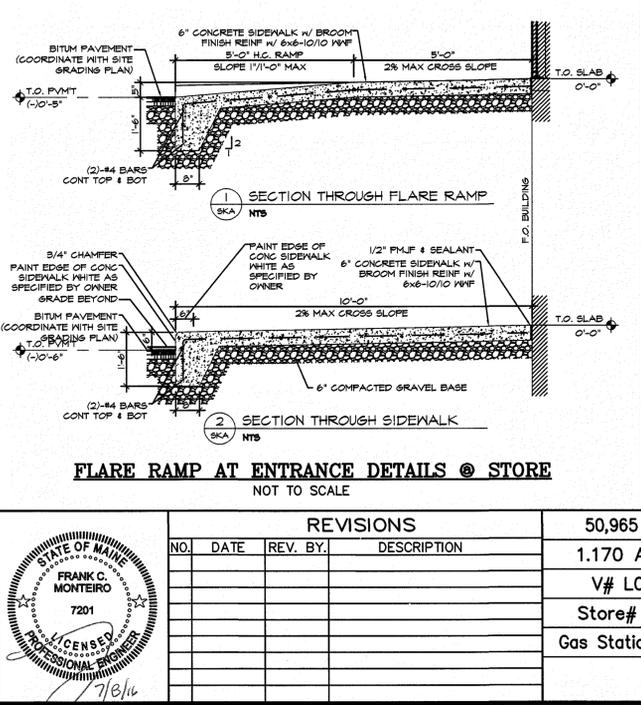
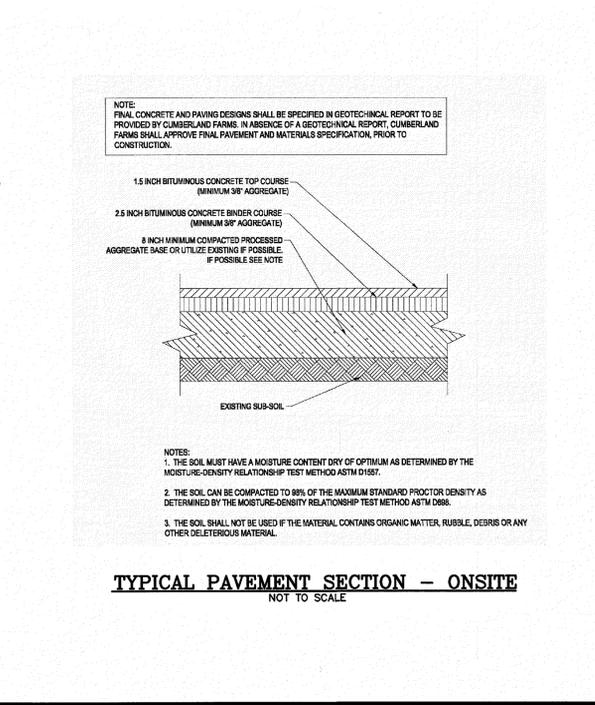
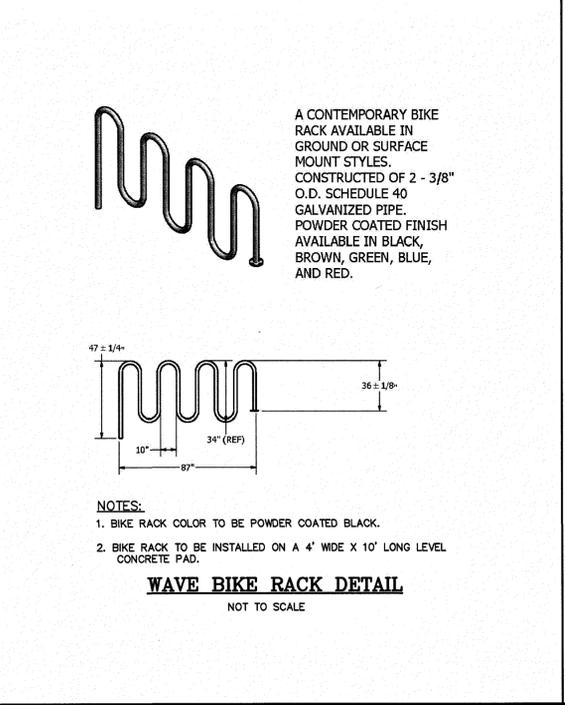
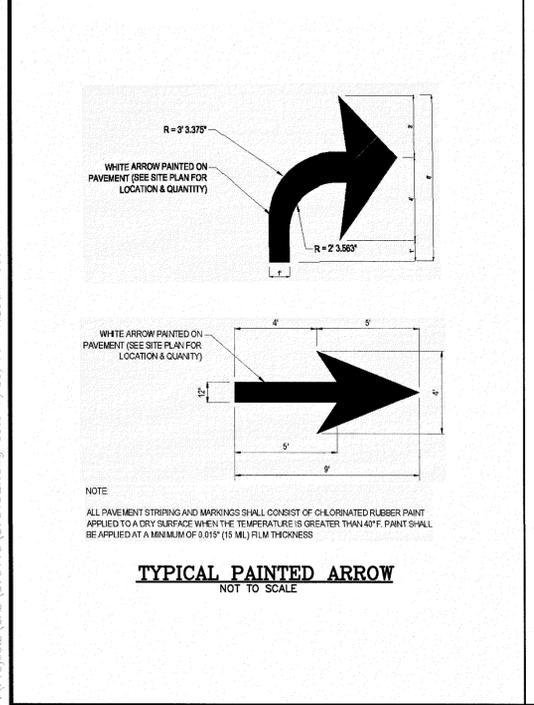
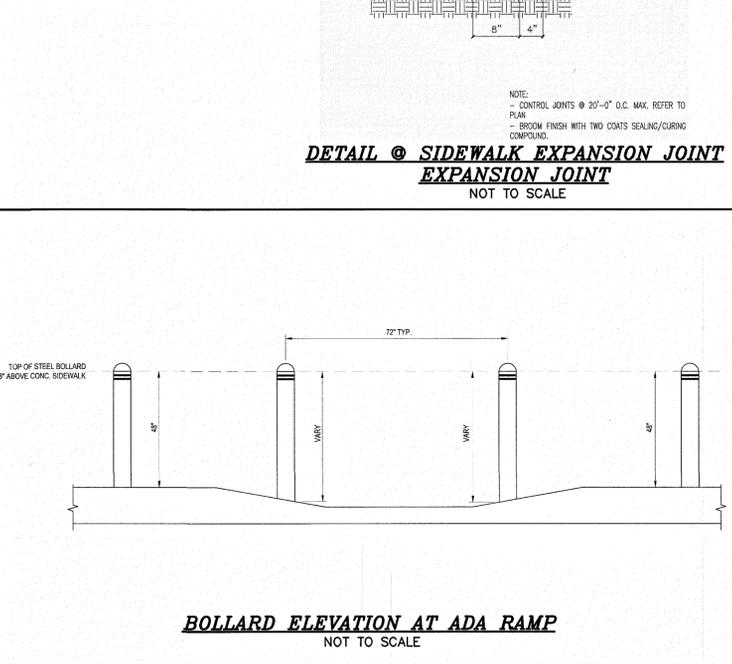
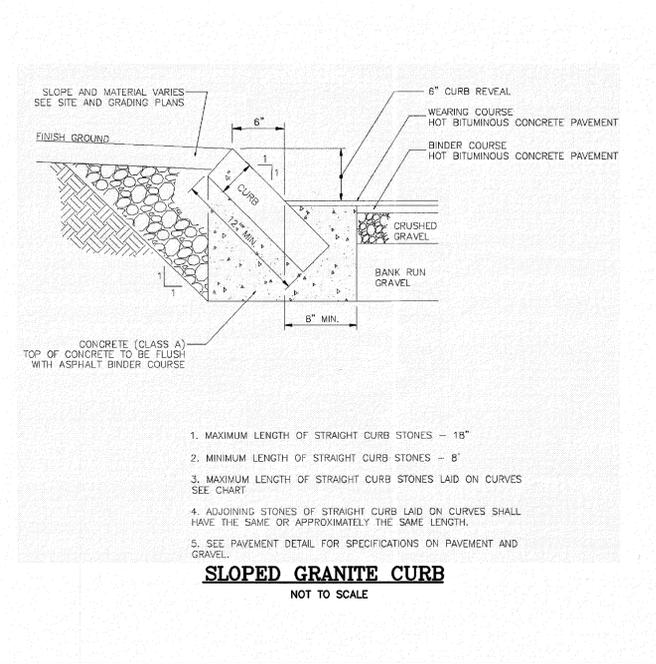
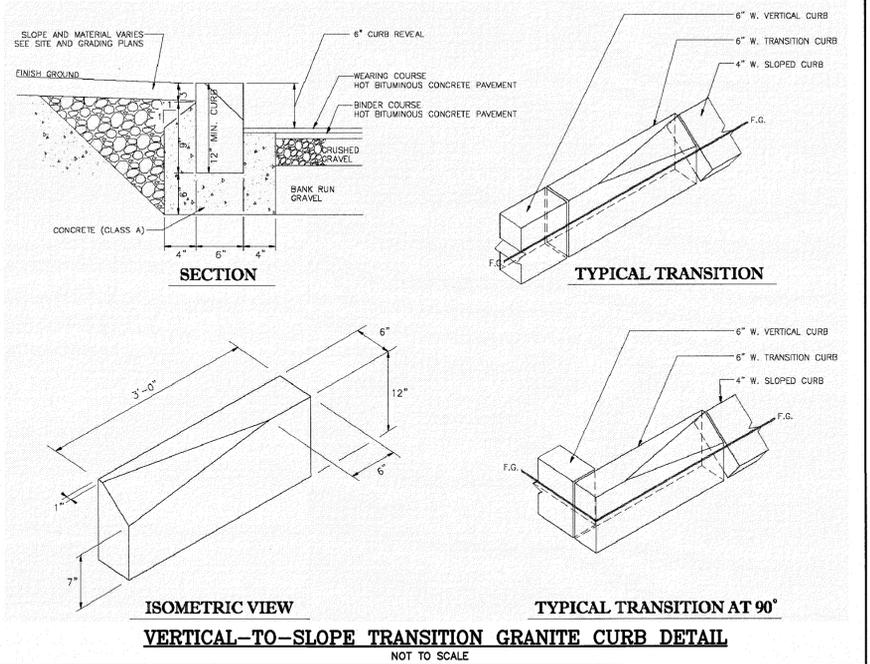
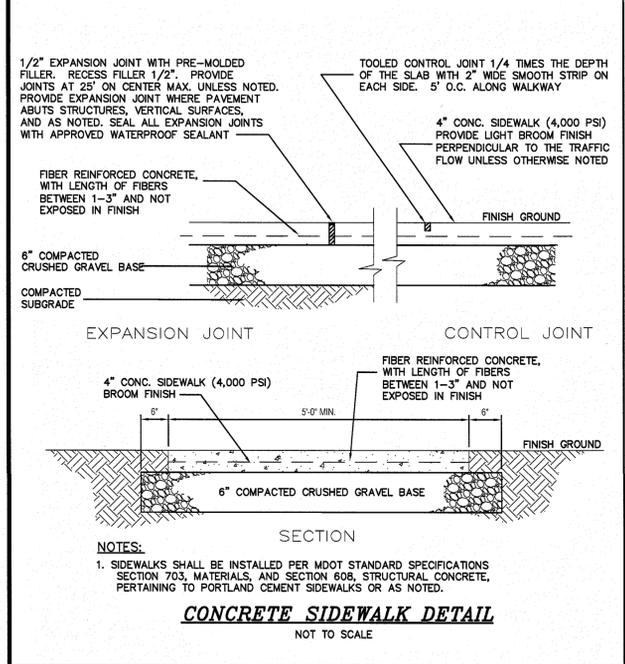
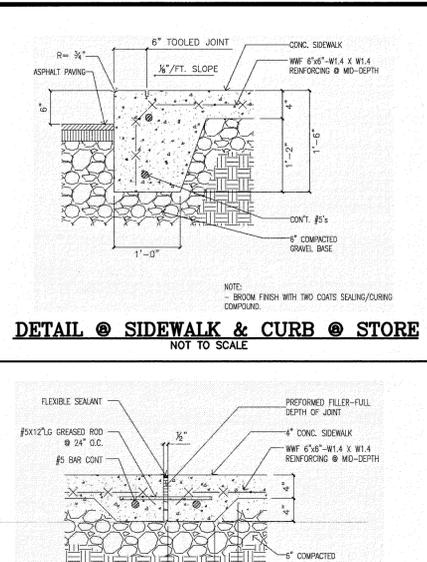
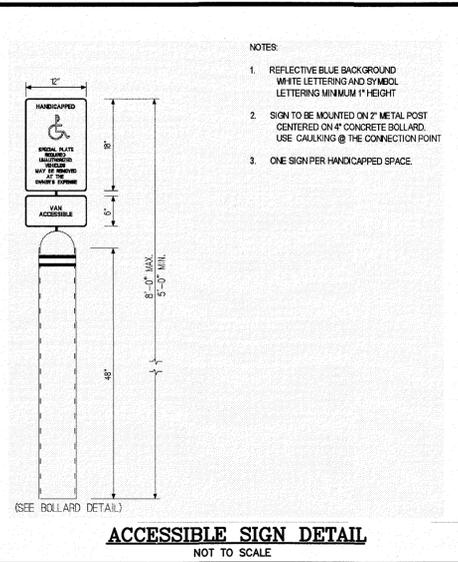
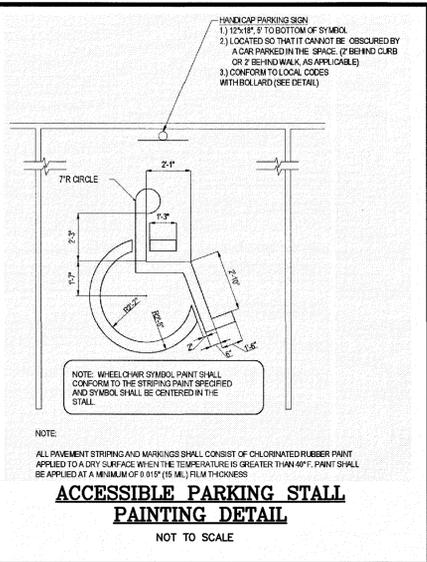
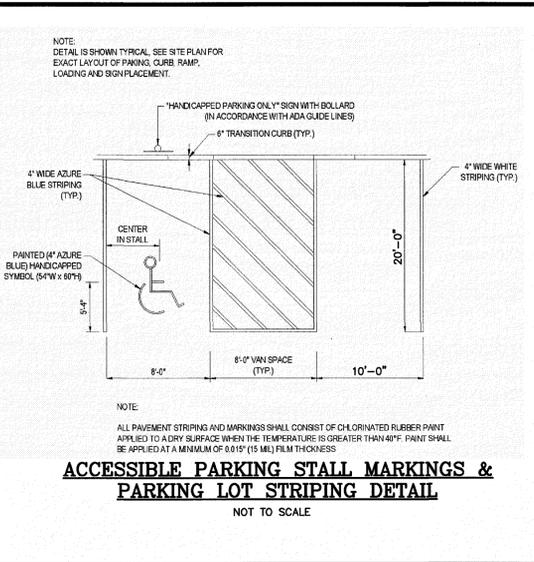
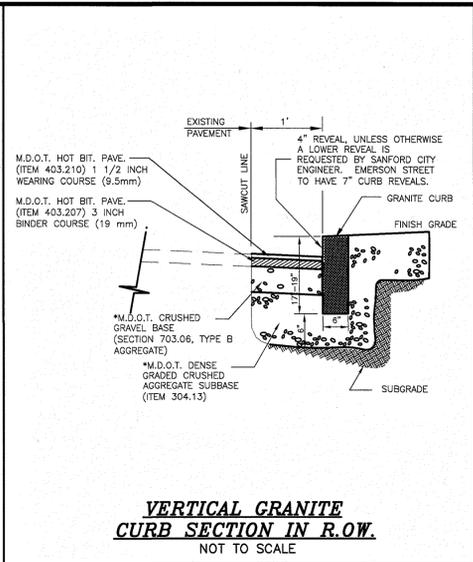
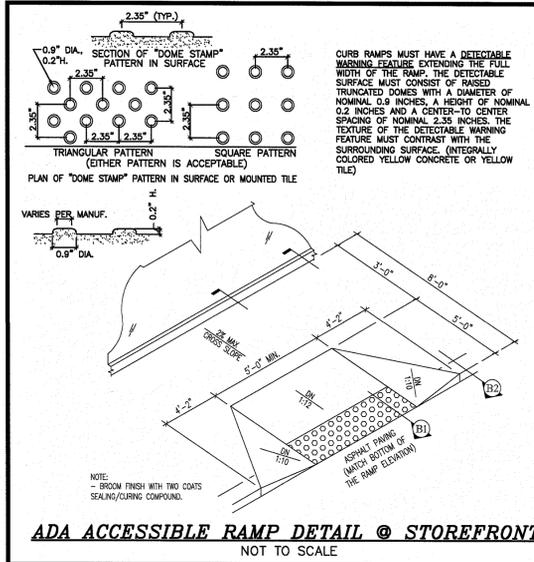
190 BATH ROAD
BRUNSWICK, MAINE 04011

SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM

LANDSCAPE PLAN
CFG08.0

MHF PROJECT NO. 375115 SHEET 8 OF 12

FAX: Projects\CAD\375115\3751SP.dwg LA 7/08/16 9:27am ccc
 Copyright © 2015 by MHF Design Consultants, Inc. All Rights Reserved.



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

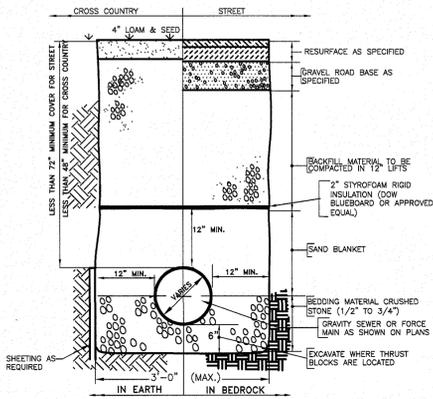
190 BATH ROAD
BRUNSWICK, MAINE 04011

44 Siles Road, Suite One
Salem, New Hampshire 03079
(603) 693-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

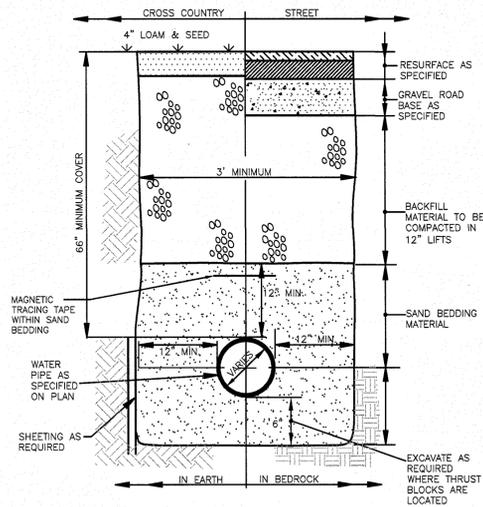
SCALE: N.T.S.
DATE: JULY 11, 2016
FILE: 3751DET.dwg
DRAWN BY: CMT
CHECKED BY: FCM

DETAIL SHEET
CFG09.0

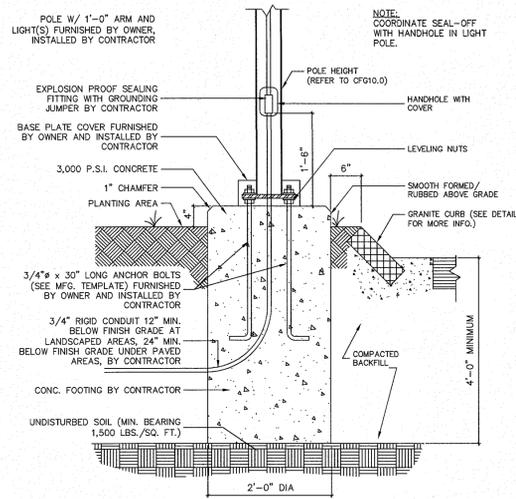
F:\Projects\CAD\3751\15\3751DET.dwg 09.0 7/05/16 11:28am ccc



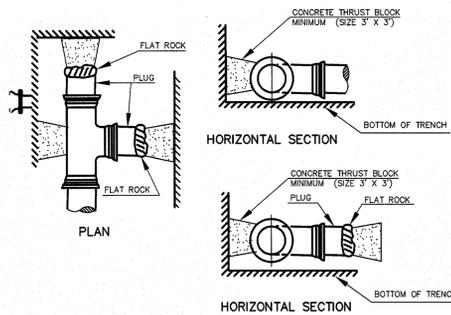
TYPICAL TRENCH SECTION FOR SANITARY SEWER SERVICE WITH LESS THAN 6' OF COVER
NOT TO SCALE



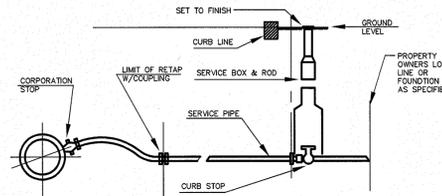
TYPICAL TRENCH SECTION FOR WATER SERVICE
NOT TO SCALE



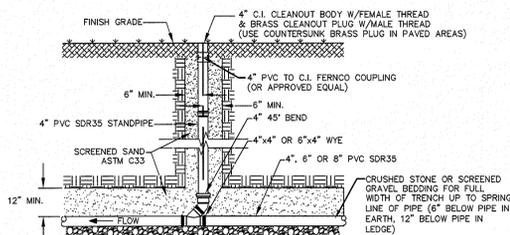
LIGHT POLE DETAIL
NOT TO SCALE



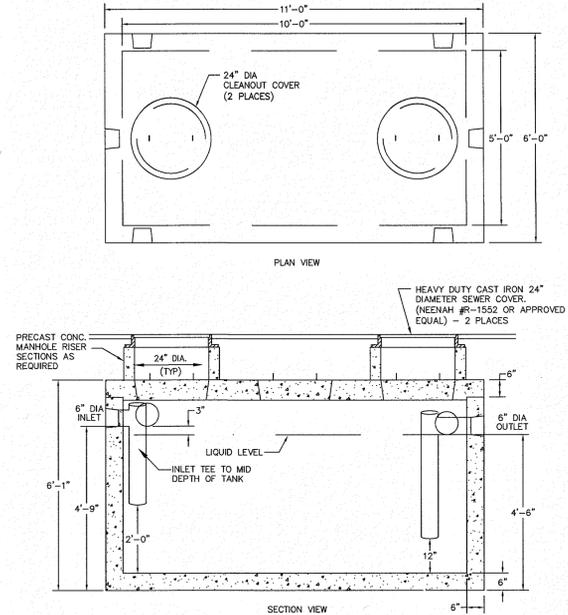
THRUST BLOCK DETAILS
NOT TO SCALE



TYPICAL SERVICE CONNECTION
NOT TO SCALE

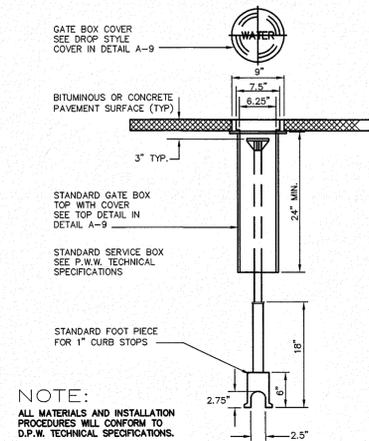


TYPICAL SEWER SERVICE CLEANOUT
NOT TO SCALE



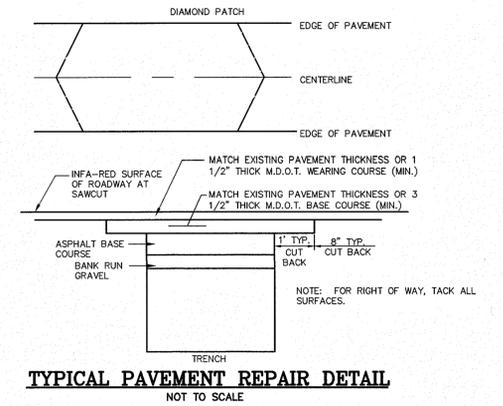
- NOTES:
1. CONCRETE: 5,000 PSI MINIMUM AFTER 28 DAYS.
 2. FINAL DESIGN OF GREASE TRAP TO MEET ALL LOCAL AND STATE REQUIREMENTS.
 3. ALL REINFORCEMENT PER ASTM C1227-93.
 4. DESIGNED FOR H-20 LOADING.
 5. TONGUE & GROOVE JOINT SEALED WITH BUTYL RESIN.
 6. TEES AND BAFFLES SOLD SEPARATELY.
 7. GREASE TRAP AS MANUFACTURED BY SHEA CONCRETE PRODUCTS OR APPROVED EQUAL.
 8. CONTRACTOR TO PROVIDE DESIGN ENGINEER SHOP DRAWINGS PRIOR TO CONSTRUCTION.

1,500 GALLON GREASE TRAP DETAIL
NOT TO SCALE

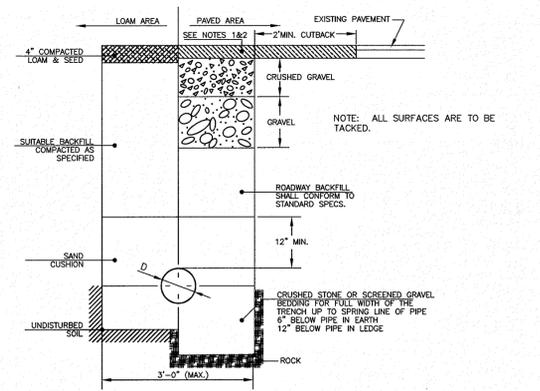


NOTE:
ALL MATERIALS AND INSTALLATION PROCEDURES WILL CONFORM TO D.P.W. TECHNICAL SPECIFICATIONS.

SERVICE BOX INSTALLATION IN PAVEMENT
NOT TO SCALE

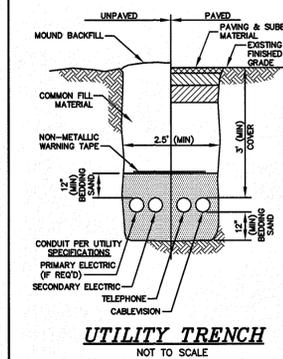


TYPICAL PAVEMENT REPAIR DETAIL
NOT TO SCALE

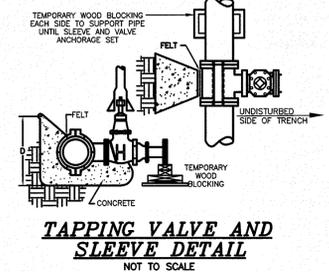


- NOTES:
1. PAVEMENT REPAIR IN EXISTING ROADWAYS SHALL CONFORM TO STREET OPENING REGULATIONS. DEPTH FOR BITUMINOUS PAVEMENT SHALL BE EQUAL TO EXISTING PAVEMENT WITH 3" MINIMUM (1 1/2" WEARING, 2 1/2" BASE).
 2. NEW ROADWAY CONSTRUCTION SHALL CONFORM TO DPW & M.D.O.T. SPECIFICATIONS.
 3. REFER TO OFFSITE AND EXISTING PAVEMENT CROSS SECTIONS FOR DEPTH OF SELECT MATERIALS, OR MATCH EXISTING DEPTH, WHICHEVER IS GREATER.
 4. NEW ROADWAY CONSTRUCTION SHALL CONFORM TO DPW & M.D.O.T. SPECIFICATIONS.
 5. A MINIMUM 2' CUTBACK IS REQUIRED AT THE TOP OF THE TRENCH WALL OVER UNDISTURBED MATERIAL.
 6. 12.5 MM HMA CAN BE PLACED FOR THE ENTIRE THICKNESS OF TRENCHES IN EMERSON STREET, NO. 9.5 FRESH HMA. MINIMUM THICKNESS FOR PAVEMENT LIFTS SHALL BE 1.25 INCHES.

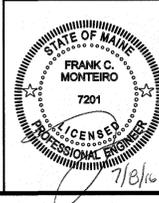
TYPICAL SEWER TRENCH DETAIL
NOT TO SCALE



UTILITY TRENCH
NOT TO SCALE



TAPPING VALVE AND SLEEVE DETAIL
NOT TO SCALE



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

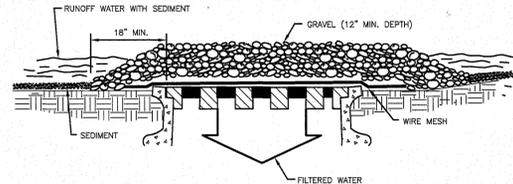
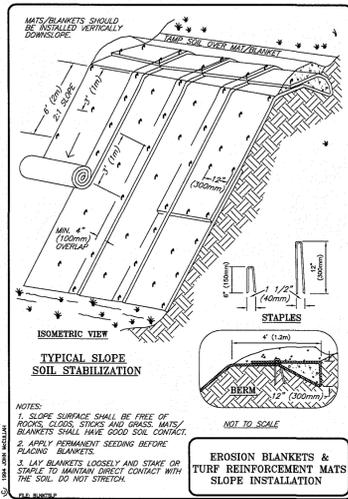
MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesgn.com

190 BATH ROAD
BRUNSWICK, MAINE 04011

Cumberland Farms
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

SCALE: N.T.S.
DATE: JULY 11, 2016
FILE: 3751DET.dwg
DRAWN BY: CMT
CHECKED BY: FCM

DETAIL SHEET
CFG09.1

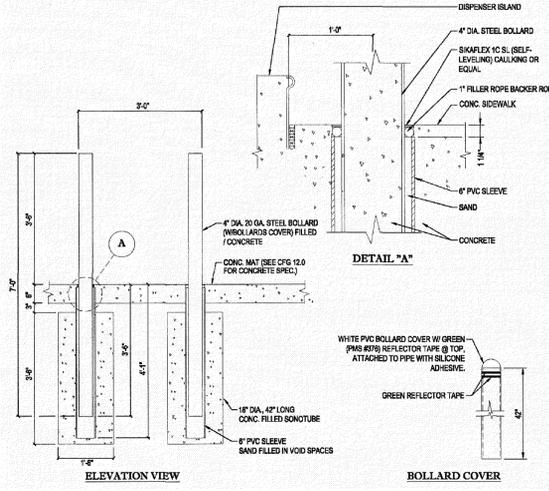


CONSTRUCTION SEQUENCE:

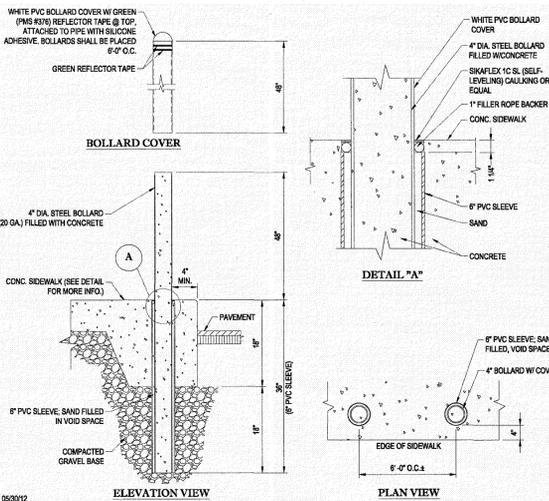
- 1) A WIRE MESH SHOULD BE PLACED OVER THE DROP INLET OR CURB OPENING SO THAT THE ENTIRE OPENING AND A MINIMUM OF 12 INCHES AROUND THE OPENING ARE COVERED BY THE MESH. THE MESH MAY BE ORDINARY HARDWARE CLOTH OR WIRE MESH WITH OPENINGS UP TO 1/2 INCH.
- 2) THE WIRE MESH SHOULD BE COVERED WITH CLEAN COARSE AGGREGATE SUCH AS SEWER STONE FOR A MINIMUM DEPTH OF 12 INCHES.
- 3) THE COARSE AGGREGATE SHOULD EXTEND AT LEAST 18 INCHES ON ALL SIDES OF THE DRAIN OPENING.

MAINTENANCE: ALL STRUCTURES SHOULD BE INSPECTED AFTER EVERY RAIN STORM AND REPAIRS MADE AS NECESSARY. SEDIMENT SHOULD BE REMOVED FROM THE TRAPPING BEFORE THE SEDIMENT HAS REACHED A MAXIMUM OF ONE HALF THE DEPTH OF THE TRAP. THE SEDIMENT SHOULD BE DISPOSED OF IN A SUITABLE AREA AND PROTECTED FROM EROSION BY EITHER STRUCTURAL OR VEGETATIVE MEANS. THE TEMPORARY TRAPS SHOULD BE REMOVED AND THE AREA REPAIRED AS SOON AS THE CONTRIBUTING DRAINAGE AREA TO THE INLET HAS BEEN COMPLETELY STABILIZED.

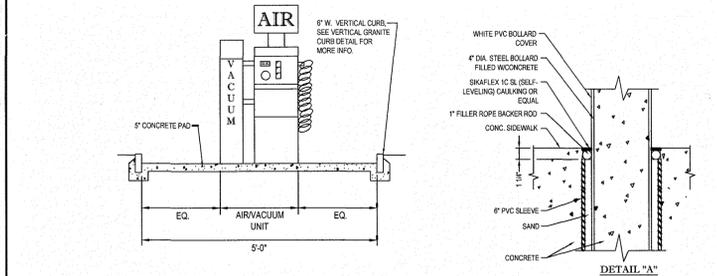
ON-SITE INLET PROTECTION DETAIL
NOT TO SCALE



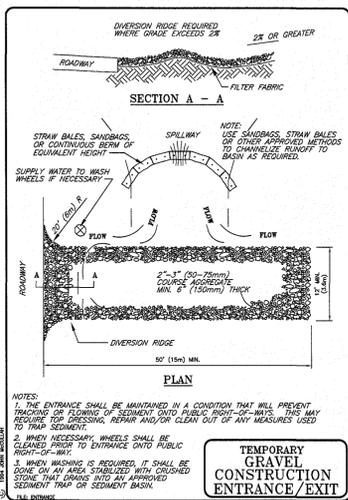
CANOPY BOLLARD DETAIL
NOT TO SCALE



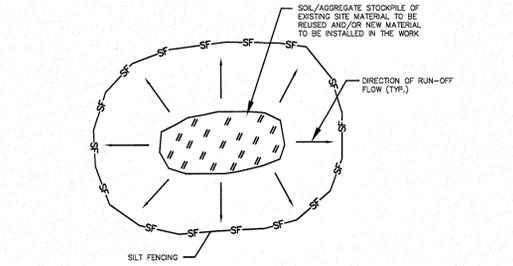
BOLLARD SET IN CONCRETE WALK
NOT TO SCALE



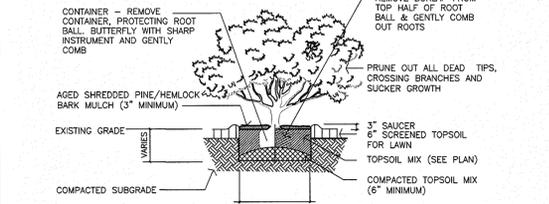
AIR TOWER DETAIL
NOT TO SCALE



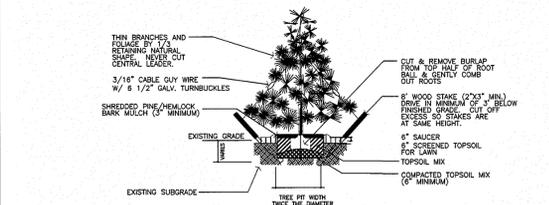
TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT



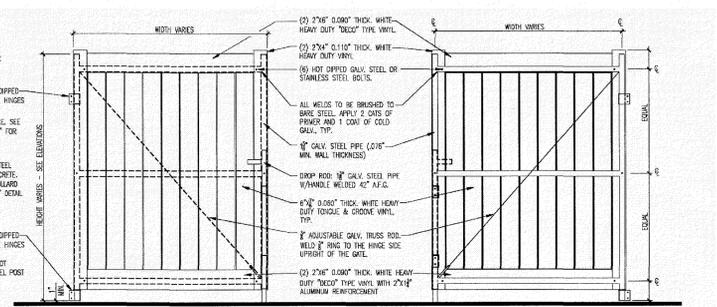
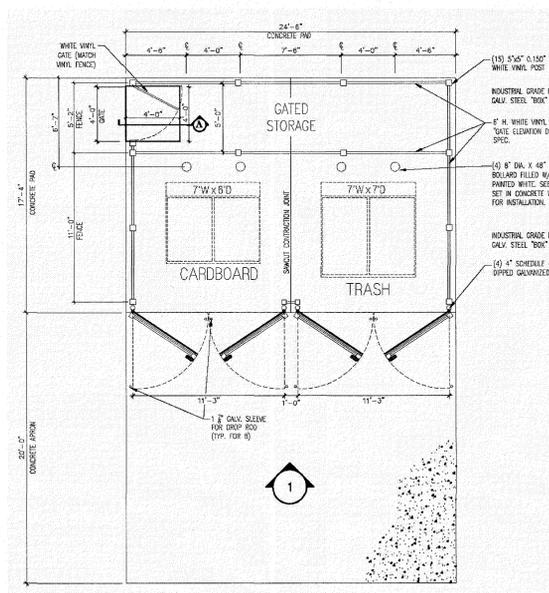
MATERIALS STOCKPILE DETAIL
NOT TO SCALE



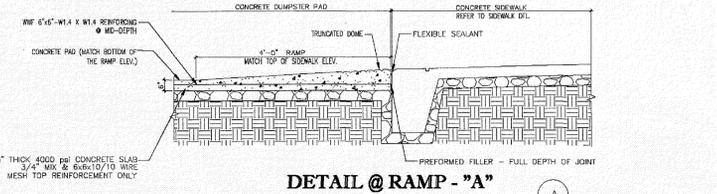
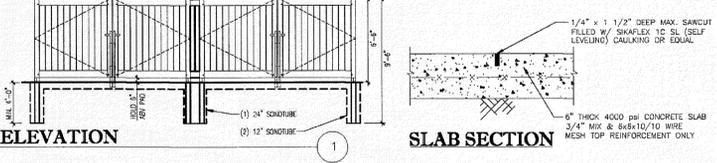
TYPICAL SHRUB PLANTING
NOT TO SCALE



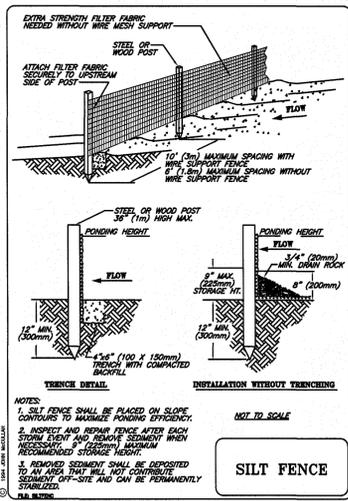
TYPICAL EVERGREEN TREE PLANTING
NOT TO SCALE



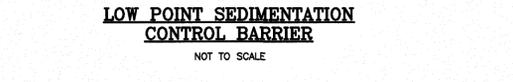
DUMPSTER ENCLOSURE - GATE ELEVATION DETAILS



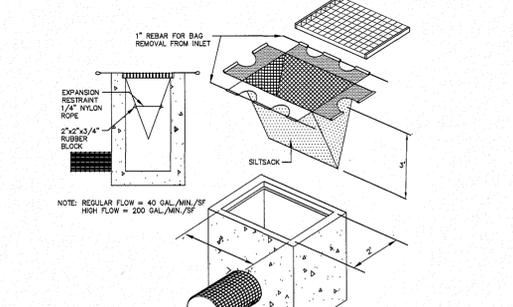
DUMPSTER DETAIL
NOT TO SCALE



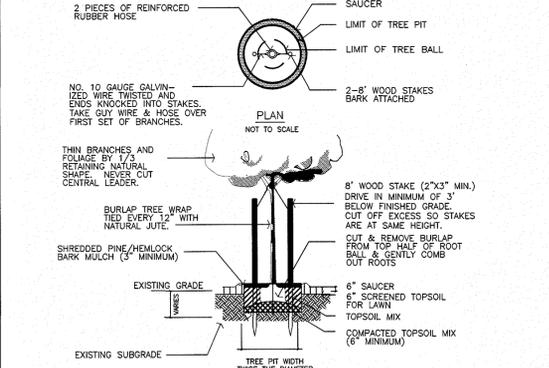
SILT FENCE



LOW POINT SEDIMENTATION CONTROL BARRIER
NOT TO SCALE

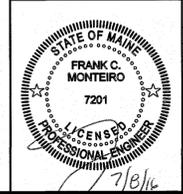


SILTSACK DETAIL-ON OR OFF SITE
NOT TO SCALE



TYPICAL TREE PLANTING
NOT TO SCALE

NOTE: FINAL CONCRETE AND PAVING DESIGNS SHALL BE SPECIFIED IN GEOTECHNICAL REPORT TO BE PROVIDED BY CUMBERLAND FARMS. IN ABSENCE OF A GEOTECHNICAL REPORT, CUMBERLAND FARMS SHALL APPROVE FINAL PAVEMENT AND MATERIALS SPECIFICATION, PRIOR TO CONSTRUCTION.

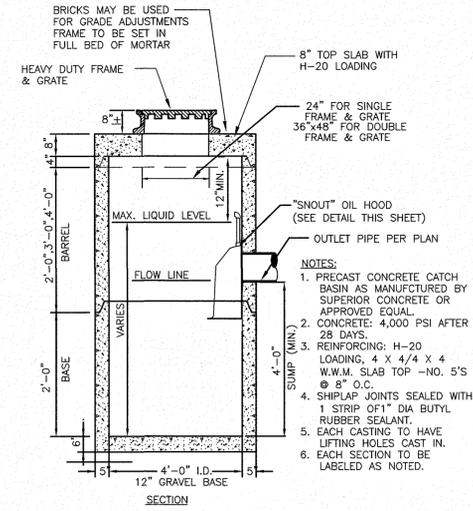
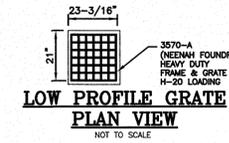


REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

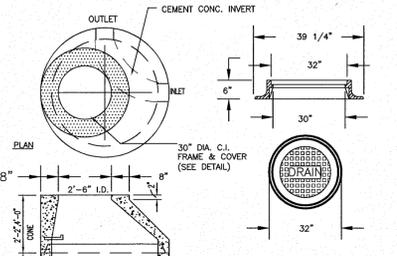
50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

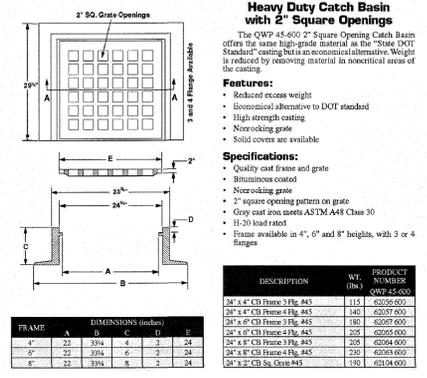
190 BATH ROAD
BRUNSWICK, MAINE 04011
CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702
SCALE: N.T.S.
DATE: JULY 11, 2016
FILE: 3751DET.dwg
DRAWN BY: CMT
CHECKED BY: FCM
DETAIL SHEET
CFG09.2



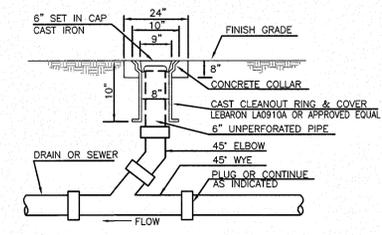
PRECAST CONCRETE CATCH BASIN WITH HOOD
NOT TO SCALE



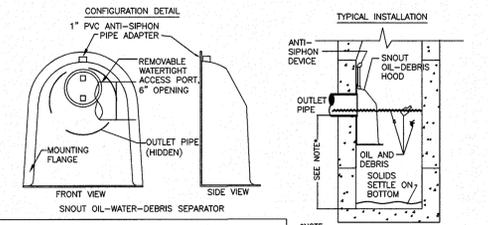
PRECAST CONCRETE DRAIN MANHOLE
MAXIMUM PIPE DIAMETER 30\"/>



CATCH BASIN FRAME & GRATE DETAIL
NOT TO SCALE



TYPICAL CLEANOUT
NOT TO SCALE



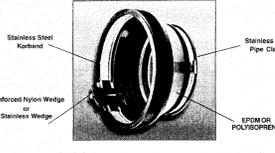
SNOUT - OIL - WATER - DEBRIS SEPARATOR
NOT TO SCALE

INSTALLATION DETAIL

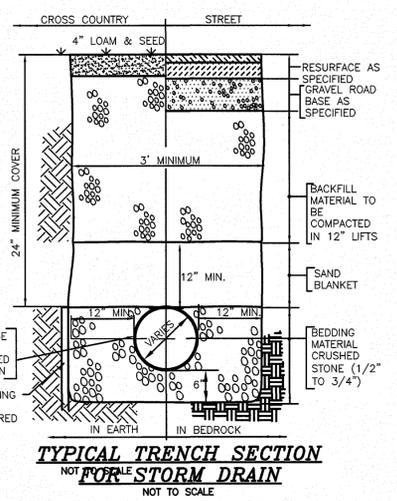
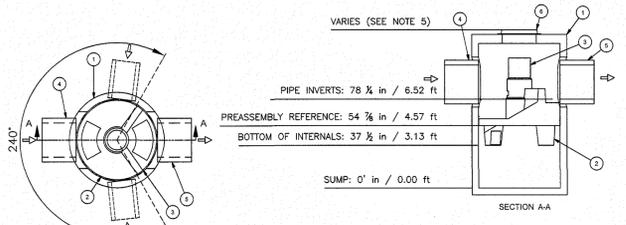
HOOD SPECIFICATION FOR CATCH BASINS AND WATER QUALITY STRUCTURES

DESCRIPTION	DATE	SCALE
OIL - DEBRIS HOOD SPECIFICATION AND INSTALLATION (TYPICAL)	09/08/00	NONE
3/8\"/>		

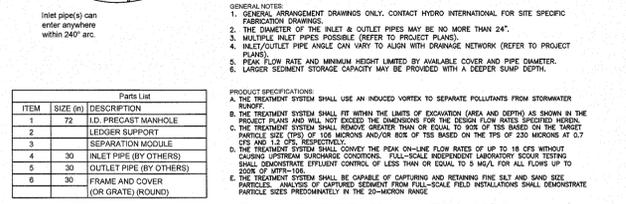
KOR-N-SEAL® I & II
FLEXIBLE PIPE-TO-MANHOLE CONNECTORS
SPECIFICATION SHEET



KOR-N-SEAL DETAIL
NOT TO SCALE

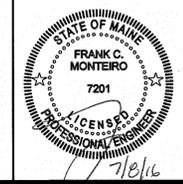


TYPICAL TRENCH SECTION FOR STORM DRAIN
NOT TO SCALE



"FIRST DEFENSE" UNIT DETAIL - FD-6HC
(OR APPROVED EQUAL)
NOT TO SCALE

NOTE: CONTRACTOR SHOULD CONFIRM SYSTEM PARTS AND OBTAIN SHOP DRAWINGS FROM MANUFACTURER PRIOR TO CONSTRUCTION.



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

MHF Design Consultants, Inc.

44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

190 BATH ROAD
BRUNSWICK, MAINE 04011

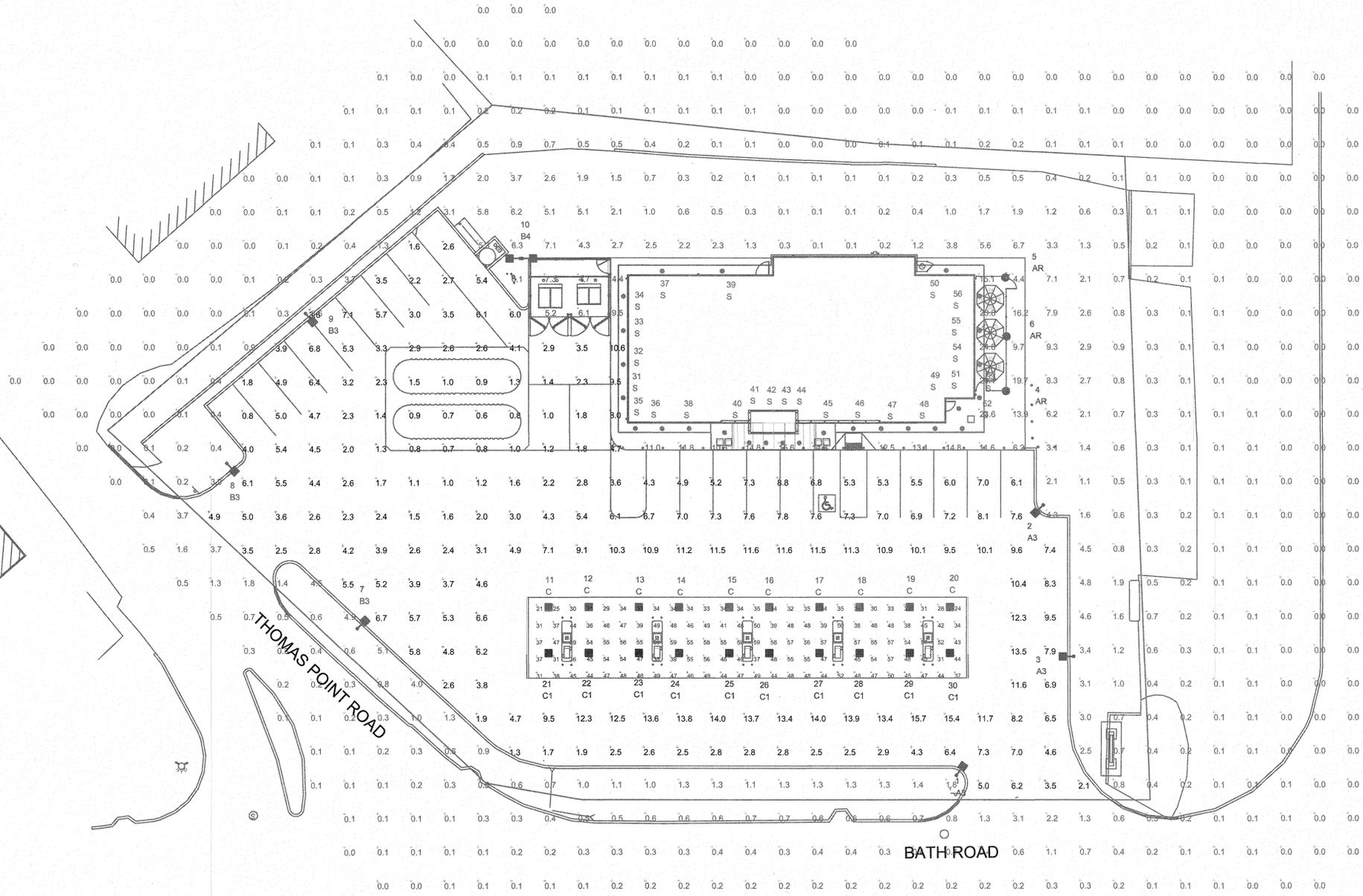
Cumberland FARMS
CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

SCALE: N.T.S.
DATE: JULY 11, 2016
FILE: 3751DET.dwg
DRAWN BY: CMT
CHECKED BY: FCM
CFG09.3

MHF PROJECT NO. 375115 SHEET 12 OF 12

F:\Projects\CAD\375115\375115.DET.dwg 09.3 7/08/16 11:27am ccc

NOTE:
- FOOTCANDLE LEVELS CALCULATED AT GRADE USING INITIAL LUMEN VALUES
- EXCEPT FOR TYPE "AR" FIXTURES, ALL POLE MOUNTED FIXTURES ARE MOUNTED ON A 15FT POLE ATOP A CONCRETE BASE FLUSH AT GRADE.
- TYPE "AR" FIXTURE IS MOUNTED ON AN 8 FT POLE ATOP A CONCRETE BASE FLUSH AT GRADE.



LumNo	Label	MTG. HGT.
1	A3	15
2	A3	15
3	A3	15
4	AR	8
5	AR	8
6	AR	8
7	B3	15
8	B3	15
9	B3	15
10	B4	15
11	C	14
12	C	14
13	C	14
14	C	14
15	C	14
16	C	14
17	C	14
18	C	14
19	C	14
20	C	14
21	C1	14
22	C1	14
23	C1	14
24	C1	14
25	C1	14
26	C1	14
27	C1	14
28	C1	14

LUM NO.	LABEL	MTG. HT.
29	C1	14
30	C1	14
31	S	12
32	S	12
33	S	12
34	S	12
35	S	12
36	S	12
37	S	12
38	S	12
39	S	12
40	S	12
41	S	16.228
42	S	16.945
43	S	16.945
44	S	16.228
45	S	12
46	S	12
47	S	12
48	S	12
49	S	12
50	S	12
51	S	12
52	S	12
53	S	12
54	S	12
55	S	12
56	S	12

LABEL	AVG	MAX	MIN	AVG/MIN	MAX/MIN
CANOPY	44.08	59	21	2.10	2.81
PAVED AREA	5.44	15.7	0.6	9.07	26.17
UNDEFINED AREA	1.23	24.8	0.0	N.A.	N.A.

SYMBOL	QTY	LABEL	ARRANGEMENT	LUMENS	LLF	ARR. WATTS	TOTAL WATTS	MANUFACTURER	DESCRIPTION
[Symbol]	3	A3	SINGLE	7896	1.040	134	402	CREE, INC.	ARE-EDG-3MB-DA-06-E-UL-XX-700-57K
[Symbol]	3	AR	SINGLE	7985	1.040	92	276	CREE INC.	ARE-EDR-5M-R5-04-E-UL-XX-700-57K
[Symbol]	3	B3	SINGLE	8480	1.040	134	402	CREE INC.	ARE-EDG-4MB-DA-06-E-UL-700-57K
[Symbol]	1	B4	BACK-BACK	8480	1.040	268	268	CREE INC.	ARE-EDG-4MB-DA-06-E-UL-700-57K
[Symbol]	10	C	SINGLE	12060	1.040	134	1340	CREE, INC.	CAN-304-5M-RS-06-E-UL-XX-700-57K
[Symbol]	10	C1	SINGLE	13696	1.040	134	1340	CREE, INC.	CAN-304-PS-RS-06-E-UL-XX-700-57K
[Symbol]	26	S	SINGLE	1757	1.000	19.8	514.8	Cree Lighting - Recessed Downlight	LR618L-40K-120V-A-DR +RC6 HOUSING

AREA

SYMBOL:	QTY:	LABEL:
	3	A3

ARE-EDG-3MB-DA-06-E-UL-XX-700-57K

SYMBOL:	QTY:	LABEL:
	3	B3
	1	B4

ARE-EDG-4MB-DA-06-E-UL-700-57K

Cree Edge™ Series
LED Area/Flood Luminaire

Product Description
The Cree Edge™ Series Area Flood Luminaire is a rugged, low profile design. Its rugged cast aluminum housing minimizes wind load requirements and features an integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins. Applications: Parking lots, walkways, campuses, roads, lighting and general site lighting applications.

Performance Summary
Patented NanoCoat™ Product Technology
Made in the U.S.A. of U.S. and imported parts
CREE Minimum TFC (C)
CCT: 4000K (+/- 300K), 5700K (+/- 500K) standard
Limited Warranty: 10 years on luminaire (10 years on ColorCast™ Deflection™ Finish)

Ordering Information
Example: ARE-EDG-4MB-DA-06-E-UL-700-57K

Product	Qty	Order	Mounting	LED Count (910)	Series	Options
ARE-EDG-3MB-DA-06-E-UL-XX-700-57K	3	DA	3MB	910	XX	700-57K
ARE-EDG-4MB-DA-06-E-UL-700-57K	3	DA	4MB	910	700	57K

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

Product Specifications

CONSTRUCTION & MATERIALS
• Area Flood Luminaire with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.

ELECTRICAL SYSTEM
• Input Voltage: 120V or 277V (50/60 Hz)
• Power Factor: 0.95 or full load
• 100% surge suppression protection standard
• 100% surge suppression protection standard
• 100% surge suppression protection standard

Recommended Cree Edge™ Series Luminaire Maintenance Factors (LMF)

Ambient	LMF	25% LMF	50% LMF	75% LMF	100% LMF
10000	1.00	0.90	0.80	0.70	0.60
15000	1.00	0.85	0.75	0.65	0.55
20000	1.00	0.80	0.70	0.60	0.50
25000	1.00	0.75	0.65	0.55	0.45
30000	1.00	0.70	0.60	0.50	0.40
35000	1.00	0.65	0.55	0.45	0.35
40000	1.00	0.60	0.50	0.40	0.30
45000	1.00	0.55	0.45	0.35	0.25
50000	1.00	0.50	0.40	0.30	0.20

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

CANOPY

SYMBOL:	QTY:	LABEL:
	3	AR

ARE-EDR-5M-R5-04-E-UL-XX-700-57K

SYMBOL:	QTY:	LABEL:
	10	C1

Cree Edge™ Series
LED Area Luminaire - Round

Product Description
The Cree Edge™ Series Area Flood Luminaire is a rugged, low profile design. Its rugged cast aluminum housing minimizes wind load requirements and features an integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins. Applications: Auto Dealerships, parking lots, campuses, roads, lighting and general site lighting applications.

Performance Summary
Patented NanoCoat™ Product Technology
Made in the U.S.A. of U.S. and imported parts
CREE Minimum TFC (C)
CCT: 4000K (+/- 300K), 5700K (+/- 500K) standard
Limited Warranty: 10 years on luminaire (10 years on ColorCast™ Deflection™ Finish)

Ordering Information
Example: ARE-EDR-5M-R5-04-E-UL-XX-700-57K

Product	Qty	Order	Mounting	LED Count (910)	Series	Options
ARE-EDR-5M-R5-04-E-UL-XX-700-57K	3	R5	5M	910	XX	700-57K
ARE-EDR-5M-R5-04-E-UL-XX-700-57K	10	R5	5M	910	700	57K

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

Product Specifications

CONSTRUCTION & MATERIALS
• Area Flood Luminaire with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.

ELECTRICAL SYSTEM
• Input Voltage: 120V or 277V (50/60 Hz)
• Power Factor: 0.95 or full load
• 100% surge suppression protection standard
• 100% surge suppression protection standard
• 100% surge suppression protection standard

Recommended Cree Edge™ Series Luminaire Maintenance Factors (LMF)

Ambient	LMF	25% LMF	50% LMF	75% LMF	100% LMF
10000	1.00	0.90	0.80	0.70	0.60
15000	1.00	0.85	0.75	0.65	0.55
20000	1.00	0.80	0.70	0.60	0.50
25000	1.00	0.75	0.65	0.55	0.45
30000	1.00	0.70	0.60	0.50	0.40
35000	1.00	0.65	0.55	0.45	0.35
40000	1.00	0.60	0.50	0.40	0.30
45000	1.00	0.55	0.45	0.35	0.25
50000	1.00	0.50	0.40	0.30	0.20

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

CANOPY

SYMBOL:	QTY:	LABEL:
	10	C1

CAN-304-5M-RS-06-E-UL-XX-700-57K

SYMBOL:	QTY:	LABEL:
	10	C1

304 Series™
LED Recessed Canopy Luminaire

Product Description
The 304 Series™ LED Recessed Canopy Luminaire is a rugged, low profile design. Its rugged cast aluminum housing minimizes wind load requirements and features an integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins. Applications: Auto Dealerships, parking lots, campuses, roads, lighting and general site lighting applications.

Performance Summary
Patented NanoCoat™ Product Technology
Made in the U.S.A. of U.S. and imported parts
CREE Minimum TFC (C)
CCT: 4000K (+/- 300K), 5700K (+/- 500K) standard
Limited Warranty: 10 years on luminaire (10 years on ColorCast™ Deflection™ Finish)

Ordering Information
Example: CAN-304-5M-RS-06-E-UL-XX-700-57K

Product	Qty	Order	Mounting	LED Count (910)	Series	Options
CAN-304-5M-RS-06-E-UL-XX-700-57K	10	RS	5M	910	XX	700-57K
CAN-304-5M-RS-06-E-UL-XX-700-57K	10	RS	5M	910	700	57K

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

Product Specifications

CONSTRUCTION & MATERIALS
• Area Flood Luminaire with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.
• 100% aluminum housing with integral, weatherlight LED driver component and high performance aluminum heat sink and heat-dissipating fins.

ELECTRICAL SYSTEM
• Input Voltage: 120V or 277V (50/60 Hz)
• Power Factor: 0.95 or full load
• 100% surge suppression protection standard
• 100% surge suppression protection standard
• 100% surge suppression protection standard

Recommended Cree Edge™ Series Luminaire Maintenance Factors (LMF)

Ambient	LMF	25% LMF	50% LMF	75% LMF	100% LMF
10000	1.00	0.90	0.80	0.70	0.60
15000	1.00	0.85	0.75	0.65	0.55
20000	1.00	0.80	0.70	0.60	0.50
25000	1.00	0.75	0.65	0.55	0.45
30000	1.00	0.70	0.60	0.50	0.40
35000	1.00	0.65	0.55	0.45	0.35
40000	1.00	0.60	0.50	0.40	0.30
45000	1.00	0.55	0.45	0.35	0.25
50000	1.00	0.50	0.40	0.30	0.20

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

SOFFIT

SYMBOL:	QTY:	LABEL:
	26	S

LR618L-40K-120V-A-DR +RC6 HOUSING

SYMBOL:	QTY:	LABEL:
	26	S

LR6 Series
LR6™ LED Downlight - 6"

Product Description
The LR6™ Downlight is an unparalleled combination of light quality and efficiency - bringing outstanding performance and value to the retrofit downlight space. Delivering up to 1800 lumens of exceptional 100-CRI light while achieving up to 100 lumens per watt, the breakthrough performance is achieved by combining the high efficacy and high quality of Cree™ TruBlue™ Technology, with an integrated driver and thermal management design. The LR6™ Downlight is available in warm or neutral color temperatures, three beam packages, and offers a variety of trim options.

Performance Summary
Patented NanoCoat™ Technology
Initial Delivered Lumens: 600-1800 Lumens
Input Power: 7.5-22 Watts
CRI: 90
CCT: 2700K, 3000K, 4000K
Lumens/Power: 100
Lifetime: 50,000 hours
Dimming: Dimmable to 5%
Most used in multiple of meter carton (MCQ) quantity of 6

Ordering Information
Example: LR618L-40K-120V-A-DR +RC6 HOUSING

Series	Initial Delivered Lumens*	CCT	Voltage	Beam Type
LR6-18	600	2700K	120V	Beam
LR6-18	600	3000K	120V	Beam
LR6-18	600	4000K	120V	Beam
LR6-18	1200	2700K	120V	Beam
LR6-18	1200	3000K	120V	Beam
LR6-18	1200	4000K	120V	Beam
LR6-18	1800	2700K	120V	Beam
LR6-18	1800	3000K	120V	Beam
LR6-18	1800	4000K	120V	Beam

USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

Product Specifications

CONSTRUCTION & MATERIALS
• Double aluminum housing protects the light source. Adjustable lip clips provide instant access for bulb changing.
• Thermal management system uses integral heat sink to conduct heat away from LEDs to optimize performance. LED surface temperature may vary slightly from ambient.
• Suitable for residential and non-residential installation.
• Clearance dimensions lower reflector reflects light while conducting heat away from LEDs, to create a uniform light transition from the lens to the ceiling above and evenly illuminates the 5.7" (146mm) space.

OPTICAL SYSTEM
• Unique combination of effective and retro-reflective optical components achieves a uniform, consistent appearance while eliminating glare, hot spots and undesirable glare.
• Components work together to optimize distribution, including to delivery of high lumenance beam in horizontal surface with as little amount of light on walls and vertical surfaces. This increases the perception of illumination.
• Open top polycarbonate diffusing lens shields direct view of LEDs and provides greater visual comfort.

ELECTRICAL SYSTEM
• Integral high efficiency power supply
• Power Factor: 0.95
• Total Harmonic Distortion: 20%
• Input Voltage: 120V/180V or 277V/347V
• Dimming: 120V dimmable to 5% with most accent dimmers; 277V dimmable to 5% with most accent dimmers. Reference www.cree.com/lighting/Products/Indoor/Downlight/LR6-Series for recommended dimmers.
• Operating Temperature Range: -40°C (-40°F) to +45°C (+105°F)

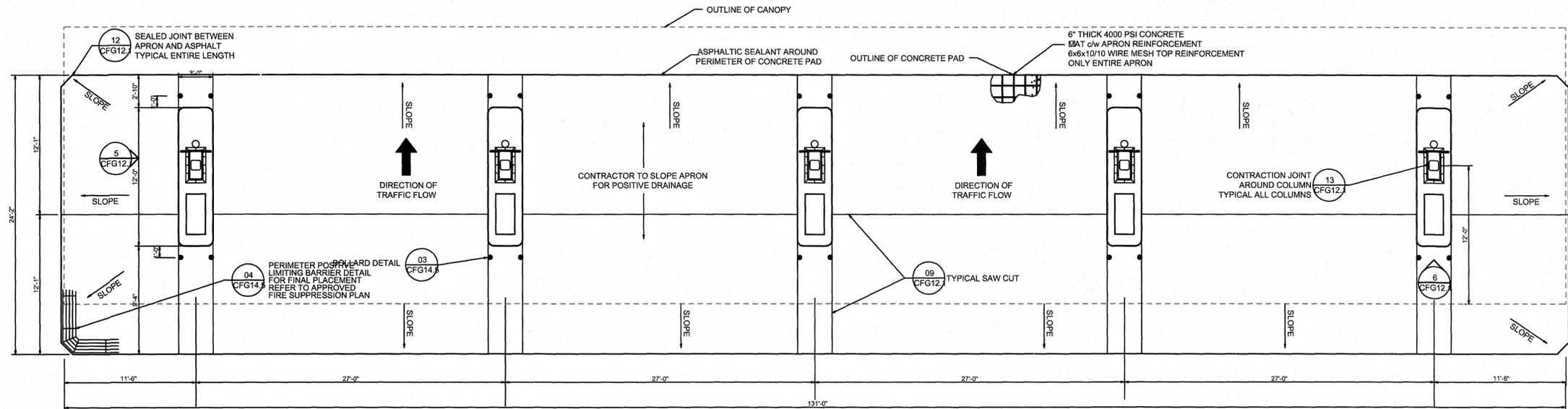
REGULATORY & VOLUNTARY QUALIFICATIONS
• cULus Listed
• Suitable for wet locations
• Meets IEC Part 15 standards for coordinated and uncoordinated operation
• Emissions rated per IEC 60529 when ordered without option
• Certified to ANSI C136.31 (2011), IASO normal irradiance standards when ordered with 100-1000 lux
• 100% surge suppression protection tested in accordance with IEEE/ANSI C82.41.2
• Luminance and beam uniformity tested in accordance with IEEE/ANSI C82.41.2
• Meets IEC Part 15 standards for coordinated and uncoordinated operation
• Luminance and beam uniformity tested in accordance with IEEE/ANSI C82.41.2
• Meets IEC Part 15 standards for coordinated and uncoordinated operation
• Luminance and beam uniformity tested in accordance with IEEE/ANSI C82.41.2
• Meets IEC Part 15 standards for coordinated and uncoordinated operation

Application Reference

Spacing	Lumens	Wattage	LWF	w/L	Average FC
4 x 4	600	6.0	62	0.84	27
4 x 8	1200	10.5	100	0.84	27
8 x 8	1800	15.8	100	0.84	27
10 x 10	2700	22.5	100	0.84	27

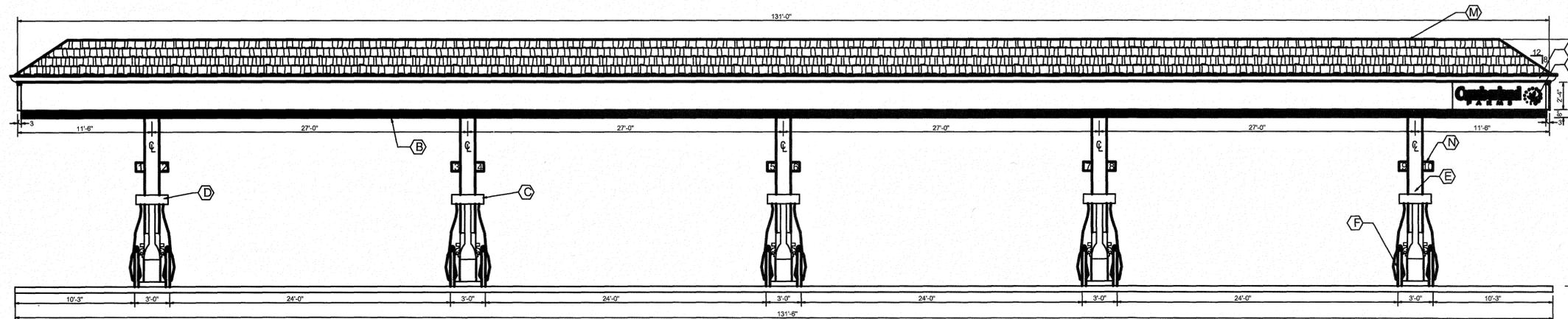
USA: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415
Canada: www.cree.com/canada T (800) 472-1234 F (905) 890-7507

STORE SIDE

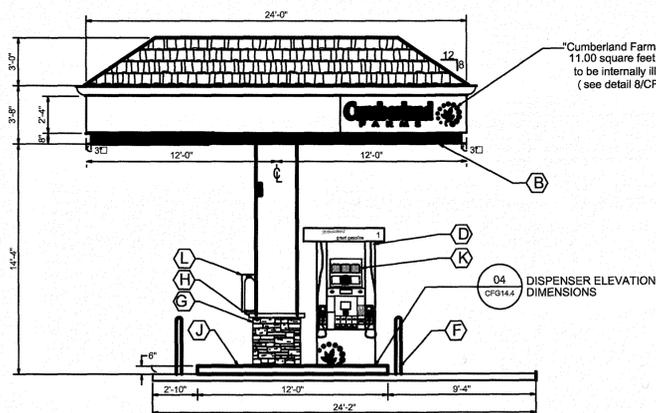


1 CANOPY PLAN VIEW
CFG12 SCALE: 3/16" = 1'-0"

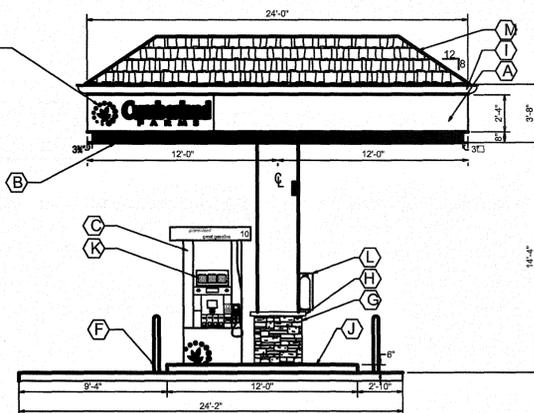
STREET SIDE



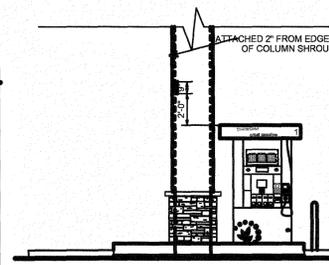
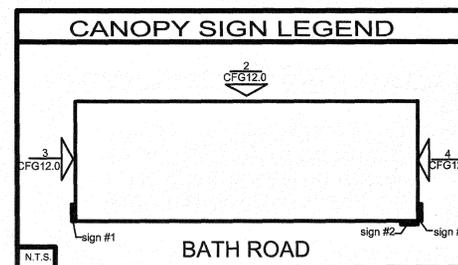
2 CANOPY FRONT ELEVATION
CFG12 SCALE: 3/16" = 1'-0"



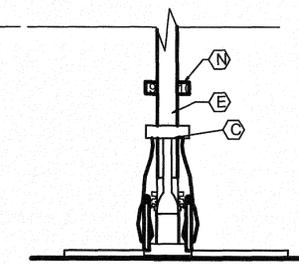
3 CANOPY LEFT ELEVATION
CFG12 SCALE: 3/16" = 1'-0"



4 CANOPY RIGHT ELEVATION
CFG12 SCALE: 3/16" = 1'-0"



5 GAS DISPENSER # SIGN LOCATION DETAIL
CFG12.0 SCALE: 3/16" = 1'-0"



6 GAS DISPENSER # SIGN LOCATION DETAIL
CFG12.0 SCALE: 3/16" = 1'-0"

- NOTES:**
- COORDINATE APRON ELEVATIONS WITH CIVIL DRAWINGS AND FIELD ELEVATIONS.
 - DISPENSER SUMPS TO BE SET IN PLACE AND USED AS A FORM TO POUR CONCRETE AROUND.
 - CONCRETE SPECIFICATIONS:
-SOIL BEARING CAPACITY REQ'D: AS PER GEOTECHNICAL REPORT
-CONC. STRENGTH: 4000psi @ 28 DAYS 3% TO 7% AIR
-TROWELLED SMOOTH FINISH WITH TWO COATS SEALING/CURING COMPOUND
-CANOPY FOOTING CONC. STRENGTH: REFER TO DRAWINGS PROVIDED BY MANUFACTURER
 - ALL EQUIPMENT INSTALLATIONS MUST COMPLY WITH MANUFACTURER'S SPECIFICATIONS.
 - ALL EQUIPMENT AND CONSTRUCTION ARE NEW AND FACILITY IS TO BE ATTENDED SELF-SERVICE.
 - REFER TO CANOPY MANUFACTURERS DRAWINGS FOR SPECIFIC CANOPY FOOTING OPTIONS.
 - REFER TO CANOPY MANUFACTURERS DRAWINGS FOR ARCHITECTURAL DETAILS ON CANOPY AND COLUMNS.
 - SEE CIVIL PLANS FOR ORIENTATION ON SITE, AND FOR TRAFFIC FLOW
 - REFER TO CANOPY DETAIL SCHEDULE ON SHEET CFG12.1 FOR ADDITIONAL INFORMATION
 - GC TO COORDINATE THE SPECIFIC LOCATIONS OF THE APRON EXPANSION JOINTS BASED ON THE FINAL POSITIVE LIMITING BARRIER DESIGN. REFER TO THE FIRE SUPPRESSION PLAN FOR THE PLB LAYOUT

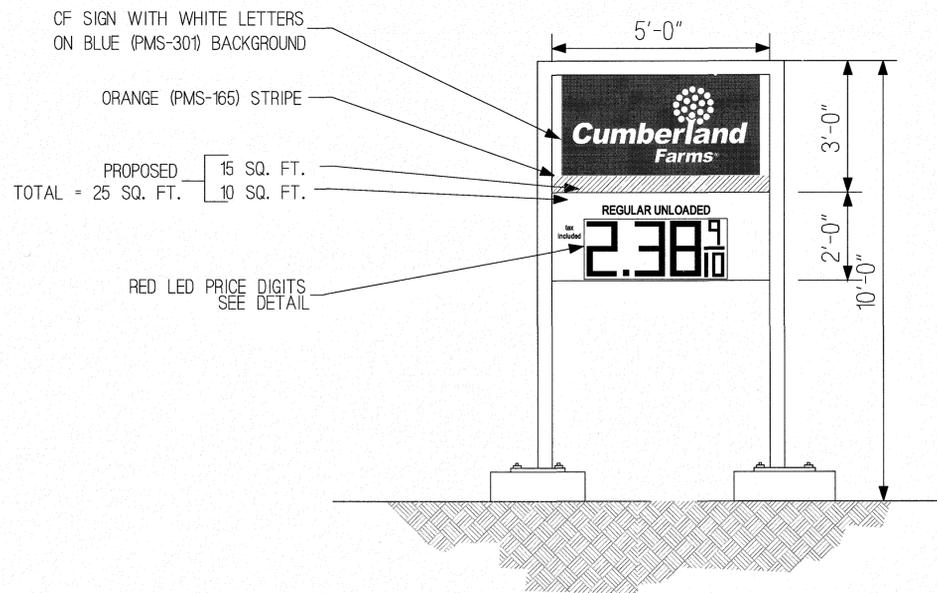
REVISIONS			V#NEW
DATE	REV. BY	DESCRIPTION	Store#NEW
			Gas Station#NEW

190 BATH ROAD, BRUNSWICK, ME

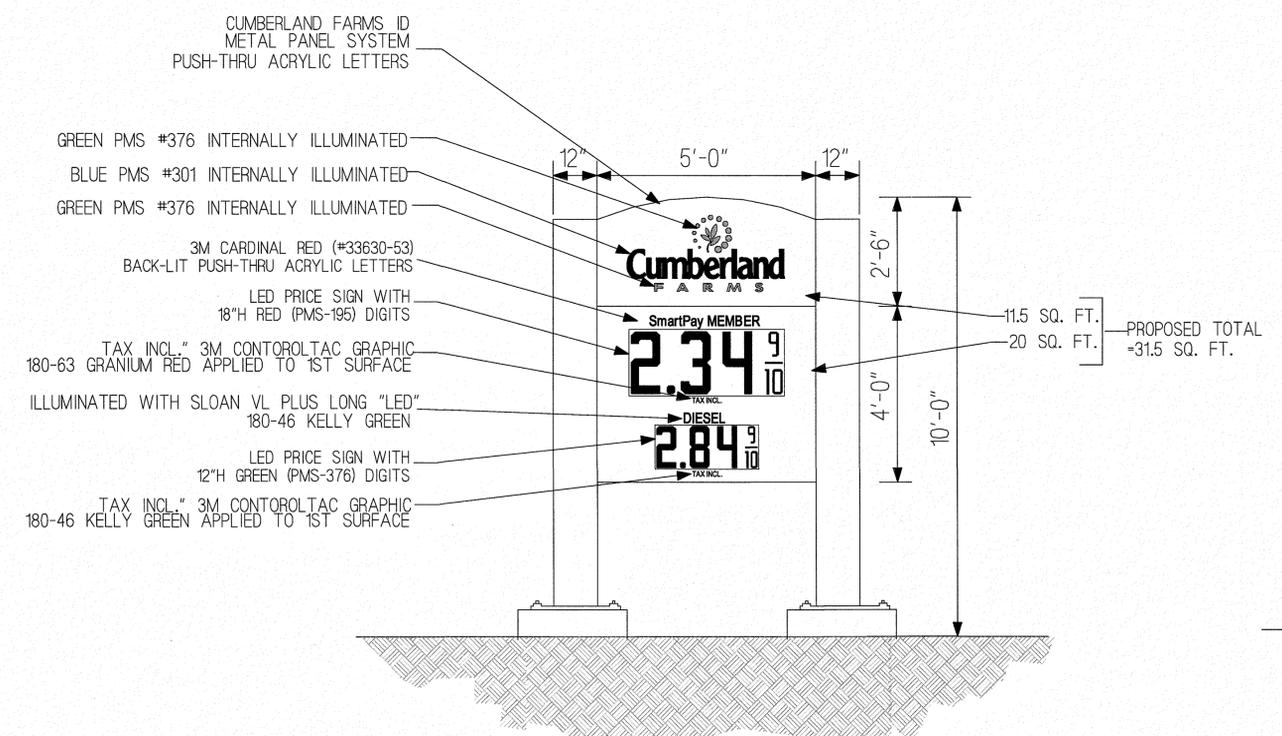
CUMBERLAND FARMS, INC.
100 Crossing Boulevard
Brunswick, Massachusetts 01702

SCALE: 3/16" = 1'-0"
DATE: 03-22-2016
FILE: x:canopyL0589
DRAWN BY: DB
CHECKED BY: **CFG12.0**

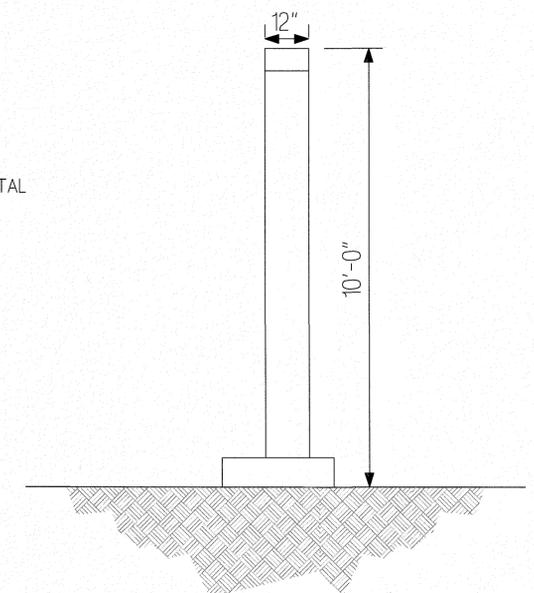
PROPOSED CANOPY PLAN & ELEVATIONS



EXISTING

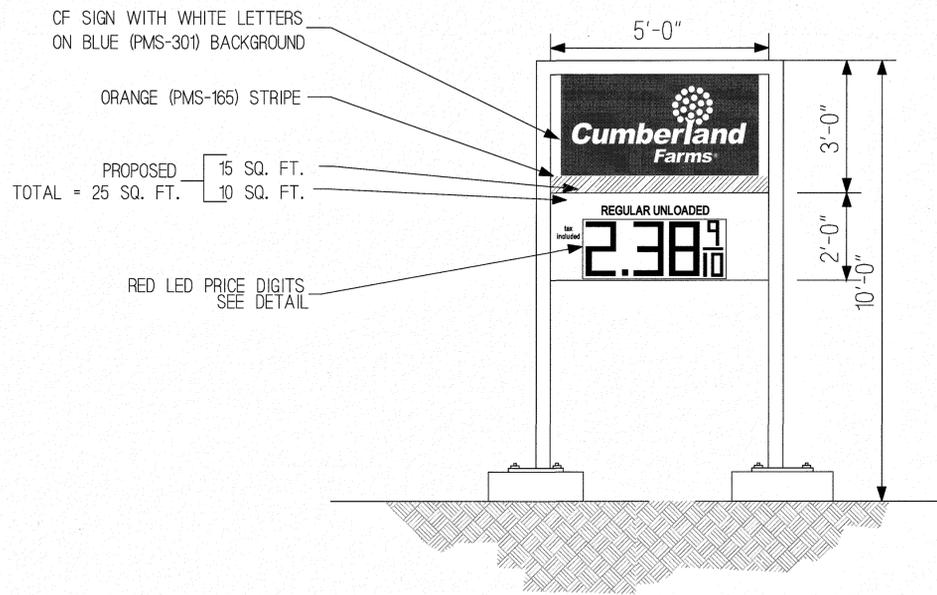


PROPOSED SIGN FRONT VIEW

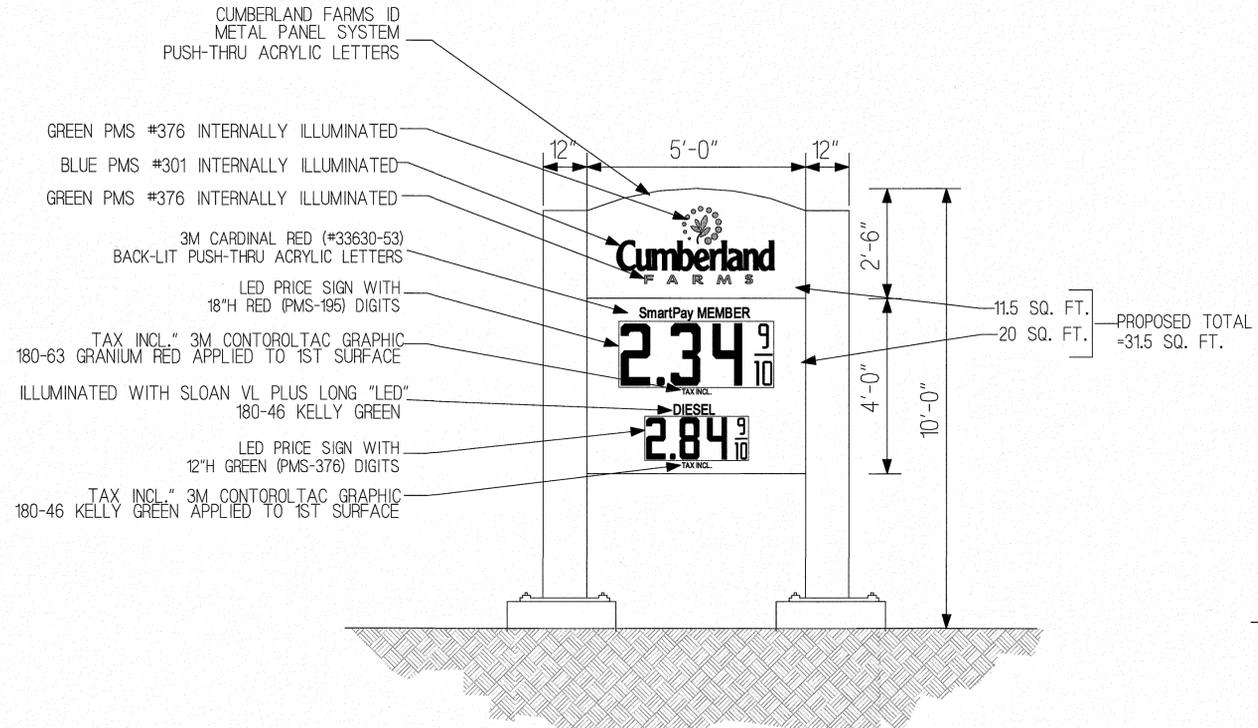


PROPOSED SIGN SIDE VIEW

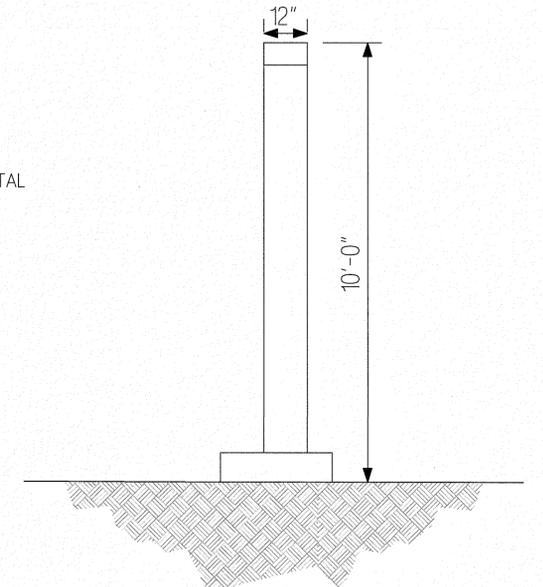
REVISIONS			L#0589	190 BATH ROAD - OLD ROUTE 1
DATE	REV. BY.	DESCRIPTION	STORE# 5585	BRUNSWICK, ME
			GAS STATION#	SCALE: 1/2" = 1'-0"
				DATE: 4-20-16
				FILE: sign.plt 10589
				DRAWN BY: DB
				CHECKED BY:
				CUMBERLAND FARMS, INC. 100 CROSSING BOULEVARD FRAMINGHAM, MASSACHUSETTS 01702
				Cumberland FARMS
				SIGN PLAN 1 CFG13.0



EXISTING



PROPOSED SIGN FRONT VIEW



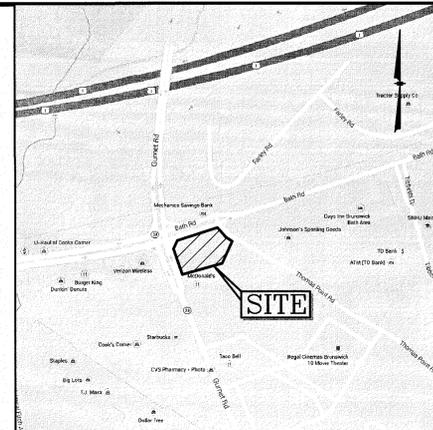
PROPOSED SIGN SIDE VIEW

REVISIONS			L#0589	190 BATH ROAD - OLD ROUTE 1
DATE	REV. BY.	DESCRIPTION	STORE# 5585	BRUNSWICK, ME
			GAS STATION#	SCALE: 1/2" = 1'-0"
				DATE: 4-20-16
				FILE: sign.plt 10589
				DRAWN BY: DB
				CHECKED BY:

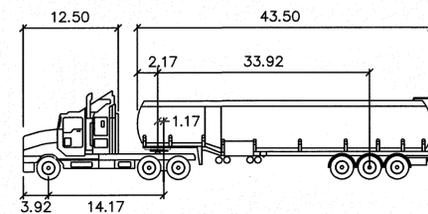
CUMBERLAND FARMS, INC.
310 CROSSING BOULEVARD
FRAMINGHAM, MASSACHUSETTS 01702

Cumberland FARMS

SIGN PLAN 2 CFG13.1



LOCATION MAP
(NOT TO SCALE)



Fuel Tanker D	feet		
Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 40.0
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		

CUMBERLAND FARMS FUEL TANKER

MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1- LOT 28
50,965 Sq.Ft.
1.170 Ac.±

PROPOSED
CONVENIENCE
STORE
4,786 SF

THOMAS POINT ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

BATH ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

ROUTE 24
(PUBLIC - VARIABLE WIDTH R.O.W.)

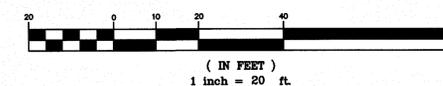
MAP CC2 LOT 53
N/F STATE OF MAINE C/O
DEPT OF TRANSPORTATION
16 STATE STATION HOUSE
AUGUSTA, ME 04333

LEGEND

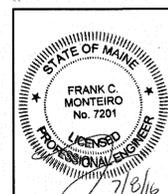
- | | |
|-----------------------------------|--------------------------------------|
| ○ IRON PIN FOUND | — TREELINE |
| □ CONCRETE BOUND FOUND | — UTILITY POLE |
| △ RAILROAD SPIKE FOUND | — DRAIN MANHOLE |
| ○ DRILL HOLE FOUND | — SEWER MANHOLE |
| — VERTICAL GRANITE CURB | — TELEPHONE MANHOLE |
| — SLOPED GRANITE CURB | — CATCH BASIN |
| — BITUMINOUS CONCRETE LIP CURBING | — WATER LINE |
| — BITUMINOUS CONCRETE BERM | — WATER VALVE |
| — OVERHEAD SERVICE WIRES | — FIRE HYDRANT |
| — DOUBLE SOLID YELLOW LINE | — GAS VALVE |
| — SINGLE SOLID WHITE LINE | — GAS LINE |
| — BROKEN WHITE LINE | — UNDERGROUND TELEPHONE LINE |
| — SIGN | — UNDERGROUND ELECTRIC AND TELEPHONE |
| — OBSERVATION WELL | |
| — TEST PIT | |
| — TEST BORING | |



GRAPHIC SCALE



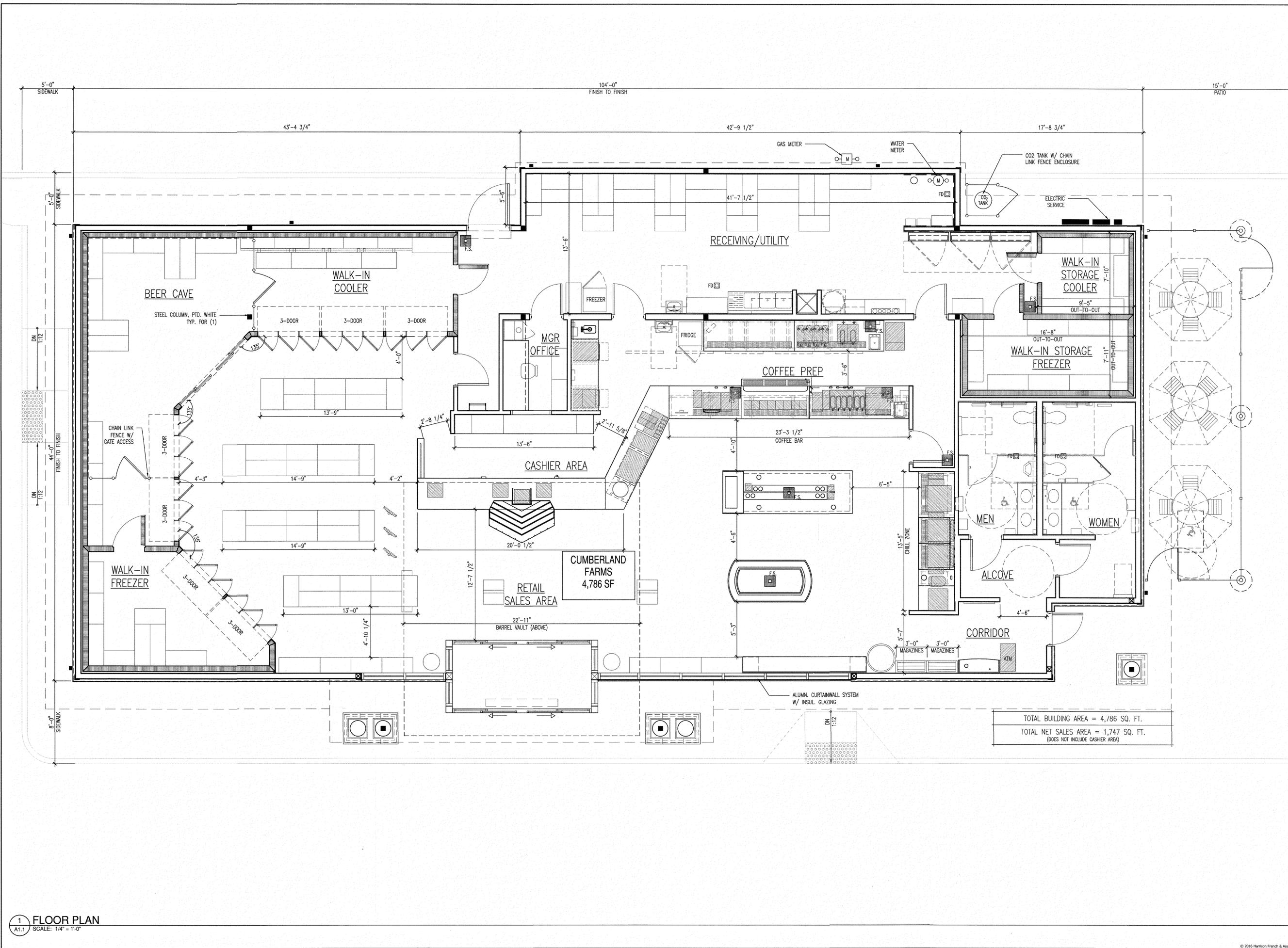
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION

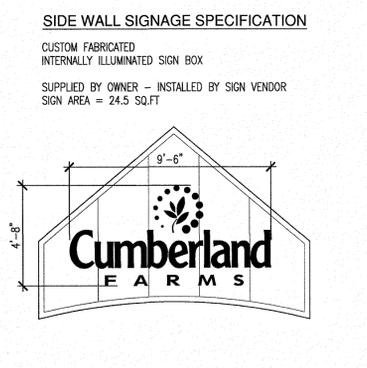
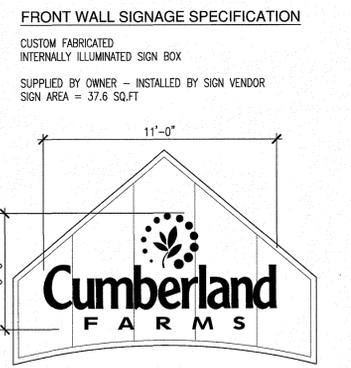
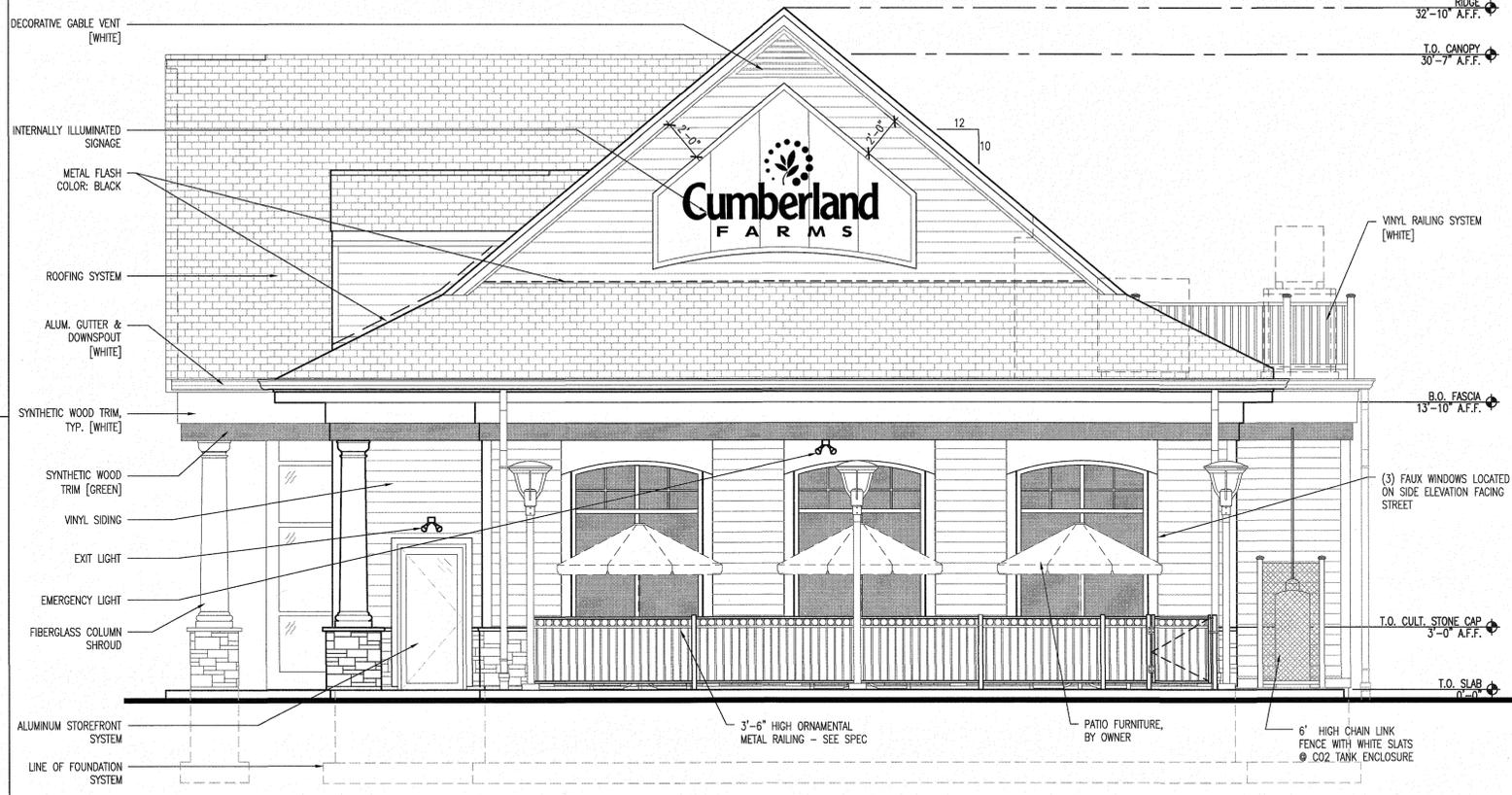
50,965 FEET
1.170 ACRES
V# L0589
Store# 5585
Gas Station# 1818

190 BATH ROAD
BRUNSWICK, MAINE 04011
SCALE: 1" = 20'
DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM
TRUCK TURN PLAN
CFG16.0



1 FLOOR PLAN
SCALE: 1/4" = 1'-0"

FINISH SCHEDULE	ANNO #	DESC.	MANUFACTURER	MODEL	COLOR	NOTES
SW-1		SYNTHETIC WOOD TRIM & FASCIA	CERTAINTEEED		PT-5; SEE A0.3	PROVIDE SCARF JOINTS ON ALL EXTERIOR SYNTHETIC WOOD TRIM. GC TO PUTTY ALL NAIL HOLES & PAINT ALL SYNTHETIC WOOD TRIM & PANELS.
SW-2		SYNTHETIC WOOD TRIM	CERTAINTEEED		PT-7; SEE A0.3	PROVIDE SCARF JOINTS ON ALL EXTERIOR SYNTHETIC WOOD TRIM. GC TO PUTTY ALL NAIL HOLES & PAINT ALL SYNTHETIC WOOD TRIM & PANELS.
FB-1		FIBERGLASS COLUMN SHROUD OR EQUAL	PACIFIC COLUMNS		PT-8; SEE A0.3	16"X9" ENDURA STONE PLAIN COLUMN ROUND SHAFT WITH TRUE ENTASIS TAPERED SMOOTH FINISH
VS-1		VINYL SIDING	CERTAINTEEED	MONOGRAM 46 DOUBLE 4"	HERRINGBONE	ROUGH CEDAR FINISH. PROVIDE ALL REQ'D ACCESSORIES AND TRIM FOR A COMPLETE INSTALLATION.
CS-1		CULTURED STONE	OWENS CORNING CULTURED STONE, LLC	COUNTRY LEDGESTONE	ECHO RIDGE	INSTALL DRYSTACK ONLY; SUPPLIED BY CFI
SHNG-1		ARCHITECTURAL ASPHALT SHINGLES	CERTAINTEEED LANDMARK	LANDMARK	COBBLESTONE GRAY	30 YEAR WARRANTY
GU-1		ALUMINUM GUTTER SYSTEM	ATAS	.032	WHITE	PROVIDE ALL ACCESSORIES REQ'D FOR A COMPLETE CONTINUOUS INSTALLATION. INSTALL PER MFG INSTRUCTIONS. ENSURE SEALED, WATERTIGHT CORNER CONNECTIONS. FLASH & SEAL TO DOWNSPOUTS AS REQ'D. PROVIDE SUPPORT STIFFENS AT MIN. 12" O.C. GUTTER SHALL BE SEAMLESS AND MIN. 6".

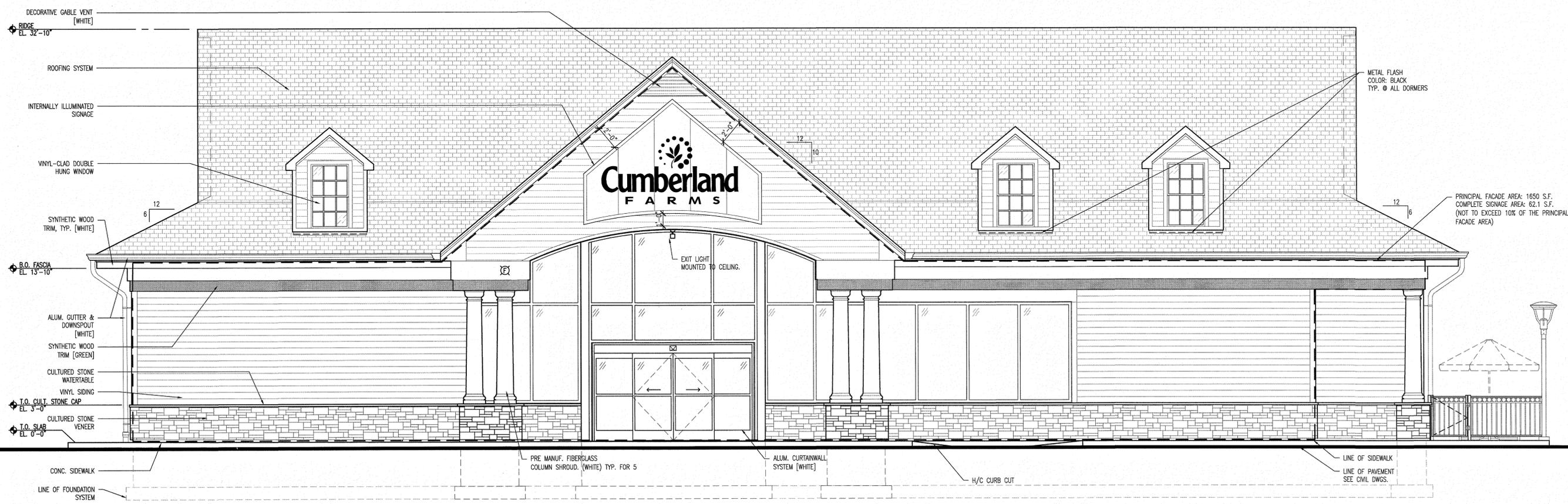


5 FINISH SCHEDULE
SCALE: NTS

2 RIGHT SIDE ELEVATION
SCALE: 1/4" = 1'-0"

4 FRONT WALL SIGNAGE DETAIL
SCALE: 1/4" = 1'-0"

3 SIDE WALL SIGNAGE DETAIL
SCALE: 1/4" = 1'-0"



1 FRONT ELEVATION
SCALE: 1/4" = 1'-0"

Project Title
New Construction:

Cumberland FARMS
Store #5585
VSH #L0589
Oracle #TBD

190 BATH ROAD
Brunswick, ME 04011
Owner
Cumberland Farms, Inc.
100 Crossing Blvd
Framingham, MA 01702
tel 508 270 1400

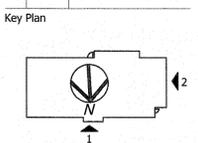
Architect
HARRISON FRENCH & ASSOCIATES, LTD
31 Haywood Street
Franklin, MA 02038
t 508.528.0770
f 508.528.9454
www.hfa-se.com

Stipulation for Reuse
THIS DRAWING WAS PREPARED FOR USE ON A SPECIFIC SITE AT BRUNSWICK, ME. CONTEMPORANEOUSLY WITH ITS ISSUE DATE ON 05/10/16 AND IT IS NOT SUITABLE FOR USE ON A DIFFERENT PROJECT SITE OR AT A LATER TIME. USE OF THIS DRAWING FOR REFERENCE OR ENGINEERING REPRODUCTION OF THIS DRAWING FOR REUSE ON ANOTHER PROJECT IS NOT AUTHORIZED AND MAY BE CONTRARY TO THE LAW.

Seal
Consultant

Revisions

No.	Date	Issued for
05/10/16	SD SUBMISSION	

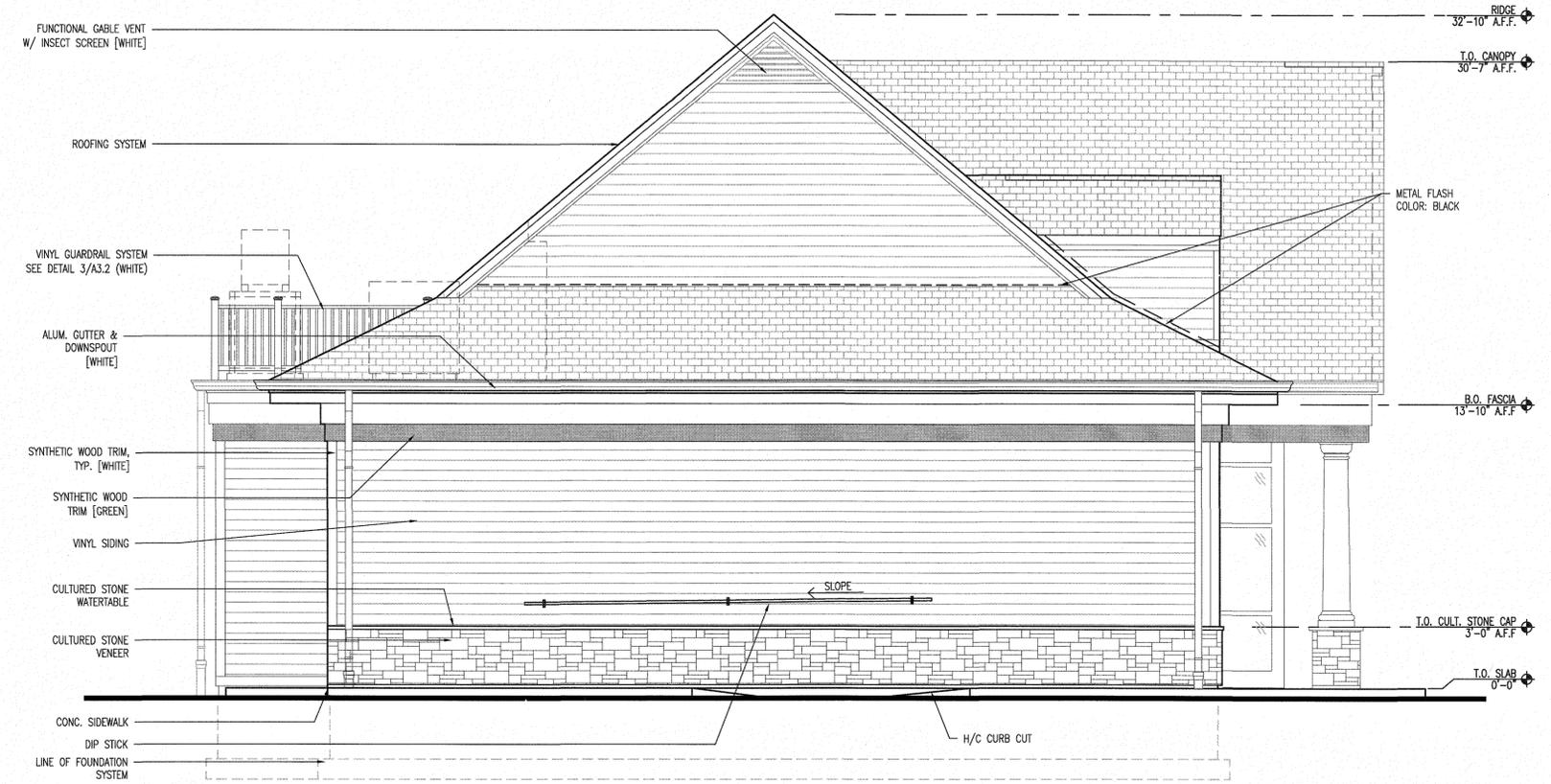
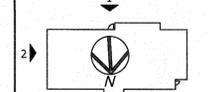


Title
EXTERIOR ELEVATIONS

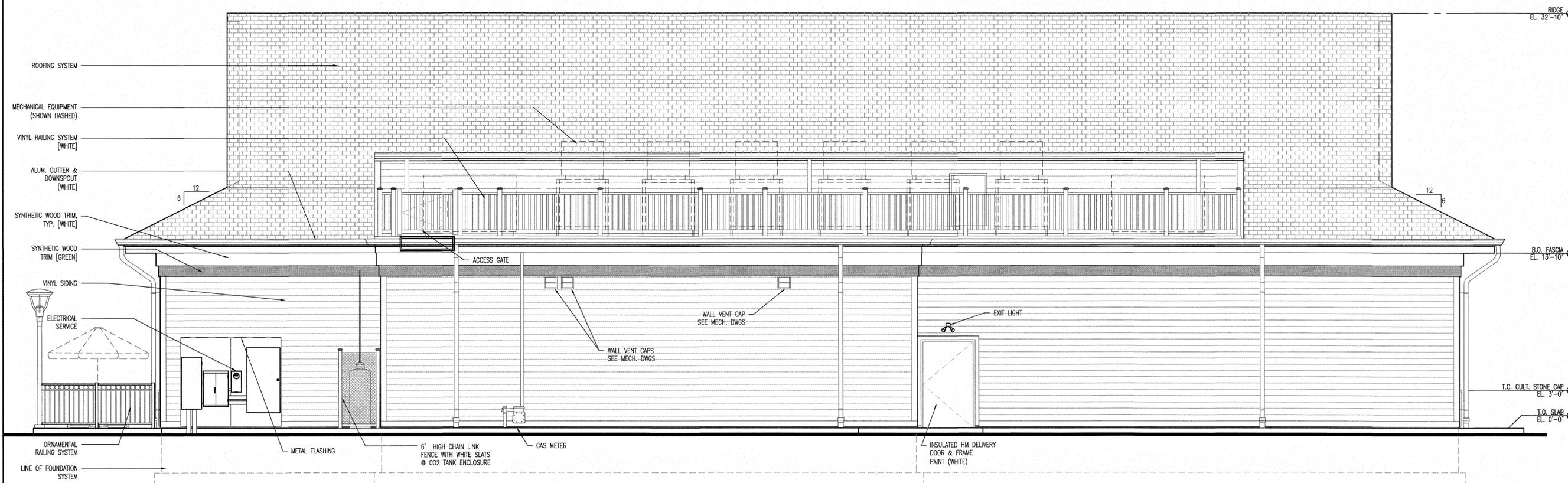
Date: 05/10/16
Drawing No.

A3.1

No.	Date	Issued for
05/10/16		SD SUBMISSION



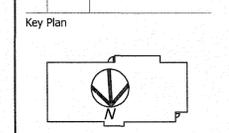
2 LEFT SIDE ELEVATION
SCALE: 1/4" = 1'-0"



1 REAR ELEVATION
SCALE: 1/4" = 1'-0"

Revisions

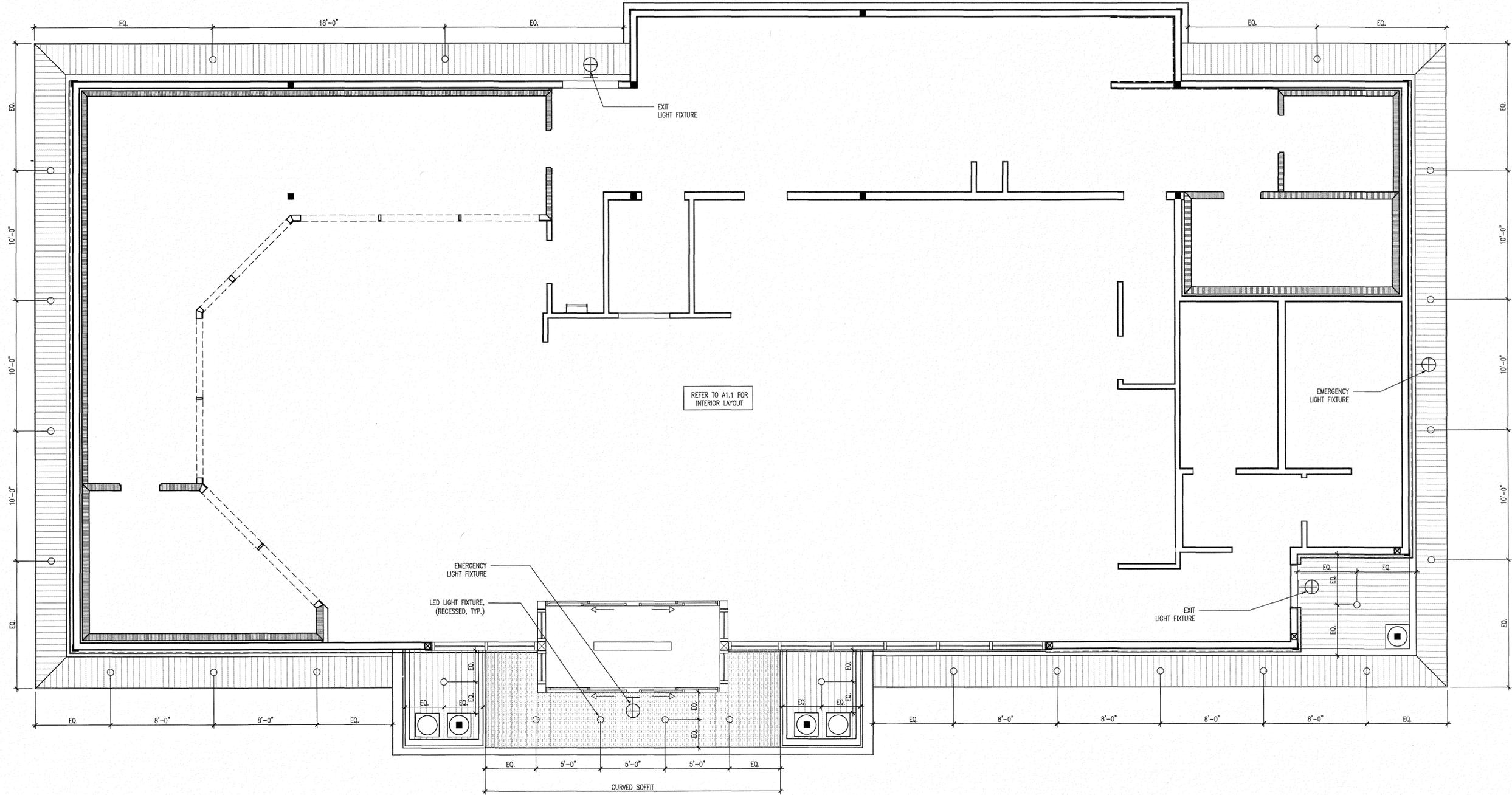
No.	Date	Issued for
05/10/16	SD SUBMISSION	



Title
EXTERIOR BUILDING LIGHTING PLAN

Date: 05/10/16
 Drawing No.

L1.1



Julie Erdman

From: John Foster
Sent: Friday, March 25, 2016 3:08 PM
To: cmt@mhfdesign.com
Cc: Jared Woolston
Subject: Town of Brunswick Survey Info on Thomas Point Rd
Attachments: ThomasPtRdSurvey1993Sht1a.pdf; SurveyReport1991RuoppTPR.pdf; BathRdThomasPtROW1961.pdf

Chris,

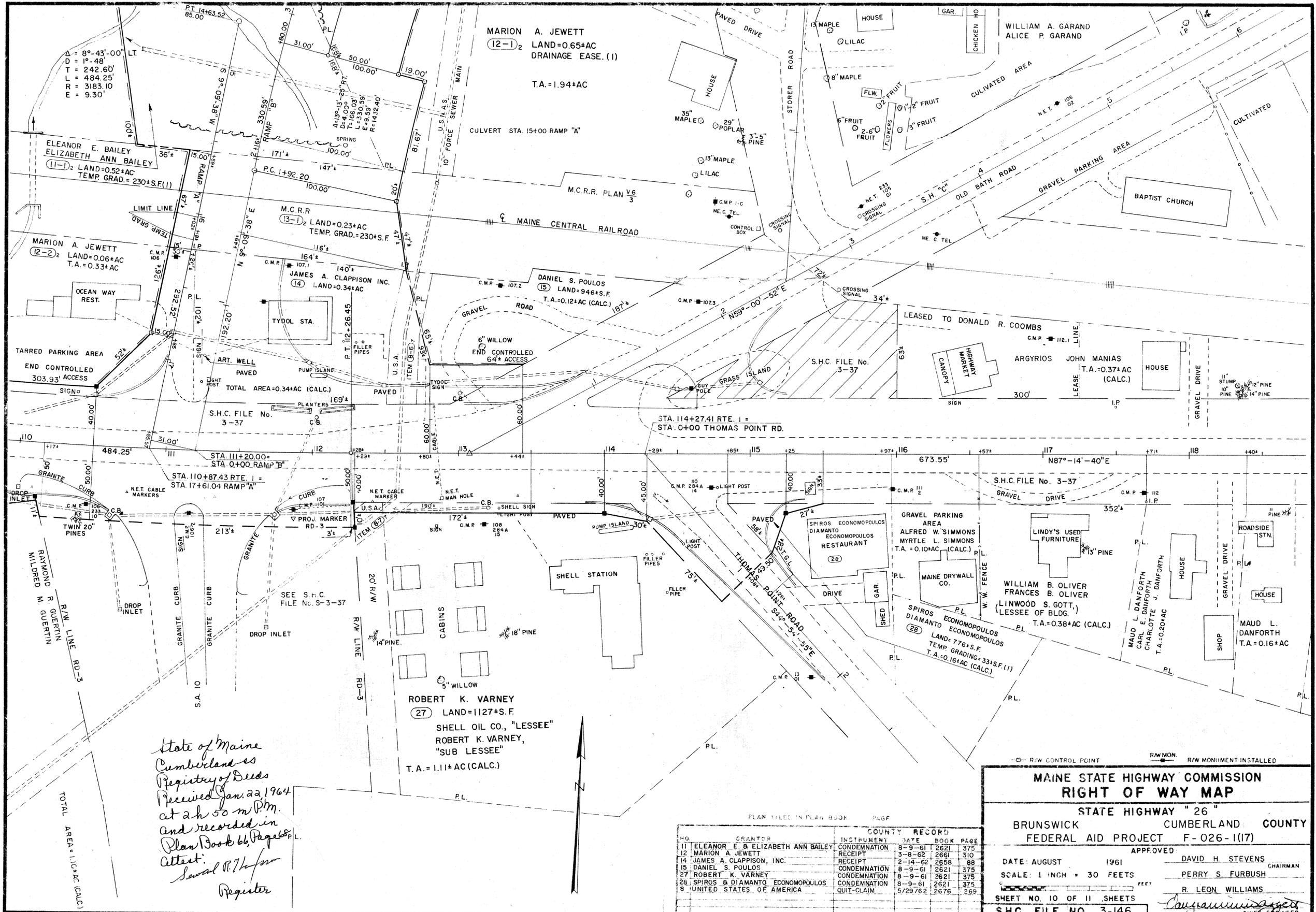
As a follow up to our meeting this morning on the Cumberland Farms project on Bath Road at Thomas Point Road I have attached a copy of our survey for the section of Thomas Point Road near your site as well as our surveyor's report from 1991. I have also attached a copy of the 1961 MDOT right-of-way plan that also shows Thomas Point Road as a 49.5 foot wide right-of-way. Would you please verify with your surveyor that the right-of-way depicted on your plan set is consistent with the attached information. Thank you, John

John Foster, Town Engineer/Public Works Director
Town of Brunswick
207.725.6654

NO.	DATE	DESCRIPTION	BY
1	10-14-60	ADDED NOTES ON BAILEY (11)	J.L.
2	4-18-62	ADDED 5' HIGH STA. 110+38, 34, 11	J.L.
3			J.L.
4			J.L.
5			J.L.

NO.	DATE	DESCRIPTION	BY
1			J.L.
2			J.L.
3			J.L.
4			J.L.
5			J.L.

State of Maine
Cumberland ss
Registry of Deeds
Received Jan. 22, 1964
at 2h 55 m P.M.
and recorded in
Plan Book 66, Page 6
Attest:
Sawal P. H. for
Register



NO.	GRANTOR	INSTRUMENT	DATE	BOOK	PAGE
11	ELEANOR E. & ELIZABETH ANN BAILEY	CONDEMNATION	8-9-61	2621	375
12	MARION A. JEWETT	RECEIPT	3-8-62	2661	310
14	JAMES A. CLAPPISON, INC.	RECEIPT	2-14-62	2658	88
15	DANIEL S. POULOS	CONDEMNATION	8-9-61	2621	375
27	ROBERT K. VARNEY	CONDEMNATION	8-9-61	2621	375
28	SPIROS & DIAMANTO ECONOMOPOULOS	CONDEMNATION	8-9-61	2621	375
8	UNITED STATES OF AMERICA	QUIT-CLAIM	5/29/62	2676	269

**MAINE STATE HIGHWAY COMMISSION
RIGHT OF WAY MAP**

STATE HIGHWAY "26"
BRUNSWICK CUMBERLAND COUNTY
FEDERAL AID PROJECT F-026-(117)

APPROVED:
DAVID H. STEVENS CHAIRMAN
PERRY S. FURBUSH
R. LEON WILLIAMS

DATE: AUGUST 1961
SCALE: 1 INCH = 30 FEET
SHEET NO. 10 OF 11 SHEETS
S.H.C. FILE NO. 3-146

Carroll Williams
CHIEF ENGINEER

07 82 22
EVITE CODE NUMBER CA
PL 35

Thomas Point Road
Summary of Right of Way Definition
by
Paul H. Ruopp, Jr., PLS
for
Town of Brunswick, Department of Public Works
August 8, 1991

Thomas Point Road is a town maintained (owned) road which runs southeasterly from the Bath Road, formerly Bath Turnpike Road, to the Adams Road in the Town of Brunswick. The road has also been known as Bull Rock Road or the road to Bull Rock Bridge. It appears to have been one of the earliest roads established in the Town of Brunswick.

The Town of Brunswick clerk's records were reviewed for a definition of Thomas Point Road. Records for roads indexed as Bull Rock Road, Thomas Point Road, Thomas Road and New Meadows Road were examined. These several names were used since they are all referenced in deeds for properties adjacent to the project area. The definitions filed under Bull Rock Road are applicable to the portion of the road much further east of the project site.

A possibility of a Town definition is in the town meeting article at Page 46 of Volume 1 clerk's records. That article refers to an acceptance of a proposed road from Capt. James Thompson's house to the Steven's River, (New Meadows River) through properties of Cornelius Thompson and Benjamin Larrabee. Unfortunately, a specific description of the road was not found in the town records. The references to James Thompson, Benjamin Larrabee and Cornelius Thompson are consistent with the lot location within the lotting scheme of the Town of Brunswick per the Proprietors Plan of Lots, see portion of lot plan attached.

The definition of the road appears to be contained in the Cumberland County Commissioners records, Volume 1 Page 69. That description defines a 4 Rod road from North Yarmouth, now Freeport, to Steven's River (New Meadows River). There are numerous courses and distances, in rods and links, without other descriptive data except for a few particular courses. One of these is the call of S45°E 248 rods along lands of Anthony Coombs and Samuel Thompson.

As shown later in this report, this abutter call is corroborated by deed research for parcels of land along the northeasterly side of Thomas Point Road. Further, the direction call for this segment also agrees with the orientation of the proprietor's lots and this portion of Thomas Point Road.

Therefore, Thomas Point Road in the project area appears to have been originally defined as a County Road, 4 Rods in width, without subsequent redefinition.

Plans by the Maine Department of Transportation indicate the right of way width of Thomas Point Road may be 3 Rods (49.5 feet). More likely, however, the width of 4 Rods (66 feet) per the County

Project 177
Thomas Point Road, Brunswick
Summary of Right of Way Definition
Page 2
August 8, 1991

Commission records is correct. In addition to the width issue, the more important question presented is not of definition but of location, or where are the right of way limits on the ground.

Monuments specifically defining the right of way may have existed at some date in the past. Presently, however, monuments specifically controlling the right of way do not exist. Physical evidence which does exist and apparently marks the limits of the properties adjoining the Thomas Point Road is not consistent with a regular definition or orientation of the road. Further, the origin and basis for those monuments is not known and not readily ascertainable. Except for one iron pipe on the northeasterly side of the road, the iron pipes which do exist were not considered to reestablish the right of way limits shown on the plans.

Extensive research has been completed by the author of this report for property surveys of the parcels of land to the northeasterly side of Thomas Point Road. This research has documented that the line dividing assessors map CC2 parcels 34 and 35 is the line between two parcels of land which were created at some date before 1767. All of the parcels southeasterly of this line to the now Adams Road originate from the property referred to as the Smith farm which is described in a deed from Samuel Thompson to Samuel Melcher on April 3, 1767, recorded Book 26 Pages 339 and 340, Cumberland County Registry of Deeds. The locus of that deed description begins at the western most corner of a piece of land that Anthony Coombs bought off Capt. James Thompson.

The parcels to the northwest of this line derive from property formerly of George Storer who deceased about 1919. Linkage from George Storer to Anthony Coombs has not been completed yet, and is beyond the scope of this project. It is anticipated that the linkage does exist to document and verify the adjoiner's deed call. It should be noted that the deed calls for Anthony Coombs and Capt. James Thompson agree with the description of definition per the County Commissioner's records.

The description in the conveyances from the heirs of George A. Storer, (Sarah C. Storer and Rufus Y. Storer), at Book 1021 Page 454 Cumberland County Registry of Deeds, calls for a boundary rock at the land of Charles R. Grover. The Charles R. Grover land was a 10⁺ acre parcel of land first described in the conveyance from James M. Smith to Winthrop B. Smith, December 28, 1872, recorded Book 433 Page 275 Cumberland County Registry of Deeds. The description in Book 433 Page 275 is deficient in some respects, but does specifically call for a stone at what is the corner in common with the Storer parcel.

Project 177
Thomas Point Road, Brunswick
Summary of Right of Way Definition
Page 3
August 8, 1991

Field investigations did not find a stone at this point, but did recover the iron pipe shown on the plan at the end of a wire fence running northeasterly from Thomas Point Road. The orientation of the fence line and distance to the Bath Road along that direction were in agreement with the distance call in the description in Book 422 Page 275. Therefore, this iron pipe was held as best representing a point on the northeasterly right of way of Thomas Point Road.

The direction of the right of way lines southeasterly from the iron pipe was set parallel with the direction of the proprietor's lot lines determined from surveys on several lots in the vicinity of this project. The basis for this is contained in the definition in the County Commissioner's records.

Northeasterly of the iron pipe the right of way line was set from the iron pipe to a point determined from Maine State Highway Commission (Department of Transportation) Right of Way Map State Highway "26" Brunswick Cumberland County, sheet 5 of 11 sheets, dated August 1961, SHC File No. 3-146, recorded Plan Book 66 Page 58 Cumberland County Registry of Deeds.

The right of way lines as shown on the plan and profile sheets and determined as described in the above analysis only represent an opinion as to most probable location of the right of way limits. However, that opinion is based upon substantial study of municipal, county and state records as well as field investigations and measurements. What is certain is that the boundaries of Thomas Point Road are doubtful or lost.

It is undesirable to publish a plan with specific boundary data for the road without the proper procedures by the Town Council to redefine and make certain the boundaries of that road. Therefore, bearings and distances for the right-of-way limits shown on the plan have not been stated because the true right-of-way limits are uncertain.

Maine Statutes, 23 MRSA 2101, provide the county and municipality with methods to make certain the boundaries of roads in such circumstances. The survey tasks necessary for a redefinition have been completed for this portion of Thomas Point Road through this project. It would be prudent for the Town of Brunswick to pursue in conjunction with the proposed reconstruction of Thomas Point Road the redefinition and monumenting of the right of way limits.

file/b:177_01.rp



William Woodside Esq. and Other Inhabitants of the
 Town of Northampton in the County of Cumberland having
 petitioned this Court at their General Sessions at Carlisle
 at Newmarket in and for the County of Cumberland that
 there had been no legal Road or High Way laid out or
 established from the Place called the Gore lying between the
 Towns of Northampton and Brunswick aforesaid to a certain
 Meadow in said Town that the Petitioners conceived it most
 necessary that such a Road should be laid out and established
 for common Convenience of Travellers passing that Way
 Wherefore the Petitioners prayed that a Commission might
 be appointed to lay out a Road of suitable Breadth from
 the same Place called the Gore to Brunswick Plain aforesaid
 and thro' the Plain to George Town at the Head of Swons
 River and also from the Road leading from Brunswick back
 to the Ferry over Androscoquin River that the Course and
 Plan of such Road be ascertained to the best of the same
 might be established as a common High Way agreeable to the
 Law in that Case made and provided

Oct 17
 Wm Woodside
 Esq. Petitioner
 John Mitchell
 Esq. Attorney
 for the Petitioners
 John Mitchell
 Esq. Attorney
 for the Respondents
 James Mitchell
 Esq. Attorney
 for the Respondents

Whereupon the Court having taken the foregoing Petition into
 Consideration and it being judged by them that the laying out
 the High Way aforesaid would be of common Convenience it was
 therefore ordered by the Court that Mr. John Lewis Esq. Mr. John
 Mitchell Esq. Mr. Thomas Mitchell Esq. and Mr. Thomas True all Justices and
 sufficient Freeholders in the Town of Northampton in said
 County be appointed a Committee to view and lay out said High
 Ways to give reasonable Notice to all Persons interested of the Time
 and Place of their Meeting and to be under Oath to perform the said
 Service according to their best Skill and Judgment with most Conve-
 nience to the Publick and least Prejudice or Damage to private
 Property; and also ascertain the Place and Course of said Road
 in the best Way and Manner they can which having done
 the said Committee or the Major part of them are to make Re-
 turn thereof to the next Court of General Sessions of the Peace to
 be held in the said County after the Service is performed under
 their Hands and Seals according to Law. And the said Committee
 having been sworn as the Law directs now Report respectively.

The Honorable his Majesty's Court of General Sessions at
 the said holden at Newmarket within and for the County of Cumberland
 on the second Tuesday of October in the said County of Cumberland
 you know that we have provided to lay out the High Ways herein mentioned and have ascertained the

Committed to
 John Mitchell
 Esq. Attorney
 for the Respondents

70
 Plans and Courses of the same by marking and stretching out the same
 as follow, viz. Beginning (the Road leading from the Gate to
 Oronoch Point, and through the Plain to the House above) in the
 Middle of the said Road about where it crosses the Line betwixt
 the Town of Brunswick, and the Gate. Hence N. 60 deg. E. 12: 07
 Hence N. 66 1/2 deg. 58. 1/2 S. 60 by Hence N. 76 E. 20 1/2. Hence N.
 80 E. 12: 11. Hence N. 66 E. 9: 13. Hence N. 84 E. 8. Hence N. 76 E.
 10: 8. Hence N. 85 E. 12. Hence N. 70 1/2 E. 6: 20. Hence S. 50 1/2 E. 9: 05.
 Hence N. 55 E. 8. Hence N. 56 E. 14. Hence N. 85 E. 12. Hence S. 80 1/2
 E. 14. Hence S. 89 E. 12: 14. Hence N. 74 E. 9. Hence N. 81 E. 12. Hence
 S. 87 E. 20. Hence S. 87 1/2 E. 8. Hence S. 65 E. 14. 5. Hence N. 67 1/2 E. 13: 2
 Hence S. 87 E. 12. Hence S. 70 E. 14. Hence N. 77 E. 7. Hence N. 57 E. 5: 19
 Hence S. 85 E. 9: 2. Hence S. 76 E. 8: 21. Hence N. 62 1/2 E. 19. Hence
 East 16 Rods. Hence N. 73 - E. 14: 6. Hence S. 73 E. 26: 07. Hence
 N. 71 E. 15. Hence N. 73 E. 8: 4. Hence S. 86 E. 15: 13. Hence N. 77 E.
 12: 12. Hence East 14. Hence N. 73 E. 24. Hence N. 81 E. 13: 8. Hence
 N. 68 1/2 E. 20. Hence S. 86 E. 12: 14. Hence S. 88 E. 16. Hence S. 86 E.
 20: 19. Hence S. 81 E. 14: 10. Hence N. 80 E. 9: 14. Hence S. 74 E. 11
 Hence S. 67 E. 12: 16. Hence S. 72 E. 12. Hence S. 85 E. 8. Hence S. 75 E.
 10: 9. Hence S. 78 1/2 E. 19. Hence S. 67 E. 21: 7. Hence S. 75 E. 9: 3. Hence
 S. 62 E. 12: 22. Hence S. 80 E. 9: 9. Hence N. 73 E. 12: 16. Hence East 13
 Hence N. 62 1/2 E. 12. Hence N. 49 E. 8: 14. Hence N. 46 E. 9: 15. Hence
 N. 34 E. 10: 20. Hence S. 30 1/2 E. 12: 13. Hence N. 35 E. 10. Hence S. 33
 E. 14: 19. Hence N. 82 E. 6. Hence S. 84 E. 7: 20. Hence N. 78 1/2 E. 12: 15
 Hence N. 81 E. 11: 12. Hence N. 63 E. 17: 7. Hence S. 60 E. 9: 17. Hence
 N. 50 E. 12: 07. Hence N. 47 E. 11: 6. Hence N. 38 E. 11. Hence N. 25 E. 8: 6
 Hence N. 30 1/2 E. 8. Hence N. 50 1/2 E. 9: 15. Hence N. 64 E. 14. Hence
 S. 87 E. 8: 07. Hence S. 86 E. 22. Hence N. 84 E. 12. - Hence N. 67 1/2 E.
 12: 9. Hence N. 85 1/2 E. 24: 12. Hence S. 74 E. 11: 9. Hence S. 65 E. 18: 10
 Hence S. 77 E. 16. Hence S. 67 1/2 E. 10: 19. Hence S. 71 E. 28: 10. Hence S.
 82 E. 12: 15. Hence S. 75 E. 23. Hence S. 51 E. 11. Hence S. 50 E. 16. Hence S.
 52 E. 12. Hence S. 64 E. 8. Hence S. 55 1/2 E. 9. Hence S. 60 E. 19. Hence
 S. 50 E. 6. Hence S. 44 E. 13: 7. Hence S. 31 E. 12: 20. Hence S. 73 E. 10: 8
 Hence N. 82 E. 9: 6. Hence N. 85 E. 9: 8. Hence S. 74 E. 20: 10. Hence
 S. 78 E. 20. Hence S. 70 E. 10: 14. Hence S. 88 E. 14: 14. Hence N. 65 E. 12: 20
 Hence N. 95 E. 16. Hence S. 74 E. 19. Hence S. 55 E. 20. Hence S. 40 E. 30
 Hence S. 29 E. 12. Hence N. 80 1/2 E. 21: 9. Hence East 18: 10. Hence N. 84
 E. 15. Hence S. 87 E. 19. Hence S. 43 E. 17: 12. Hence S. 40 E. 22: 10. Hence
 S. 35 E. 15: 9. Hence S. 57 E. 16. Hence S. 82 E. 14. Hence S. 73 E. 25: 10
 Hence S. 70 E. 22. Hence S. 60 E. 15: 20. Hence S. 48 E. 54. Hence S. 52 1/2 E.
 20. Hence S. 55 E. 17: 5. Hence N. 50 E. 14. - Hence S. 73 1/2 E. 15: 12. Hence

N 61 E 26 Hence N 87 E 26 Hence N 38 1/2 E 26 10/16
 four courses last mentioned are on a line between
 two deaths of the one N 1/2 E 12:10 Hence N 20 E 14 Hence N
 38 E 12 Hence N 51 E 10:10 Hence N 57 1/2 E 14 Hence N 57 1/2 E 20
 Hence N 77 1/2 E 14:9 Hence N 57 E 23:12 Hence N 55 E 24 1/3 on the
 line between Daniel Thomson and Anthony Coombs (Jnr) Hence
 S 2 W 7:21 Hence N 45 E 10:21 Hence S 27 E 11:4 Hence S 11 E 14
 Hence S 60 1/2 E 11:11 Hence S 62 E 17:16 Hence S 75 E 8:20
 Hence S 57 E 13 Hence S 57 E 8 Hence N 48 E 30 Hence S 61 E 18:9
 Hence S 60 E 20:10 (10 rods at this distance is over a bridge and
 caucused over a piece of marsh belonging to Baron Stintley Esq:
 upon which marsh we think it reasonable he should be allowed
 to join his line to the survey (bridge) Hence N 50 E 11:2
 Hence S 80 E 13:2 Hence S 20 E 12:14 Hence S 74 E 22:17
 Hence S 39 1/2 E 7:17 Hence S 67 1/2 E 6:7 Hence N 52 E 11:20
 Hence S 52 E 10:24 Hence S 40 E 37:10 Hence N 67 E 12 Hence
 N 41 1/2 E 20:8 Hence N 23 E 24:4 Hence N 16 1/2 E 72:12 Hence
 N 16 E 37:10 Hence N 20 E 7:4 Hence N 26 E 12 Hence N 12 1/2
 E 17:20 Hence N 12 E 34:14 Hence N 40 E 40 Hence N 37 E 30
 Hence S 41 E 40 Hence N 52 E 16:4 Hence N 24 E 17:5 Hence
 N 30 E 19 Hence N 47 E 12 Hence N 19 E 12:20 Hence N 20 E
 24:8 Hence N 14 E 26:10 Hence N 10 E 24 Hence N 15 E 35:21
 Hence N 12 E 53:6 Hence N 26 E 7:20 Hence N 12 E 37:2 Hence
 N 8 E 12:17 Hence N 22 1/2 E 50:22 Hence N 8 E 13 Hence N 7 1/2 E
 25:13 Hence N 32 E 27:8 Hence N 35 E 28:20 Hence N 37 E
 29 Hence N 37 E 19 Hence N 48 E 10:15 Hence N 30 E 16:22
 Hence N 52 E 22:20 Hence S 28 E 35 Hence N 58 E 8:15 Hence
 S 73 E 17:17 to Swons River come distance below the bridge

where the bridge now is we being informed that George Town can-
 not bring a road to meet it at that place by reason of high rocky
 land the high way to be four rods in width and to lay two rods
 on each side the foregoing lines and stations excepting the
 last distance which is above the marsh to the river which we
 have stated out two rods wide. — The other high way from
 the road leading from Brunswick north to the Ferry over Andros-
 copyn River we have proposed to lay out as follows viz Coppen
 the middle of the Ferry place so called at Andros copyn
 River was high in the Marsh Hence S 57 1/2 W 4:4 1/2 Hence
 N 72 W 18: Hence S 87 W 14 Hence S 57 W 11:15 Hence S 82 W
 16:12 Hence S 72 W 8:12 Hence S 68 W 20:14 Hence S 72 W 24
 Hence S 40 W 14 Hence S 50 W 13:4 Hence S 47 W 4:4 1/2 to
 the

Comities
 Andros copyn
 from Brunswick
 to the Andros copyn
 River



Stevens or New Meadows River

- 58. Joseph Smith 56 1/4 a.
- 57. Joseph Smith 56 1/4 a.
- 56. Joseph Smith 57 a.
- 55. Joseph Smith 57 a.
- 54. Tobias Ham 57 a.
- 53. Jacob Sharpe 61 1/2 a.
- 52. Thomas Gray 62 1/2 a.
- 51. Chas. Cassida 61 1/2 a.
- 50. Benjamin Thompson 61 1/2 a.
- 49. David Thompson 72 1/2 a.
- 48. Philp Jennings 85 1/4 a.
- 47. Samuel Hinckley 85 1/4 a.
- 46. John Whitney 86 1/4 a.
- 45. Benjamin Parker 86 1/4 a.
- 44. Aaron Hinckley 86 1/4 a.
- 43. Seth Hinckley 86 1/4 a.
- 42. Sidney Hinckley 94 1/4 a.
- 41. Emma Hinckley 97 1/2 a.
- 40. Alexander Thompson 100 a.
- 39. Samuel Whitney 100 a.
- 38. Cornelius Thompson 100 a.
- 37. Benjamin Karrobee 100 a.
- 36. Benjamin Karrobee 100 a.
- 35. Benjamin Karrobee 100 a.
- 34. Samuel Hinckley 100 a.
- 33. John Barrows 100 a.
- 32. John Barrows 100 a.
- 31. John Barrows 100 a.
- 30. Saml Hinckley 100 a.
- 29. Jacob Eaton 100 a.
- 28. Peter Coombs 100 a.
- 27. Peter Coombs 100 a.
- 26. Peter Coombs 100 a.
- 25. Isaac Snow 100 a.
- 24. John Jordan 100 a.
- 23. James Jordan 100 a.
- 22. Peter Coombs 100 a.
- 21. George Coombs 100 a.
- 20. George Coombs 100 a.
- 19. George Coombs 100 a.
- 18. Bothshelo 100 a.
- 17. Thomas Berry 100 a.
- 16. Peter Coombs 100 a.
- 15. Peter Woodword 100 a.
- 14. John Coombs 100 a.
- 13. John Coombs 100 a.
- 12. Peter Coombs Jr 100 a.
- 11. Peter Coombs Jr 100 a.
- 10. Peter Coombs Jr 100 a.
- 9. Peter Coombs Jr 100 a.

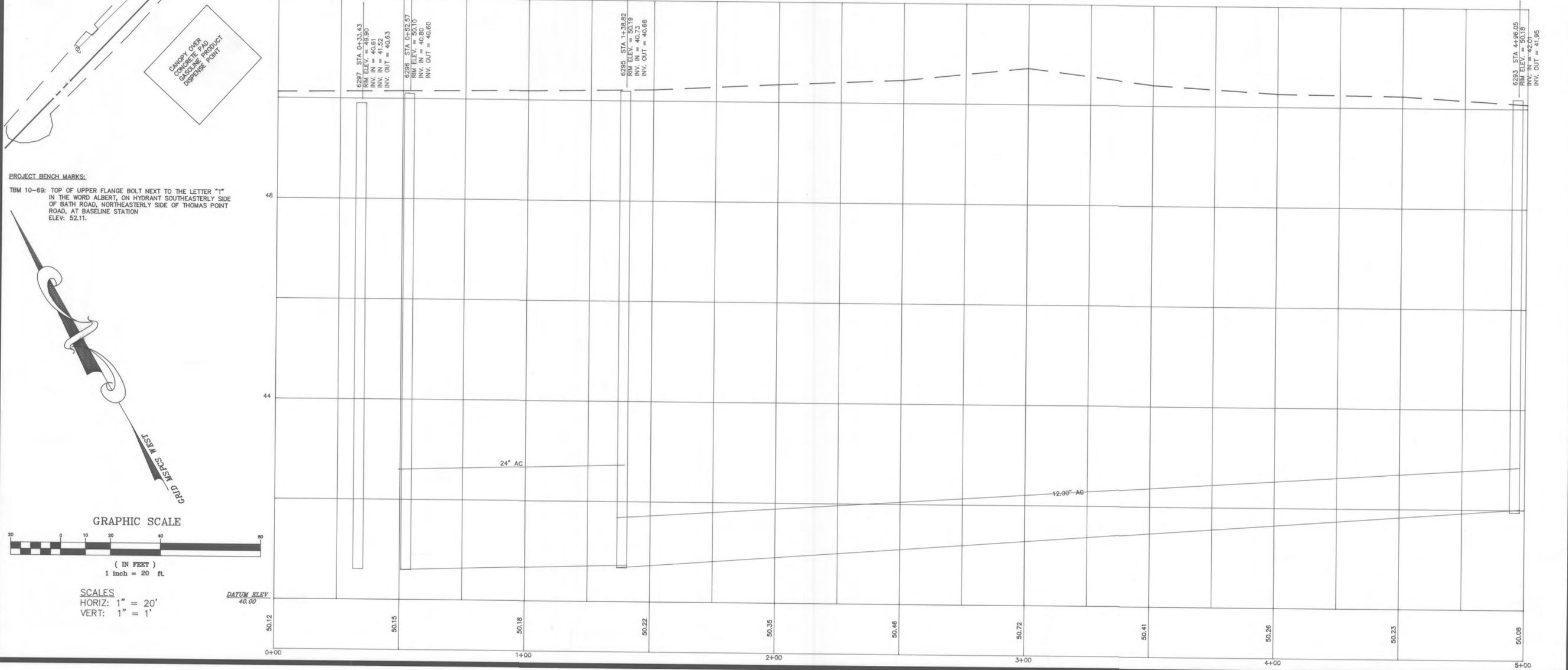
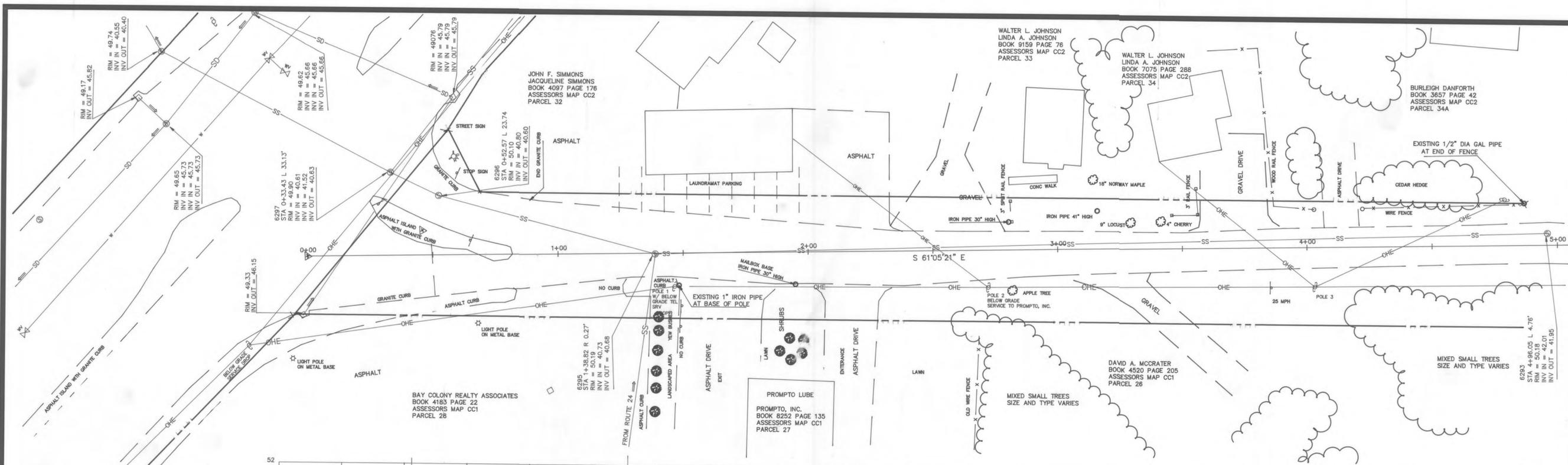
288 Acres.
Town
Commonage
1000 Acres.

Main Point

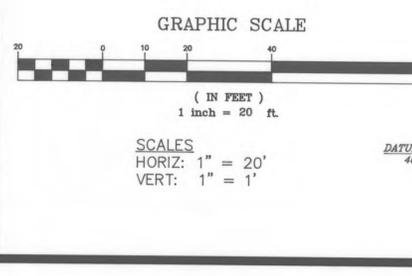
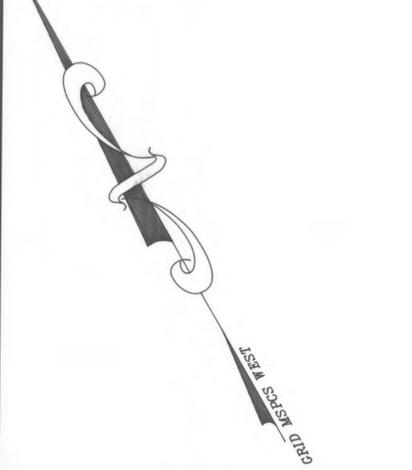
Mericonaeq Neck

Gurnett pt

- 58. Stephen Staples 100 a.
- 57. Stephen Staples 100 a.
- 56. Stephen Staples 100 a.
- 55. Stephen Staples 100 a.
- 54. Stephen Staples 100 a.
- 53. Stephen Staples 100 a.
- 52. Stephen Staples 100 a.
- 51. Stephen Staples 100 a.
- 50. Stephen Staples 100 a.
- 49. Stephen Staples 100 a.
- 48. Stephen Staples 100 a.
- 47. Stephen Staples 100 a.
- 46. Stephen Staples 100 a.
- 45. Stephen Staples 100 a.
- 44. Stephen Staples 100 a.
- 43. Stephen Staples 100 a.
- 42. Stephen Staples 100 a.
- 41. Stephen Staples 100 a.
- 40. Stephen Staples 100 a.
- 39. Stephen Staples 100 a.
- 38. Stephen Staples 100 a.
- 37. Stephen Staples 100 a.
- 36. Stephen Staples 100 a.
- 35. Stephen Staples 100 a.
- 34. Stephen Staples 100 a.
- 33. Stephen Staples 100 a.
- 32. Stephen Staples 100 a.
- 31. Stephen Staples 100 a.
- 30. Stephen Staples 100 a.
- 29. Stephen Staples 100 a.
- 28. Stephen Staples 100 a.
- 27. Stephen Staples 100 a.
- 26. Stephen Staples 100 a.
- 25. Stephen Staples 100 a.
- 24. Stephen Staples 100 a.
- 23. Stephen Staples 100 a.
- 22. Stephen Staples 100 a.
- 21. Stephen Staples 100 a.
- 20. Stephen Staples 100 a.
- 19. Stephen Staples 100 a.
- 18. Stephen Staples 100 a.
- 17. Stephen Staples 100 a.
- 16. Stephen Staples 100 a.
- 15. Stephen Staples 100 a.
- 14. Stephen Staples 100 a.
- 13. Stephen Staples 100 a.
- 12. Stephen Staples 100 a.
- 11. Stephen Staples 100 a.
- 10. Stephen Staples 100 a.
- 9. Stephen Staples 100 a.
- 8. Stephen Staples 100 a.
- 7. Stephen Staples 100 a.
- 6. Stephen Staples 100 a.
- 5. Stephen Staples 100 a.
- 4. Stephen Staples 100 a.
- 3. Stephen Staples 100 a.
- 2. Stephen Staples 100 a.
- 1. Stephen Staples 100 a.



PROJECT BENCH MARKS:
 TBM 10-69: TOP OF UPPER FLANGE BOLT NEXT TO THE LETTER "T" IN THE WORD ALBERT, ON HYDRANT SOUTHEASTERLY SIDE OF BATH ROAD, NORTHEASTERLY SIDE OF THOMAS POINT ROAD, AT BASELINE STATION ELEV: 52.11.



APPROVED	REVISIONS
NO	
DWG BY: PRR DATE: 2 JUL 1991	CHD BY: PRR DATE: 8 AUG 1991
APPROV BY: PRR DATE: 8 AUG 1991	SCALE: AS NOTED
B OK NO: 22.24	PROJECT NO: 177



SITELINES, INC.
 ENGINEERS PLANNERS SURVEYORS
 SUITE 202 FORT ANDROSS 14 MAINE ST. BRUNSWICK, MAINE 04011
 (207) 725-1200 FAX: (207) 725-1114

THOMAS POINT ROAD
 STATION 0+0 TO 5+00
 BRUNSWICK, MAINE
 FOR
 DEPARTMENT OF PUBLIC WORKS
 TOWN OF BRUNSWICK

DRAFT Planning Board Input to Town Council Regarding Potential Use of a Tax-Acquired Property, 946 Mere Point Road

Prior to deciding whether to retain ownership of a tax-acquired parcel, 946 Mere Point Road, the Brunswick Town Council requested input from the Recreation Commission, Conservation Commission, Marine Resources Committee, Rivers and Coastal Waters Commission and the Planning Board regarding its potential use. A vacant residential structure is currently located on the 4+ acre waterfront parcel, located within a residential neighborhood.

The Planning Board review focused on guidance provided by the 2008 Comprehensive Plan and the current Zoning Ordinance requirements for the Coastal Protection 1 Zoning District. It did not make a recommendation as to whether or not the Town Council should retain ownership since that is to be decided by Town Council.

The Planning Board offers the following for Town Council consideration:

Should Town Council retain ownership:

- Any use would be considered a municipal use. A municipal use is permitted within any district and must comply with dimensional and density standards for the applicable district.
- The parcel is located within the Coastal Protection 1 District, a rural area zoning district. It is not a Residential or Town Residential District, all located with the Town's defined growth area. Consequently, a Special Permit is not required for a change of use from a residential to nonresidential use as has been implied during public comment.
- Any nonresidential use will need a change of use permit and may need to go through the development review process. Consideration should be given to relocating the existing access to a more centralized location, as well as other neighborhood protections including cut-off lighting and the provision of off-street parking.
- Any change of use shall be in compliance with NRPZ standards.

Should Town Council sell the parcel:

- Establish protective covenants for the onsite grave site as part of the conveyance documents.
- Establish covenants for the protection of wildlife habitat.
- Consider dedicating sale proceeds and future taxes from the parcel to improving the utilization of existing town-owned waterfront properties.

**BRUNSWICK PLANNING BOARD
MEETING MINUTES
JUNE 14, 2016**

MEMBERS PRESENT: Chair Charlie Frizzle, Vice Chair Margaret Wilson, Bill Dana, Jeremy Evans, Sande Updegraph, and Richard Visser

MEMBERS ABSENT: Jane Arbuckle

STAFF PRESENT: Anna Breinich, Director of Planning and Development; Jeff Hutchinson, Codes Enforcement Officer; Amanda Perkins, Bowdoin College Summer Intern

A meeting of the Brunswick Planning Board was held on Tuesday, June 14, 2016, in Town Council Chambers, 85 Union Street. Chair Charlie Frizzle called the meeting to order at 7:00 P.M.

1. Case #16-019 Maine Street Station Site Plan Approval Extension: The Planning Board will review and take action on a Site Plan Approval Extension request submitted by authorized representatives, Sitelines, PA, for JHR Development of Maine, LLC for Maine Street Station which was approved on June 24, 2008 and two-year extension approvals on April 27, 2010, May 22, 2012 and May 13, 2014, respectively. The most recent approval is scheduled to expire on June 24, 2016. The remaining two (2) recorded lots are located at 16 Noble Street (building lot 5) and the corner of Station Avenue and Union Street (building lot 6) within the Town Center 1 (TC1) Zoning District, (Map U16, Lots 105 and 106).

Charlie Frizzle introduced the application for site plan reapproval extension. Curt Neufeld, of Sitelines, added that there are only two lots left to be developed. One lot is currently being used as a parking lot and may not be useable. The other, with access off Noble Street is not dead. Richard Visser asked about the parking lot area. Curt replied that the area is being used as a parking lot, but the original proposal did have a building drawn at this location and this is still the intent. Curt noted that the Noble Street lot does include shared parking. Sande Updegraph asked if the Noble Street lot would displace anything that is currently there. Anna Breinich replied that this is one building that was apartments and was purchased and made part of the original Maine Street Station plan and subdivision.

MOTION BY BILL DANA TO APPROVE THE SITE PLAN FOR JHR DEVELOPMENT OF MAINE, LLC FOR MAINE STREET STATION. MOTION SECONDED BY SANDE UPDEGRAPH, APPROVED UNANIMOUSLY.

2. Presentation by the Zoning Ordinance Rewrite Committee of the draft Zoning Ordinance for Planning Board review

Charlie Frizzle provided a brief overview of where the current Zoning Ordinance is and what needs to be done in order to address issues within the current ordinance. Charlie said that the ordinance has not been updated since 1997 and does not reflect the 2008 Brunswick Comprehensive Plan requirements. In addition, between now and 2008, Brunswick has gained a

major development opportunity with closure of the Navy base and creation of Brunswick Landing. Charlie said that in 2013, Brunswick began the rewrite process with the goal of reflecting the planning direction by both the Comprehensive Plan and the Brunswick Landing Planning process, make the document more user friendly, simplify the structures of the zoning districts and their uses, better integrate controls for Brunswick Landing with those of the rest of the Town, taking a look at best practices being used elsewhere and ensure that the ordinance complies with Maine State Law. Charlie reviewed those involved in the makeup of the Zoning Ordinance Committee and said that they have held over 100 work sessions and public forums, there has been the creation of a Facebook page and said that staff has received well over 100 public written comments with regards to the update. Charlie explained the analysis done by Clarion. In July 2014 the first publicly reviewed draft was published. A second draft was issued in July 2015. Tonight, a draft has been issued for Planning Board review. Charlie said that that there are 3 parts yet to be completed: Stormwater Management, Shoreland Protection, and Signs, all of which will be forthcoming. Charlie explained the adoption process. Anna Breinich reviewed the highlights and changes within the draft Zoning Ordinance.

Curt Neufeld of Sitelines, said that this draft has been a major step forward and even though he salutes the idea of eliminating minimum lot sizes, pointed out that if someone wanted to add accessory units in Downtown, it would not be possible with a maximum density per dwelling unit of 4,000 sq. ft. Anna Breinich replied that in downtown area, there are no dimensional standards. Curt asked if for accessory units at higher density, are there any provisions in the ordinance for parking. Margaret Wilson replied that they have loosened the parking requirements. Curt asked for ZORC to consider parking stall width from 26 feet to 24 feet.

3. Other Business: No other business.

4. Approval of Minutes:

MOTION BY BILL DANA TO APPROVE THE MINUTES OF JANUARY 12, 2016.
MOTION SECONDED BY RICHARD VISSER, APPROVED UNANIMOUS AMONG THOSE PRESENT.

MOTION BY JEREMY EVANS TO APPROVE THE MINUTES OF MARCH 22, 2016.
MOTION SECONDED BY BILL DANA, APPROVED UNANIMOUS AMONG THOSE PRESENT.

MOTION BY RICHARD VISSER TO APPROVE THE MINUTES OF APRIL 5, 2016.
MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

MOTION BY MARGARET WILSON TO APPROVE THE MINUTES OF APRIL 26, 2016.
MOTION SECONDED BY BILL DANA, APPROVED UNANIMOUSLY.

MOTION BY SANDE UPDEGRAPH TO APPROVE THE MINUTES OF MAY 10, 2016.
MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

Adjourn

This meeting was adjourned at 8:29 P.M.

Respectfully Submitted,

Tonya Jenusaitis

Recording Secretary

**BRUNSWICK PLANNING BOARD
MEETING MINUTES
JUNE 28, 2016**

MEMBERS PRESENT: Chair Charlie Frizzle, Jane Arbuckle, Bill Dana, Jeremy Evans, and Richard Visser

MEMBERS ABSENT: Vice Chair Margaret Wilson and Sande Updegraph,

STAFF PRESENT: Jared Woolston, Town Planner

A meeting of the Brunswick Planning Board was held on Tuesday, June 28, 2016, in Town Council Chambers, 85 Union Street. Chair Charlie Frizzle called the meeting to order at 7:00 P.M.

1. Case #16-010 Beacon Ridge Subdivision: The Planning Board will review and take action on a Sketch Plan Major Development Review application submitted by authorized representatives from Site Design Associates for Ecopath Developers, LLC, to revise a Sketch Plan that was approved by the Planning Board on April 5, 2016 with the construction of a 1,200 linear foot access road for a single family residential subdivision comprised of twenty four (24) lots, located in the R6 (Cook's Corner Neighborhood); Cooks Corner Zoning District; and Natural Resource Protection Zone (NRPZ) (Map 42, Lot 16).

Jared Woolston introduced the application for Beacon Ridge Subdivision and reviewed the project summary.

Tom Saucier with Sight Designs, reviewed the site and said that after discussions with the Sewer District, the subdivision will have public sewer. This has allowed the applicant to rethink the layout of the subdivision and they are now proposing a 24 unit subdivision. Tom said that they will include a walking trail to the remaining property and have included a designated open space, roughly 5 acres, for subdivision requirements. Tom said that they have mapped the wetlands and that most of the lots do not have wetlands on them. Tom pointed out that Lot 5 does have some wetlands and although they could obtain a permit from DEP to fill this, they have drawn the building envelope to exclude the wetlands. This subdivision is proposed to be completed in three 8-lot phases and a phasing plan will be provided with the Final Plan submission. Jane Arbuckle asked if the land with the pedestrian walkway is being set aside. Tom replied that the land is being set aside with no encumbrances at this time. Jane asked if any egg masses were found even though there were no significant egg masses. Tom replied that they will include a report in the final plan submission, but that the surveys showed no significant masses or they were different species / different seasons; the masses certainly did not rise to the level of significance. Richard Visser asked about sidewalks. Tom replied that they are not proposing sidewalks with this subdivision and that it would cost for the town to extend the sidewalks beyond the subdivision. Bill Dana asked if the hammerhead road would be accessible for emergency vehicles. Tom replied that the road and the hammerhead are both being built to Town standards. Jane asked if it was acceptable that the area designated for open space is largely wetlands. Charlie Frizzle replied that as long as they are not asking for the Conservation Commission /

Town to maintain ownership of the land via deed restriction, it is acceptable. Charlie pointed out though that the new Zoning Ordinance will change this. Jared Woolston stated that Anna Breinich has asked that the Conservation Commission and the Recreation Commission both review the proposed open space.

Chair Charlie Frizzle opened the meeting to public comment.

Marcus Headley asked who will maintain the road after construction is complete. Charlie Frizzle replied that as long as the road is private, the homeowners will have to maintain it and if and when the road is taken over by the Town, the Town will maintain it.

Chair Charlie Frizzle closed the public comment period.

MOTION BY BILL DANA THAT THE BOARD DEEMS THE SKETCH PLAN TO BE COMPLETE. MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

MOTION BY RICHARD VISSER THAT THE BOARD APPROVES THE SKETCH PLAN. MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

2. Case #16-022 Mid-Coast Health Services Sketch Plan: The Planning Board will review and take action on a Sketch Plan Major Development Review application submitted by authorized representatives from Pine Tree Engineering for Mid-Coast Health Services to construct three (3) new parking areas containing one hundred fifteen (115) new parking spaces. The site is located at 123 Medical Center Drive within the CC (Cooks Corner Center) Zoning District, the Medical Use Overlay Zone (MUZ). The parcel contains the Natural Resource Protection Zone (NRPZ), and Rural Brunswick Smart Growth Overlay District - Wildlife Habitat Block, (Map 45, Lot 32).

Jared Woolston introduced the Sketch Plan Major Development application and reviewed the project summary.

Charlie Frizzle disclosed that although he no longer serves on Midcoast/ Parkview Health, he does serve on several committees such as the Finance Committee, the Midcoast Medical Committee and more importantly a Building Steering Committee. Charlie said that some month ago he attended a steering committee meeting where they first discussed additional parking, but that since the initial meeting, he has not attended a meeting where this project was discussed. Normally, Charlie said that he would recuse himself from the Board, but given that Margaret Wilson is absent and cannot serve as Vice Chair and that there is not a full Board present, Charlie asked if the Developer and Applicant minded if he stayed as a voting member. No Board members or members of the public, including the applicant had any issues with Charlie remaining a voting member.

Applicant representative, Rob Prue, of Pinetree Engineering, introduced himself. Jared Woolston pointed out that the Findings of Fact will need to be revised to reflect the new plan. Rob reviewed the proposed parking locations on the site plan. Rob said that the site has had several different additions / expansions including medical offices, emergency department entrance and parking. All the additions have been approved by the Planning Board and Maine

DEP. Rob said that the geometry, landscaping and lighting plans will be the same as the rest of the parking areas so that the new parking will not look like an addition.

MOTION BY RICHARD VISSER THAT THE BOARD DEEMS THE SKETCH PLAN TO BE COMPLETE. MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

Jane Arbuckle asked what the darker green is on the plan. Rob replied that the darkest green is remaining forested land, the lighter green is forested wetlands that has been mapped and the olive green is developed areas that are basically landscaped. Jane asked if there will be a walkway from the proposed right side parking area. Rob replied that there will be. Charlie Frizzle added that the proposed parking will be mainly staff parking. Rob said that the intent is to not have pedestrian parking in the new lots, but more designated employee parking. Bill Dana asked what the slope / elevation was from the proposed parking to the street and how tall is the retaining wall. Rob replied that it will be 4 to 5 feet with crushed stone. Richard Visser asked how many vehicles are on or near the 81 Medical Center Drive. Rob replied that there are 134 parking spots currently and they would be adding a bed for 67 more. Richard expressed his concern that this is a lot of vehicle traffic for one entrance / exit. Richard asked about snow removal. Mike replied that they use Crooker to remove the snow when needed and it has worked well in the past. Jane Arbuckle asked about the drainage from the left lot as it is close to the NRPZ. Rob replied that they are proposing to install a new underground storage basin that will be filtered and then discharged into the forested wetlands.

Chair Charlie Frizzle opened the meeting to public comment. No comments were made and the comment period was closed.

MOTION BY BILL DANA THAT THE BOARD APPROVES THE SKETCH PLAN. MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

3. Case #16-024 Sweet Dreams Major Development Review Amendment: The Planning Board will review and take action on a combination Sketch & Final Plan Major Development Review application submitted by Marcus Headley for Sweet Dreams Inc. to provide new outdoor display areas. The site is located at 256 Bath Road within the CC (Cooks Corner Center) Zoning District (Map 45, Lot 55).

Jared Woolston introduced the application for Sweet Dreams Inc to provide new outdoor display areas and reviewed the project summary.

Marcus Headley, applicant, reviewed the site layout and said that they have been at this location for roughly 16 years. Marcus said that they wish to propose some small pressure treated pad approval that they put in unknowing that they needed Planning Board approval. Marcus said that he has been working with the Codes Enforcement Officer closely and that they are also seeking to put in an impervious area of crushed stone and have included a drainage plan completed by Wright Pierce. Marcus said that that the purpose of the stone area is to sell small type homes and garden sheds. Marcus said that the cross hatching across the side is currently grassed area with a lot of shrubbery and trees. They currently use the area to display outdoor furniture and would

like to continue to do this. Marcus said that since the original development they have added additional shrubbery and 10 additional trees. Bill Dana asked if there was a slope between the display area and the Bill Doge parking lot. Marcus replied that there is a buffer of large trees and a natural swail that runs along the lot; Doug Rice, of Wright Pierce, has taken advantage of the swail to create a drainage area should they do the impervious area. Richard Visser asked if they still planned on asking for a waiver regarding the Class A Soil survey. Marcus replied that the soils have not changed since the original building was put in so they are still requesting a waiver. Jared pointed out that what has changed is that the area was forested and now it is lawn area. Marcus provided a landscape history of the lot. Jared pointed out that there is email correspondence included in the packet which discusses some of the history.

Chair Charlie Frizzle opened the meeting to public comment. No comments were made and the comment period was closed.

MOTION BY RICHARD VISSER TO APPROVE THE WAIVER REQUEST FOR CLASS A SOIL SURVEY. MOTION SECONDED BY JANE ARBUCKLE, APPROVED UNANIMOUSLY.

MOTION BY BILL DANA THAT THE MAJOR DEVELOPMENT REVIEW AMENDMENT IS DEEMED COMPLETE. MOTION SECONDED BY RICHARD VISSER.

MOTION BY JEREMY EVANS THAT THE MAJOR DEVELOPMENT REVIEW AMENDMENT IS APPROVED WITH THE FOLLOWING CONDITIONS:

1. That the Board's review and approval does hereby refer to the these findings of fact, the plans and materials submitted by the applicant and the written and oral comments of the applicant, his representatives, reviewing officials, and members of the public as reflected in the public record. Any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require a review and approval in accordance with the Brunswick Zoning Ordinance.
2. That prior to the issuance of a building permit for existing and proposed structures as required by the Codes Enforcement Officer (CEO), a copy of the final stormwater management plan, including a narrative, and maintenance plan shall be signed and sealed by the design engineer or other qualified professional and provided to the satisfaction of the Director of Planning and Development.
3. That prior to the issuance of a building permit for existing and proposed structures as required by the CEO, test pit logs at the location of the proposed infiltration BMP, and a statement from the design engineer or other qualified professional that the proposed stormwater treatment system including two (2) infiltration areas, and a sediment trap was constructed to function as it was designed shall be provided to the satisfaction of the Director of Planning and Development

MOTION SECONDED BY JANE ARBUCKLE, APPROVED UNANIMOUSLY.

4. Other Business:

- Upcoming meeting on July 5 will include Zoning Ordinance Review of Chapters 1 & 2 as well as an additional agenda item.
- July 12 meeting will commence with Zoning Ordinance rewrite Chapter 3.
- Note regarding Town Lot review site walk on July 9th at 946 Mere Point Road to include joint Commissions.
- There will be a meeting on meeting August 2, 2016.

5. Approval of Minutes: No minutes were reviewed at this meeting.

Adjourn

This meeting was adjourned at 7:52 P.M.

Respectfully Submitted,

Tonya Jenusaitis

Recording Secretary