



Town of Brunswick, Maine

PLANNING BOARD

85 UNION STREET, BRUNSWICK, ME 04011

PLANNING BOARD
REVISED AGENDA
BRUNSWICK TOWN HALL
85 UNION STREET
COUNCIL CHAMBERS
TUESDAY, OCTOBER 25, 2016; 7:00 P.M.

- ~~1. **Case #16-041 – 12 Bunganuc Landing Road Shoreline Stabilization:** The Planning Board will review and take action on a combination **Sketch/Final Plan Major Development Review** application submitted by authorized representatives from Walsh Engineering for Benjamin Carey's shoreline stabilization activity that results in excess of 100 cubic yards of filling and earthmoving on a mapped highly unstable bluff, and the 100-year floodplain adjacent to tidal waters (coastal wetlands) as designated on the Federal Emergency Management Agency's (FEMA) Flood Insurance Rate Maps within the **Natural Resource Protection Zone (NRPZ)**. The project is located in the **Coastal Protection Zone (CP1) Zoning District, (Map 29, Lot 35).***~~
2. **Case #16-035 –Cumberland Farms:** The Planning Board will review and take action on a **Final Plan Major Development Review** application submitted by authorized representative Sandra L. Guay for a proposed Cumberland Farms retail store with fuel service and associated canopy at 190 Bath Road in the **Cooks Corner Center (CC) Zoning District; (Map CC1, Lot 28).**
3. **Case # 16-047, Change of Use:** The Planning Board will review and take action on a combination **Sketch/Final Plan Major Development Review** application submitted by authorized representatives from Sitelines, P.A. for Allied Composite Center LLC, to change the use of a 28,000 square foot building to Service Business, Class 2. The proposed development is located at Lot 4 of the Brunswick Industrial Park at 8 Business Parkway in the **I2 (Church Road Industrial Park) Zoning District (Map 17, Lot 68).**
4. **Other Business**
5. **Approval of Minutes**

****Item rescheduled for November 10, 2016 at 7:00 PM***

This agenda is mailed to owners of property within 200 feet of proposed development sites. Please call the Brunswick Department of Planning and Development (725-6660) with questions or comments. Individuals needing auxiliary aids for effective communications please call 725-6659 or TDD 725-5521. This meeting will be televised.

DRAFT FINDINGS OF FACT
Major Development Review
Cumberland Farms Redevelopment Project
Planning Board Review Date: October 25, 2016

Project Name: Cumberland Farms
Address: 190 Bath Road
Brunswick, ME 04011
Case Number: 16-035
Tax Map: Assessor's Map CC1, Lot 28
Zoning: Cooks Corner Center (CC) Zoning District
Applicant: LHB Enterprises, Inc.
3 Battery Wharf, #3411
Boston, MA 02109

Authorized Representative: Sandra L. Guay, Esq.
234 Main Street
Biddeford, ME 04005

Staff reviewed the application and has made a determination of completeness.

PROJECT SUMMARY

Staff review is based on the Major Development Review application to redevelop the existing Cumberland Farms retail and motor fuel outlet prepared by Sandra Guay as most recently revised on October 7, 2016. The application includes a plan set as follows:

1. CFG01.0 prepared by MHF Design Consultants, Inc. entitled, "Title Sheet" dated July 11, 2016 and revised most recently on August 26, 2016;
2. CFG02.0 prepared by MHF Design Consultants, Inc. entitled, "ALTA/ACSM Land Title Survey" dated July 11, 2016 and revised most recently on August 26, 2016;
3. CFG03.0 prepared by MHF Design Consultants, Inc. entitled, "Demolition Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
4. CFG04.0 prepared by MHF Design Consultants, Inc. entitled, "Site Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
5. CFG05.0 prepared by MHF Design Consultants, Inc. entitled, "Grading and Drainage Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
6. CFG06.0 prepared by MHF Design Consultants, Inc. entitled, "Erosion and Sedimentation Control Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
7. CFG07.0 prepared by MHF Design Consultants, Inc. entitled, "Utility Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
8. CFG08.0 prepared by MHF Design Consultants, Inc. entitled, "Landscape Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
9. CFG09.0 prepared by MHF Design Consultants, Inc. entitled, "Detail Sheet" dated July 11, 2016 and revised most recently on August 26, 2016;
10. CFG09.1 prepared by MHF Design Consultants, Inc. entitled, "Detail Sheet" dated July 11, 2016 and revised most recently on August 26, 2016;

11. CFG09.2 prepared by MHF Design Consultants, Inc. entitled, "Detail Sheet" dated July 11, 2016 and revised most recently on August 26, 2016;
12. CFG09.3 prepared by MHF Design Consultants, Inc. entitled, "Detail Sheet" dated July 11, 2016 and revised most recently on August 26, 2016;
13. CFG10.0 prepared by red leonard associated entitled, "Lighting Plan" dated May 16, 2016 (Page 1 of 2);
14. CFG10.1 prepared by red leonard associated entitled, "Fixture Specification Sheet" (Page 2 of 2);
15. CFG11.0 prepared by Bohler Engineering entitled, "Fire Suppression Plan" dated May 16, 2016;
16. CFG12.0 prepared by Cumberland Farms, Inc. entitled, "Proposed Canopy Plan & Elevations" dated March 22, 2016;
17. CFG13.0 prepared by Cumberland Farms, Inc. entitled, "Sign Plan" dated April 20, 2016 and revised most recently on August 17, 2016;
18. CFG16.0 prepared by MHF Design Consultants, Inc. entitled, "Truck Turn Plan" dated July 11, 2016 and revised most recently on August 26, 2016;
19. A1.1 prepared by Harrison French & Associates, Ltd. entitled, "Floor Plan" dated May 10, 2016;
20. A3.1 prepared by Harrison French & Associates, Ltd. entitled, "Exterior Elevations" dated May 10, 2016;
21. A3.2 prepared by Harrison French & Associates, Ltd. entitled, "Exterior Elevations" dated May 10, 2016;
22. DS1.1 prepared by Harrison French & Associates, Ltd. entitled, "Downspout Location Plan" dated May 10, 2016;
23. L1.1 prepared by Harrison French & Associates, Ltd. entitled, "Exterior Building Lighting Plan" dated May 10, 2016.

The proposed Final Plan Major Development Review application was submitted by authorized representative, Sandra Guay for LHB Enterprises, Inc., (Cumberland Farms) to redevelop an existing parcel with a new Cumberland Farms retail store with gasoline sales at 190 Bath Road (Map CC1, Lot 28). The project involves demolishing an existing 6,883 square foot building that contains a Cumberland Farms Convenience Store, AT&T Retail shop, and Papa John's pizza shop; and constructing a new 4,786 square foot Cumberland Farms Convenience Store building. The existing gasoline pipes, fueling locations, and underground fuel tanks will be removed from the site, and replaced with new underground tanks and pumps. The proposed pumps are proposed in a new configuration on-site with five (5) fuel dispensers that contain two (2) fueling locations per dispenser. The proposed fueling stations will be covered by a new 21-foot wide by 131-foot long canopy structure. The proposed parking lot will be reconfigured to contain 19 parking spaces for patrons, and two (2) additional parking spaces for employees for a total of 21 parking spaces. The proposed project is subject to the requirements of the CC1 Zoning District, including the *Cooks Corner Design Standards* that were adopted by the Brunswick Town Council on May 20, 2002.

The applicant provided the following list of requested waivers from the Cooks Corner Design Standards: *Corner Lot Treatment, Corner Buildings, Entrance (Corner Lot), Side Lot Parking, Relationship of Building to Thomas Point Road, Relationship of Building to Bath Road.*

Review Standards from Section 411 of the Town of Brunswick Zoning Ordinance

411.1 Ordinance Provisions

The proposed development complies with all applicable provisions and standards of the Cooks Corner Center (CC) Zoning District. As indicated in the project description, the applicant proposes several waivers to the Cooks Corner Design Standards. *The Board finds that the provisions of Section 411.1 are satisfied.*

411.2 Preservation of Natural Features

The proposed disturbance will be located within an area of existing development with few natural features. The proposed development maximizes the preservation of natural features of the landscape, and does not occur within or cause harm to any land which is not suitable for development. *The Board finds that the provisions of Section 411.2 are satisfied.*

411.3 Surface Waters, Wetlands and Marine Resources

Surface waters and marine resources were not identified within the proposed project area. The proposed development will not adversely affect the water quality of Casco Bay or its estuaries. *The Board finds that the provisions of Section 411.3 are satisfied.*

411.4 Flood Hazard Areas

The FEMA Flood Insurance Rate Map indicates that the project is not located within a FEMA flood hazard area; therefore there is minimal risk of flooding. *The Board finds that the provisions of Section 411.4 are satisfied.*

411.5 Stormwater Management

The proposed development satisfies the recommended storm water quality standards described in Storm Water Management for Maine: Best Management Practices, published by the State of Maine Department of Environmental Protection, November, 1995 as amended. *The Board finds that the provisions of Section 411.5 are satisfied.*

411.6 Groundwater

The Board finds that the development will not alone or in conjunction with existing activities adversely affect the quality or quantity of groundwater. *The Board finds that the provisions of Section 411.6 are satisfied.*

411.7 Erosion and Sedimentation Control

The proposed development will be constructed in accordance with Best Management Practices and will not cause unreasonable soil erosion or a reduction in the land's capacity to hold water so that a dangerous or unhealthy situation results. *The Board finds that the provisions of Section 411.7 are satisfied.*

411.8 Sewage Disposal

The existing development is served by municipal sewer, and the proposed redevelopment project intends to reconnect to municipal sewer. *The Board finds that the provisions of Section 411.8 are satisfied.*

411.9 Water Supply

The existing development utilizes public water sources; and intends to have a water source that is adequate to serve the proposed development, and that will have no adverse impact on existing water supplies by reconnecting to public water upon project completion. *The Board finds that the provisions of Section 411.9 are satisfied.*

411.10 Aesthetic, Cultural and Natural Values

The proposed project is within an existing developed site and will not disturb any natural areas. The proposed development will have no undue adverse effect on the scenic or natural beauty of the area, historic sites, significant wildlife habitats identified by the Maine Department of Environmental Protection or by the Town Of Brunswick, or rare and irreplaceable natural areas or any public rights for physical or visual access to a shoreline. *The Board finds that the provisions of Section 411.10 are satisfied.*

411.11 Community Impact

With the exception of a possible traffic impact discussed further at Section 411.12, no new impacts to traffic, the public school system, recreation, public safety, and public works resources have been identified. *The Board finds that the provisions of Section 411.11 are satisfied.*

411.12 Traffic

The Town Engineer expressed concern for potential traffic congestion, and safety from the proposed introduction of two-way access at the current one-way driveway entrance from Bath Road. At the request of the town, a third party traffic engineer from Sebago Technics reviewed the application to address concerns for traffic congestion, and safety. A copy of the review is provided in a memorandum to James Seymour from Stephen Sawyer entitled, "Traffic Peer Review – Cumberland Farms, Brunswick" and dated September 22, 2016.

As indicated in the review, the third party engineer advised against allowing right turns from the current site entrance at Bath Road as proposed. The applicant provided a response to the third party review with notable findings including observations that the existing driveway at Bath Road currently accommodates two-way traffic, and crash history within the project vicinity indicates no cause for concern. On October 19, 2016, the applicant's representatives met with staff and the third party review consultants for the town to discuss alternatives to the proposed development to minimize the probability of public road congestion and unsafe conditions. During the meeting, the representatives for the applicant maintained their analysis indicates no concern is warranted; however, they agreed to site plan modifications to mitigate concerns for traffic congestion at the proposed two-way entrance including an approximately 25-foot long, three-inch tall raised and textured island at the driveway entrance to provide separation between lanes and serve as a deterrent to cross traffic entering the site from Bath Road. The applicant further considered widening the proposed entrance to accommodate the new island, and provide additional traffic safety at the Bath Road entrance. The applicant was advised to consider reorienting the dumpster enclosure at the west side of the proposed building to a smaller footprint in order to allow the building and pump stations to be shifted slightly to the west for added isle width, and safety improvements. Provided the applicant mitigates the Bath Road entrance to the satisfaction of the Town Engineer, the proposed project is not anticipated to cause unreasonable public road congestion or unsafe conditions. *The Board finds that the provisions of Section 411.12 are satisfied provided the Bath Road entrance is designed to the satisfaction of the Town Engineer.*

411.13 Pedestrian and Bicycle Access and Safety

As required by ordinance, the applicant is required to provide sidewalks along public road frontage. The Bicycle and Pedestrian Advisory Committee (BBPAC) reviewed the proposed development and determined the proposed bicycle access and accommodations were acceptable and pedestrian access is a priority at the project location, specifically, through the Zoning Ordinance requirement for sidewalks along public streets. The staff provided review and advice to the applicant regarding preferred methods for establishing safe public access to the site, and encouraging interconnectedness between locations within the project vicinity. The staff requested

the applicant consider providing a public sidewalk along the rear property boundary between Gurnet Road (Route 24), and Thomas Point Road as a preferred alternative to a sidewalk along public road frontage (i.e. Thomas Point Road, Bath Road, and Gurnet Road).

As a result, the applicant provided a memorandum of understanding for the Town of Brunswick to accept \$10,000 for use in the Cooks Corner Zoning District, and a public sidewalk easement along the rear property line from State Route 24 to Thomas Point Road. As drafted, the sidewalk easement would be provided to the town at the time the town is ready to construct sidewalks but only within 10 years from the date of Planning Board approval. If the sidewalk is not ready to be constructed by the Town of Brunswick within the 10 year period the sidewalk easement would expire. In conversations with the applicant's representative, the staff understands that the town would be allowed to request an extension on the 10 year expiration period should the town's construction schedule for sidewalks reach the termination of the agreement. As drafted, the town would have the right to install and maintain sidewalks and all associated infrastructure such as lighting, landscaping, and stormwater management systems. The proposal is designed to accommodate bicyclists and pedestrians, and addresses issues of bicycle and pedestrian access, safety and circulation both within the site and to points outside of the site. *The Board finds that the provisions of Section 411.13 are satisfied provided a final draft of the proposed easement deed, and associated site plan that depicts the proposed easement known as "Exhibit A" is provided to the satisfaction of the Director of Planning and Development.*

411.14 Development Patterns

The proposed development will be located at an existing developed site within the growth area. The proposed development is respectful of Brunswick's historic development patterns and will have no adverse impact on areas which are primarily residential. *The Board finds that the provisions of Section 411.14 are satisfied.*

411.15 Architectural Compatibility

The proposed project is the redevelopment of an existing structure, gasoline dispensing station, and associated canopy. The proposed development is compatible with its surroundings, and conforms to the Cooks Corner Design Standards. *The Board finds that the provisions of Section 411.15 are satisfied.*

411.16 Municipal Solid Waste Disposal

The Solid Waste Impact Fee is based on 1-ton of solid waste per year, with an associated impact fee of \$258.56. The proposed development is not a substantial change and therefore a solid waste impact fee is not required. *The Board finds that the provisions of Section 411.16 are satisfied.*

411.17 Recreation Needs

A recreation impact fee is not required for the proposed commercial use. *The Board finds that the provisions of Section 411.17 are not applicable.*

411.18 Access for Persons with Disabilities

The application indicates that the proposed development will accommodate ADA requirements. *The Board finds that the provisions of Section 411.18 are satisfied.*

411.19 Financial Capacity and Maintenance

The applicant is an established business at the proposed development site that has demonstrated financial capacity and ability to maintain itself. The developer has adequate financial and technical capacity to complete the project, and once it is completed, the project is expected to

have adequate resources to maintain itself. *The Board finds that the provisions of Section 411.19 are satisfied.*

411.20 Noise and Dust

During construction, work must be done in consideration of reasonable times and decibel levels, and in accordance with Section 524.1 of the Brunswick Zoning Ordinance. Best Management Practices will be used in order to prevent dust migration during demolition and construction. Upon project completion the proposed development is not anticipated to generate unreasonable noise or dust. *The Board finds that the provisions of Section 411.20 are satisfied.*

411.21 Right, Title and Interest

The applicant has sufficient right, title and interest to develop the land. *The Board finds that the provisions of Section 411.21 are satisfied.*

411.22 Payment of Application Fees

The applicant has paid all applicable development review application fees. *The Board finds that the provisions of Section 411.22 are satisfied.*

DRAFT MOTIONS
Cumberland Farms Redevelopment Project
PLANNING BOARD APPROVAL DATE: October 25, 2016
CASE NUMBER: 16-035

Motion 1: That the Major Development Review Final Site Plan application is deemed complete.

Motion 2: That the Board approves the following requested waivers:

1. Corner Lot Treatment;
2. Corner Buildings;
3. Entrance (Corner Lot);
4. Side Lot Parking;
5. Relationship of Building to Thomas Point Road;
6. Relationship of Building to Bath Road.

Motion 3: That the Major Development Review Final Site Plan is approved with the following conditions:

1. That the Board’s review and approval does hereby refer to these findings of fact, the plans and materials submitted by the applicant and the written and oral comments of the applicant, its representatives, reviewing officials, and members of the public as reflected in the public record. Any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require a review and approval in accordance with the Brunswick Zoning Ordinance.
2. Prior to the issuance of a building permit, a final draft of the proposed sidewalk easement deed, and associated site plan that depicts the proposed

easement area known as “Exhibit A” is provided to the satisfaction of the Director of Planning and Development.

3. Prior to issuance of a building permit, the Bath Road entrance is designed to the satisfaction of the Town Engineer and the approved design is depicted site plans are provide to the Director of Planning and Development.

** Please note that Development Review Site Plan approvals by the Planning Board or Staff Review Committee shall expire at the end of two years after the date of final approval unless all construction has been completed by that date (Section 407.4.B of the Brunswick Zoning Ordinance).*

July 27, 2016

STAFF REVIEW COMMITTEE NOTES

Staff present:

Anna Breinich (Director of Planning and Development), John Foster (Public Works Director/Town Engineer), Clinton Swett (Assistant Assessor), Jeff Hutchinson (CEO), Ken Brilliant (Fire Chief), Nolan Robbins (Brunswick Sewer Department), Amanda Perkins (Bowdoin Fellow) **Non-voting staff:** Jared Woolston (Planner), **Applicants Present:** Chris Tymula (MHF Design Consultants, Inc., Project Engineer), Dalina Boryszewski (Cumberland Farms, Design Specialist), Sandra Guay (Project Attorney), Heather Monticup (Greenman-Pederson, Traffic Engineer) **Public present:** Will Wilkoff (Chair, Bicycle & Pedestrian Advisory Committee)

1. **Case #16-035 –Cumberland Farms:** The Staff Review Committee will review and provide a recommendation to the Planning Board on a **Sketch Plan** Major Development Review application submitted by authorized representative Sandra L. Guay for a proposed Cumberland Farms retail store with fuel service at 190 Bath Road in the **Cooks Corner Center (CC) Zoning District; Assessors Map CC1, Lot 28.**

Chris Tymula (MHF Design Consultants, Inc., Project Engineer): Mr. Tymula brought plans and boards with photo simulation overlays showing different views of the current site and the proposed future conversion. He made a brief presentation, including mention of a sewer easement along the back and side of the property that connects to Thomas Point Road. Basically, everything currently at the site will be razed; the existing store will be removed, all the pavement will be removed, the parking and the canopy. As part of the project, 3 (three) 8,000 gallon underground storage tanks will be removed and replaced with 2 (two) 20,000 gallon storage tanks. There will be a new canopy with 5 (five) dispensing units and 10 (ten) fueling positions. They are maintaining the existing curb cut with a slight modification to allow easy entry and exit. There is a total of 19 (nineteen) striped parking spaces plus 2 (two) employee parking spaces. The existing impervious coverage of the site is about 46,500 sq. ft. With the redevelopment, they will actually reduce impervious to 35,000 sq. ft. For signage, based on the frontage of the site, they are allowed 2 (two) signs, both of which are 31.5 sq. ft. in size, along with the canopy signs and signs on the building. There is a fenced-in dumpster closure on the site and a bike rack, which was discussed at the Bike/Ped meeting the week before. They propose an outside seating/patio area, site lighting, and snow storage throughout the site. They believe the appearance is a marked improvement from the current site. They are proposing a variety of catch basins and 2 (two) hydrodynamic separators on the site for stormwater management, which will connect into the drain systems currently there along both Bath and Thomas Point Roads. Roof drainage for the canopy and the store will connect to that same system. They have a plan for construction, including sediment and erosion control and fencing. They will maintain connections into existing gas, water line and sewer lines. A new power pole will be installed offsite that will connect to an existing pole that straddles the line between the Cumberland Farms and the MacDonald's properties. They have created a substantial landscaping plan, with a variety of low-growth plantings along the front to maintain visibility for sight distance, and other trees and shrubs to surround the site. Their lighting plans shows a variety of lighting throughout the site, including down-facing can lights for the convenience store and flush-mounted canopy lights, to provide safety and prevent spillage to neighboring properties.

John Foster:

- John sent Chris the town survey of Thomas Point Road and doesn't see that it's been addressed
 - Chris: Talked to their survey manager, who felt that the information John provided was consistent with what they had done for their survey

- You're depicting the right-of-way a few feet behind the curb in front of the CMP power line – DOT clearly shows the right-of-way behind the pole. Although you don't utilize it in your site development, you are not depicting the right-of-way as we understand it.
 - Chris: He provided the information to their survey manager, but he will speak to him again
- We had given you the survey report, the reference and the DOT plan. He didn't get a chance to look up your plan book and references to see the ones he has looked at, but it's again, it's not consistent with the way we see the right-of-way on Thomas Point Road.
 - Chris: Either way he doesn't see this as causing a significant change to the plans
- It's not impacting it, per se, but the town has looked at modifications to that intersection. If your plan holds true, there's nothing that could be done there. The laundromat guy across the street thinks he owns halfway out in the road, and you're showing you're halfway out in the road; there's barely room for a car to travel. It's inconsistent with the information John had sent them.
- He noticed an "entrance only" off Bath Road and now you want an entrance/exit. It's right near the intersection. You are also reconfiguring the pumps – before they were parallel with Bath Road, and now they're vertical. He's concerned that cars will be pulling in from Bath Road and starting to queue up. You have no throat to your driveway. There's no setback, there is no time for cars to stack onsite if they want to wait for a pump or someone's starting to jockey. The pump islands are much closer now to your entrance. He has no idea why an exit is needed there, because all they can do is turn right, and you're dumping them right out in the middle of Cook's Corner intersection – a 7-lane approach on all sides. He would prefer that to be closed and just go with the Thomas Point Road. Now it's pretty wide open at the entrance, but moving that pump island over and reorienting the ways the cars stack causes him concern.
 - Chris: They will look at that.
 - Heather Monticup, traffic engineer: It is restricted to a right in, right out on Bath Road. No one could try to make a left turn there. Because they are reorienting the pumps, this type of pump works a lot better for onsite circulation. When they are all stacked up like they are today, you run into potentially having back up on the site. They need the 2 (two) driveways to get the fuel delivery trucks in and out. They have to drop fuel on the right side, so the trucks would have to almost do a 180. The driveway on Bath Road is important to the site. The reason for opening it is because now that it is starting gate you're going to have people all different ways, and allowing the right out, which would be restricted to a right out, will help promote circulation instead of everyone having to go to that one driveway. They reviewed collisions at this driveway; there were only 3 (three) rear ends in the 3 (three) years they looked at. It doesn't appear that collisions at the driveway today are a problem.
- Believes that changing the traffic pattern will have traffic coming out there more often, and with the way the pump islands are oriented, they are going to have a difficult time making a right turn out of that driveway, and are just going to impede traffic trying to come in from a busy road at a major intersection. He wouldn't recommend the plan.
 - Clint: Could we add another entrance point off Gurnet Road?
 - Heather: Doesn't think that would work
 - Chris: Gas station developments are purely predicated on how the trucks can get in and out of the site. That drove the design. If they eliminate that driveway altogether, at the very least they would need to have an "enter only" purely for the tanker trucks, as it is today. If they're looking for a driveway entrance on Rt. 24, it's becoming too close to the striped stop bar and there wouldn't be enough

queue because they have a right turn only and then thru lanes and left turn only. It would be more of an issue getting a driveway along Rt. 24 than it would be worth.

- On the pump islands, is there one dispensing per side?
 - Chris: Yes. There are 5 (five) islands and 10 (ten) fuel locations, so there's one per side.

Anna Breinich:

- What is your typical size lot for an installation like this?
 - Chris: An acre to an acre and a half – they aren't usually very large lots
- She likes the façade treatments and the canopy itself. What happens if you're driving from the front, you can't even see the building. She doesn't know if there's a way of reconfiguring the pumps where you can see the building. Right now, all you see is canopy straight on, and we need to be able to see the front as well. It would be great to be welcoming to the entire corner.
 - Chris: The cashiers at the counter need to be able to see, for safety reasons, and the canopy has to have a minimum height for vehicles to get under. Those 2 (two) things dictate where the canopy goes.
- There's a good example of a canopy in the back in Topsham, and it's working fine.

John Foster:

- Storm drains – I know you connect to the storm drain system now, but you connect to the one structure on Bath Road in the front, and now you're indicating 2 (two) drainage quadrants and it's connecting to a catch basin on Thomas Point Road. Basically, you're showing your floats coming in almost opposite, at almost 160 degrees, the outlet flow. I don't care for that, but could possibly connect into the line rather than go through a catch basin lateral that goes down into the structure. That's a new connection or new load on that particular reach of the drainage system, which is separate, because the one that you've connected to in front goes towards Cook's Corner and across it, and discharges into the drainageway by the pawn shop.
 - Chris: We could reorient that – we could add an inline structure or add another structure
- The bottom line is we need to talk about how it connects – the best way – and we need to double check the capacity of that stretch. The other option is to connect to your other one that connects to the one in front. Right now everything's draining to that one in the front from what I see of your existing plans. A peer review of the site circulation might be appropriate.
 - Chris: They are also reducing ¼ acre of pavement

Will Wilkoff:

- When Cumberland Farms presented to the Brunswick Bicycle & Pedestrian Advisory Committee (BBPAC) recently, the committee was happy with the bike parking, but were unsure about the pedestrian access. They all agreed that the whole area is a pedestrian nightmare. At that meeting, he thinks they gave the impression that they didn't see the need for sidewalks, and that was based on their mistaken thought that there weren't any sidewalks near the area. It turns out that Coastal Orthopedics, down Thomas Point Road, has sidewalks which are part of a new development, and that would be a logical thing to continue. That would be an area that should at least consider pedestrian access. There is also sidewalk along the Bath Road and stops at Johnson's Sporting Goods. They would like to have a chance to rethink the 24 side, but there's a good argument for having sidewalks on the Thomas Point side, and some continuity among the stores on 24 might be helpful.

Anna Breinich:

- The whole idea is to get sidewalks throughout the Cook's Corner area – there are some major projects going on in the next year. She explained to them the commitments she has gotten from other businesses to put in sidewalks when reconstruction is done in their area. John Foster would need to be involved.
 - Chris: Right now, there wouldn't be enough room to run sidewalks. If the town is going to push for sidewalks, they may need to get some easements because they would be partially on Cumberland Farms property and partially on the town's, and they would lose a significant amount of landscaping on the frontage.
- Could you push the building back slightly?
 - Chris: Right now there's a sewer easement there, and they can only push back a maximum of 2 (two) feet.
- Do you know where it's located versus the easement?
 - Chris: Yes, he had spoken to the Sewer Department because they had looked into encroaching into the easement and readjusting things, and it was his understanding that there is no possible way they can put the building in that easement.

Will Wilkoff:

- On the Rt. 24 side, is there room south of that vegetation there?
 - Chris: This is all off their property and within the right-of-way. This is proposed to be green space, so there is the potential for a sidewalk somewhere in that area. He watched the video from the meeting last week and his take was that it wasn't going to be an issue, and they didn't need sidewalks.
- We were working without all of the information, it turns out.
 - Chris: We can discuss it more. I can't commit to anything at this point. Would it be possible to run a sidewalk within this right-of-way and then a sidewalk to the building?
 - Will: Yes
 - Anna: That would be great
 - Chris: Worries about a "sidewalk to nowhere"
 - Anna: Like anything else, you've got to start

Jeff Hutchinson:

- Where is the sidewalk going to go after coming down Thomas Point Road when it hits Bath Road?
 - Anna: We need Bath Road as well
- Could a sidewalk be installed from the Thomas Point Road between Cumberland Farms and Prompto and then along the easement out back to get to Rt. 24, so that pedestrians aren't out in the middle of that nasty intersection?
 - Chris: He would need to talk to Cumberland Farms more because he's talking providing pedestrian access through their site, connecting 24 to Thomas Point Road.
 - Anna: That would leave you what you have designed
 - Jeff: It would remove pedestrians from near that nasty intersection
 - Cumberland Farms always has concerns about liability, not that it can't be worked out, would need an easement or something
 - Jeff: There's a 30-foot wide sewer easement there now
 - Heather: It would just have to be modified to allow pedestrians
 - Chris: It certainly would eliminate the need to reduce the landscaping on the front, which is for beautification
 - Anna: ? pedestrian-activated signal in that area? Need to talk to John Foster about where would be a good location

Ken Brilliant (Fire Chief):

- No questions at this time

Jeff Hutchinson:

- Currently out back it is all paved
 - Chris: For their stormwater management plan they decided to eliminate the pavement to stop circulation behind the building

Anna Breinich:

- Will you be working with DEP for environmental permitting?
 - Chris: Yes
- Where are the dumpsters located?
 - Chris: In this fenced-in area, fully screened, unlike the current dumpsters
- She does like Jeff's suggestion of a rear sidewalk and it could solve a lot of problems

Jeff Hutchinson:

- AT&T, Papa John's – where are they headed?
 - Lease is month-to-month; they have been approached about this project
- What is the construction date?
 - Chris: Typically it's a 16-week time frame, and they would like to get started as soon as possible
- New signs are monument signs, but by looking at the sketches they are actually pole signs
 - Chris – apologizes if he misspoke – they are freestanding signs
 - Jeff: You are limited to 25 sq. ft. for freestanding pole signs – you could have the same effect by creating monument signs – the sign face would need to either be sitting on a true monument or appear to be sitting on a base, similar to a gravestone

Clint Swett:

- No questions or comments

Jared Woolston:

- Wondering about interconnectivity with other businesses, even before they were discussing a sidewalk out back – wondered if they could work with these other 2 (two) businesses to put a little spur path between parking lots for passive egress, if possible

Anna Breinich:

- Coastal Ortho is not just orthopedics; it's also PT and a sports venue. It's used heavily.

Jared Woolston:

- Likes the reduction in impervious, but the proposed plantings are fairly small – wonders if they've looked at getting larger deciduous trees, like some oaks that would grow tall
 - It's for visibility – they don't want to block the canopy
 - Heather – trunks small and canopy high – doesn't want to impede site view
- Here and there he thinks site distance might be mitigated by placement, and you might be able to get away with a few large trees
 - Jeff: the trees might get decimated by CMP if they're by the power poles
- If the power lines are 20 feet, then the trees that they have proposed will be an issue anyway at 30 feet – might as well get some native oak – possibly along Thomas Point Road, as Jeff pointed out
 - Chris: We will talk with our landscaper
- Waivers – administratively, it makes sense to get this in front of the Planning Board for their consideration, but only to take action at final plan – not at sketch plan. Ms. Breinich agreed.

Chris Tymula:

- They can sit down to talk about access on Bath Road, the easement/right-of-way and options for drainage connection
- Doesn't believe the minor items will be an issue
- Any other issues or concerns with the general layout?
 - Jared: Nice proposal for a sketch plan – flush out the minor things discussed for final plan
 - Anna: John Foster suggested a peer review on traffic
 -

The meeting was adjourned.



TOWN OF BRUNSWICK, MAINE

INCORPORATED 1739

DEPARTMENT OF PLANNING AND DEVELOPMENT
85 UNION STREET
BRUNSWICK, ME 04011

ANNA BREINICH, FAICP
DIRECTOR OF PLANNING & DEVELOPMENT

PHONE: 207-725-6660
FAX: 207-725-6663

MEMORANDUM

TO: The Planning Board
FROM: Jared Woolston, Planner
DATE: July 29, 2016
RE: Sketch Plan Review of Cumberland Farms Redevelopment (Case #16-035)

PROJECT SUMMARY

Staff has reviewed the Sketch Plan application and determined that it is complete.

The proposed Sketch Plan Major Development Review application was submitted by authorized representative, Sandra Guay for LHB Enterprises, Inc., (Cumberland Farms) to redevelop an existing parcel with a new Cumberland Farms retail store with gasoline sales at 190 Bath Road (Map CC1, Lot 28). The project involves demolishing an existing 6,883 square foot building that contains a Cumberland Farms Convenience Store, AT&T Retail shop, and Papa John's pizza shop; and constructing a new 4,786 square foot Cumberland Farms Convenience Store building. The existing gasoline pipes, fueling locations, and underground fuel tanks will be removed from the site, and replaced with new underground tanks and pumps. The proposed pumps are proposed in a new configuration on-site with five (5) fuel dispensers that contain two (2) fueling locations per dispenser. The proposed fueling stations will be covered by a new 21-foot wide by 131-foot long canopy structure. The proposed parking lot will be reconfigured to contain 19 parking spaces for patrons, and two (2) additional parking spaces for employees for a total of 21 parking spaces. The proposed project is subject to the requirements of the CC1 Zoning District, including the *Cooks Corner Design Standards* that were adopted by the Brunswick Town Council on May 20, 2002.

The BBPAC reviewed the proposed Sketch Plan at their meeting on July 21, 2016. During the meeting, the applicant requested consideration for the advising the Planning Board to waive the sidewalk requirement within the Cooks Corner Design Standards. The BBPAC indicated that if no other sidewalks were within the project vicinity it would be practical to advise in favor of

waiving sidewalks. Staff advised that the BBPAC would be allowed to consider the project at the time of Final Plan review.

The Staff Review Committee (SRC) reviewed the Sketch Plan application on July 27, 2016. During the SRC meeting, the applicant indicated that the proposed Cumberland Farms Convenience Store would be a standalone facility that no longer contains the existing Papa John's Pizza shop, and AT&T Retail shop. The applicant described the plans that were submitted within the application for the SRC.

During the SRC meeting, the Director of Planning and Development asked the applicant to consider reconfiguring the proposed building, and fueling stations by moving the proposed fuel pumps to the rear of the parcel, and the building towards the front of the parcel. A representative for the applicant, Chris Tymula indicated that the proposed orientation was considered the most attractive for patrons. The applicant's representatives indicated that a 30-foot wide sewer easement at the rear of the parcel renders the area within the sewer easement unbuildable as indicated by the Brunswick Sewer District. The Town Engineer expressed concerns for access to the site from the Bath Road entrance and recommended that a third party engineer review traffic circulation unless the entrance to Bath Road was deleted or the proposed fueling stations were reoriented to avoid congestion within Bath Road should vehicles on-site block the Bath Road entrance as they wait for an unoccupied fuel pump. The traffic engineer for the applicant, Heather Monticup, P.E. responded that the two (2) proposed entrance driveways (Bath Road and Thomas Point Road) are required for delivery trucks entering and exiting the site; and the proposed layout was determined to be acceptable for traffic circulation. Director of Planning and Development, Anna Breinich advised that the Planning Board may consider conditioning a third party review of traffic circulation to satisfy the concerns of the Town Engineer. The Town Engineer noted that the storm drain capacity may be limited at the location where stormwater is proposed to be directed. The applicant indicated that the proposed reduction in impervious area was found to reduce the volume of runoff. The Town Engineer advised that stormwater may need to be redirected to ensure adequate capacity within the public stormwater system at peak flows. The Co-Chair of the Bicycle and Pedestrian Advisory Committee (BBPAC), Will Wilkoff attended the SRC meeting to address the requirement for sidewalks within the Cooks Corner Design Standards. The Co-Chair indicated that due to a lack of information about the project site, the BBPAC was remiss in advising the applicant that sidewalks would likely be waived by the Planning Board at the time of Final Plan review. Mr. Wilkoff indicated that recent findings that sidewalks exist within the project vicinity, and that a nearby business agreed to contribute funds to the Town of Brunswick for sidewalks would likely yield an opinion from the BBPAC that sidewalks should be required by the Planning Board in accordance with the Cooks Corner Design Standards. The applicant indicated that a portion of the frontage on Route 24 is not owned by the applicant and if sidewalks were required by the Planning Board an easement may be required. The SRC requested consideration for providing a sidewalk from the property boundary at Thomas Point Road to the boundary at Route 24 along the abutting properties to the east (Prompto 10 Minute Oil Change, and McDonalds) rather than the frontage to public streets to avoid possible safety issues, and maintain the proposed landscaped areas. The Town Planner advised the applicant to consider providing pedestrian interconnectivity with neighboring businesses (Prompto 10 Minute Oil Change, and McDonalds) with a pedestrian access easement to neighboring lots if allowed by the abutting businesses. The Planner further advised that the

proposed landscaping plan may be enhanced with some strategically placed native deciduous trees to provide tree canopy within an area of town that is primarily unvegetated pavement. The applicant's representatives indicated that consideration would be made for providing large trees within the landscaping plan but any new tree would need to avoid blocking a vehicle operator's line of site to oncoming traffic. The CEO advised that any proposed plantings should be placed in areas that avoid power lines at mature heights since future maintenance would require the removal of hazardous limbs.

The applicant provided a list of waiver requests that would be considered at the time of Final Plan Review as follows: *Corner Lot Treatment, Corner Buildings, Entrance (Corner Lot), Side Lot Parking, Relationship of Building to Thomas Point Road, Relationship of Building to Bath Road.* Reference is made to the Cooks Corner Design Standards for the above described waivers.

A Sketch Plan of the proposed development was prepared by MHF Design Consultants, Inc. entitled, "Site Plan" dated July 11, 2016.

APPROVED MOTIONS
SKETCH PLAN MAJOR REVIEW – CUMBERLAND FARMS
Case # 16-035

Motion 1. That the Board deems the Sketch Plan to be complete.

Motion 2. That the Board approves the Sketch Plan.

EXHIBIT A

Space above for recording information

**QUITCLAIM EASEMENT DEED
(with Covenant)**

KNOW ALL PERSONS BY THESE PRESENTS that **L. H. B. ENTERPRISES, INC.**, a Massachusetts corporation with an address of 3 Battery Wharf, Unit 3411, Boston, MA 02109 (“Grantor”), grants to **THE TOWN BRUNSWICK**, a municipal corporation, with a principal address of 28 Federal Street, Brunswick, Cumberland County, Maine 04011 (“Grantee”), with Quitclaim Covenants, a certain easement over and across property of the Grantor herein, situated in Brunswick, Cumberland County, Maine, said easement area being more particularly described in Exhibit A, attached hereto and incorporated herein by reference.

Grantor hereby grants a perpetual easement to Grantee for that area of Grantor’s property that lies over and above the area marked as “Sidewalk Easement Area” as depicted on said Exhibit A, ~~said easement area being 20 feet in width at all points~~. Said easement is for the limited purpose of use as a public sidewalk. Said easement includes all statutory rights and obligations incident to ownership and use of a public sidewalk, including the obligation of Grantee to construct, maintain, and repair said sidewalk, any lighting, landscaping, or other appurtenant fixtures related to the public sidewalk, and to remove snow and/or place sand or salt within the easement area.

Neither Grantor nor its successors, lessees or assigns shall take any action to block or otherwise impede public access to the Sidewalk Easement Area without reasonable advance notice to and approval of the Town of Brunswick.

The property of the Grantor which is subject to the aforesaid easement are those premises conveyed to Grantor by Lily H. Bentas, by deed dated April 29, 1997, and recorded in the Cumberland County Registry of Deeds in Book 13060, Page 174.

The said L. H. B. Enterprises, Inc. has caused this instrument to be signed in its corporate name by Lily H. Bentas, its President, duly authorized, this ___ day of _____, 20__.

L. H. B. ENTERPRISES, INC.

Name: Lily H. Bentas
Title: President

STATE OF _____

_____, ss,

_____, 20__

Then personally appeared the above-named, Lily H. Bentas, President of said L. H. B. Enterprises, Inc., duly authorized, and acknowledged the foregoing instrument to be her free act and deed in her said capacity and the free act and deed of said L. H. B. Enterprises, Inc.

Before me,

Notary Public

Print Name:

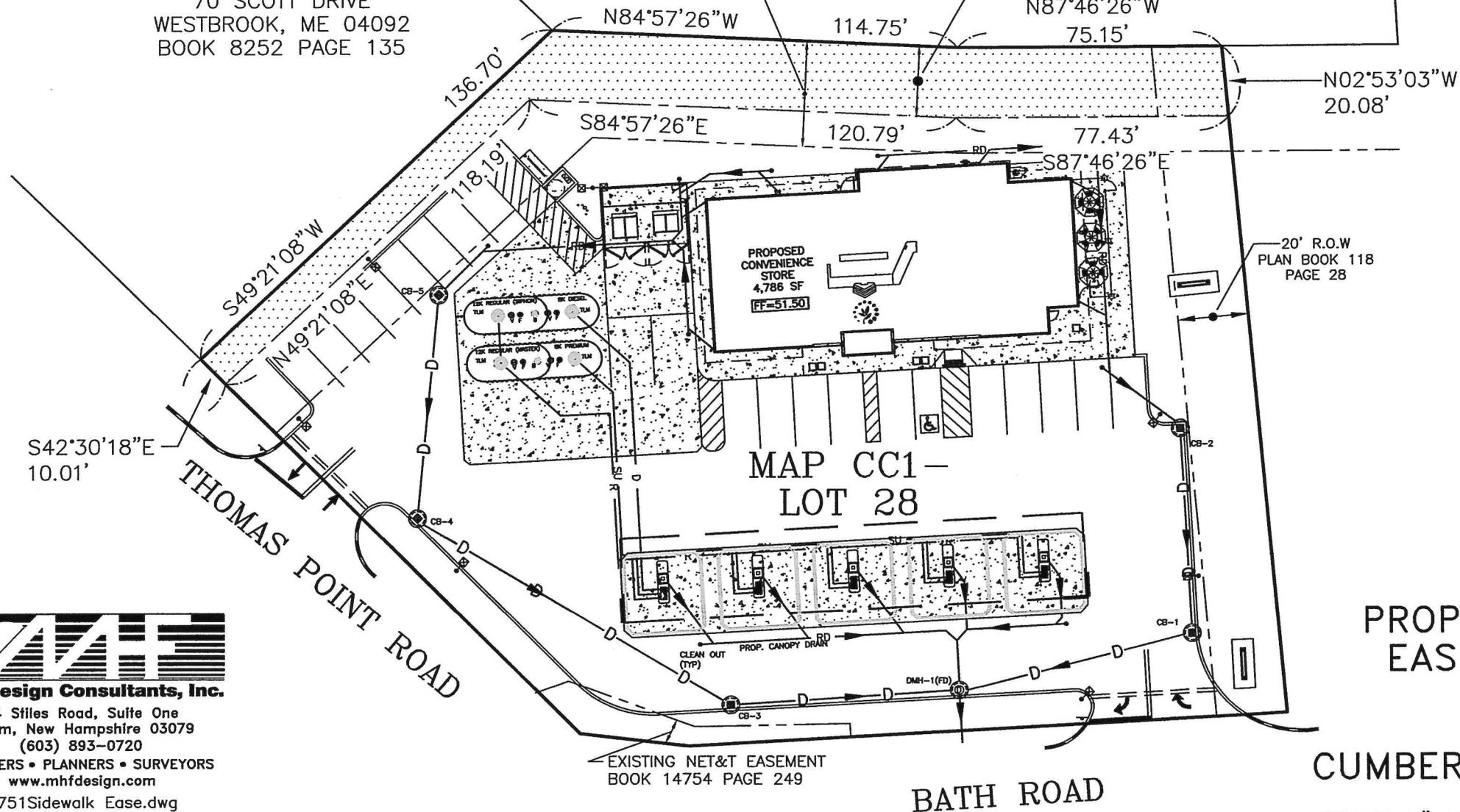
Commission Expires:

MAP CC1 LOT 27
N/F PROMPTO, INC.
70 SCOTT DRIVE
WESTBROOK, ME 04092
BOOK 8252 PAGE 135

MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

PROPOSED SIDEWALK EASEMENT
5,156 Sq.Ft.

EXISTING 30' SEWER
EASEMENT TO
THE BRUNSWICK
SEWER DISTRICT
BOOK 2985
PAGE 694



ROUTE 24
DRAFT
10/14/16

**PROPOSED SIDEWALK
EASEMENT EXHIBIT**

190 BATH ROAD
BRUNSWICK, MAINE
PREPARED FOR

CUMBERLAND FARMS, INC.

SCALE: 1"=40' DATE: OCTOBER 14, 2016



44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com
3751Sidewalk Ease.dwg
MHF PROJ. #375115

Julie Erdman

From: Christopher Tymula <cmt@mhfdesign.com>
Sent: Wednesday, September 07, 2016 4:42 PM
To: Lucy Burnell; Jared Woolston
Cc: Julie Erdman; Sandra Guay; Frank C. Monteiro; Dawn Johnson; Dalina Z. Boryszewski; Kathleen Sousa; Robert C. Schuler
Subject: RE: Cumberland Farms Final Plan Application
Attachments: 3751-Drainage Memo--Rev1.pdf

Attached is the PDF of the Site Plan Set as requested, via drop box link due to file size. Also attached is the revised drainage memo.

<https://www.dropbox.com/s/r24z5x2oi63yod7/CFG%20Brunswick%20ME%20Site%20Plan%20Set%20dated%208-26-16.pdf?dl=0>



Chris M. Tymula | *Project Manager* | cmt@mhfdesign.com
MHF Design Consultants, Inc.
44 Stiles Road, Suite One | Salem, NH 03079
p: 603-893-0720 | f: 603-893-0733 | www.mhfdesign.com

From: Lucy Burnell [<mailto:lburnell@woodedlaw.com>]
Sent: Wednesday, September 07, 2016 3:52 PM
To: jwoolston@brunswickme.org
Cc: jerdman@brunswickme.org; Sandra Guay; Christopher Tymula
Subject: Re: Cumberland Farms Final Plan Application

Good afternoon Mr. Woolston,

Attached please find a pdf copy of the Cumberland Farms Final Plan Application submitted on September 2nd. Chris Tymula from MHF Design Consultants will be sending you the plans in digital form, if he has not done so already (he may have included a CD with the final plan submission last week).

Please contact me if you need anything further. Thank you,

Lucy Burnell
Legal Assistant to Sandra L. Guay
Woodman Edmands Danylik Austin Smith & Jacques, P.A.
234 Main Street
P.O. Box 468
Biddeford, ME 04005
Telephone: 207-284-4581
Fax: 207-284-2078

August 31, 2016

Ms. Anna Breinich
Director of Planning and Development
Town of Brunswick
85 Union Street
Brunswick, ME 04011

Re: Cumberland Farms, Inc.
Map CC1 Lot 28
190 Bath Road
Brunswick, ME 04011

Sub: Drainage Memorandum Revision

Dear Ms. Breinich,

MHF Design Consultants, Inc., on behalf of our client, Cumberland Farms Inc., is pleased to provide the following information summarizing the proposed site re-development project relative to the effects it will have on stormwater management on the surrounding areas at the above referenced site. Please refer to the attached Proposed Site Re-Development Plans, dated July 11, 2016, last revised August 26, 2016, as prepared by this office.

The property is located on a parcel of land that is approximately 1.12 Ac in size and identified as Tax Map CC1, Lot 28 and is located in the Cooks Corner Center zoning district at the southeast intersection of Bath Road and Route 24. The lot is currently occupied by a 6,883 sf Cumberland Farms Convenience Store which includes an AT&T Retail shop and Papa John's Pizza shop. The site also includes a fuel dispensing area with 4 dispensers (8 fueling locations), an overhead canopy, 16 striped parking spaces and many additional un-striped paved areas accessed by driveways located along both Bath Road and Thomas Point Road.

Cumberland Farms Inc. proposes to raze the existing building and fuel dispensing area and construct a new standalone Cumberland Farms convenience store (4,786 sf), a new fuel canopy with five (5) dispensers (10 fueling locations), new fuel piping and two (2) new 20,000 gallon double wall fiberglass underground fuel storage tanks. Site improvements will also include a reconfigured parking area, new trash enclosure, bike rack and modified entrance driveways. New utility connections including water, gas, electric and sewer are proposed along with new landscaping to enhance the site.

Re-development of this parcel will result in a decrease in onsite impervious coverage of approximately 11,267 sf which will in turn reduce both the peak rate and volume of runoff discharging into the Bath Road and Thomas Point Road closed drainage systems. The proposed on-site stormwater management system includes installing several deep sump, hooded catch basins and the installation of a hydrodynamic separator prior to discharge into the existing closed drainage system in Bath Road which will significantly improve the stormwater quality. While



there is an existing closed drainage system currently onsite, it appears that the pipes are undersized and that little or no pretreatment of the stormwater is provided before leaving the site.

Based on the reduction in impervious coverage of over 1/4-ac and the implementation of a new stormwater management system, we believe that the owner is improving the current conditions while reducing peak rates of runoff and volume discharging from the site and that there will be no negative impact on the abutting properties as a result of the proposed redevelopment project.

Please review the attached information and should you have any questions, please feel free to call our office at your convenience.

Sincerely,
MHF Design Consultants, Inc.

A handwritten signature in black ink, appearing to read 'Chris Tymula', is written over the typed name and title.

Chris Tymula
Project Manager

F:\Projects\Eng\375115\3751-Drainage Memo--Rev1.doc

Cc: Dalina Boryszewski - Cumberland Farms, Inc. (EB Upload)
Sandra Guay - Woodman Edmands Danylik Austin Smith & Jacques, P.A.

Julie Erdman

From: Jared Woolston
Sent: Friday, September 23, 2016 11:52 AM
To: 'Sandra Guay'
Subject: FW: 16382.Traffic Peer Review Comments. -Cumberland Farm re-development
Attachments: 16382.Traffic Peer Review Comments.pdf

Sandra: Here is the traffic review from Sebago. There are substantive concerns with two-way traffic that require additional information, and perhaps revisions to the site plan. As I understand it from our conversation today, your client will continue to work with our staff, and seek Planning Board approval at the October 11, 2016 meeting instead of this Tuesday. The agenda is in the process of being updated this morning. When the 10/11 meeting agenda is prepared I will email you a copy.

Regarding your client's proposed easement, I received comments from the town's legal Counsel this morning and I will work with them, and our Public Works Director to make sure the language is acceptable and send you any requested changes. At this time, the language changes only require minor adjustments regarding allowed work in the easement; and a mechanism for accepting the proposed funds (\$10,000) is being considered.

Jared Woolston
Planner
Town of Brunswick
85 Union Street
Brunswick, ME 04011

(207) 725-6660, ext. 4022 (v)
(207) 725-6663 (f)
jwoolston@brunswickme.org
www.brunswickme.org

From: James Seymour [mailto:jseymour@sebagotechnics.com]
Sent: Friday, September 23, 2016 10:48 AM
To: Jared Woolston
Cc: 16382
Subject: FW: 16382.Traffic Peer Review Comments. -Cumberland Farm re-development

Jared,
Here is Steve's comments.

Looks like he Ok with the numbers but not the Right exit out onto Bath Road.

Let us know if more is needed.
Thanks

Jim Seymour, P.E. *Senior Project Manager*
Office: 207.200.2100 | Direct: 207.200.2083 | Cell: 207.632.1199
75 John Roberts Rd., Suite 1A, South Portland, ME 04106
jseymour@sebagotechnics.com | www.sebagotechnics.com
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From: Steve Sawyer
Sent: Thursday, September 22, 2016 6:02 PM
To: James Seymour <jseymour@sebagotechnics.com>
Subject: FW: 16382.Traffic Peer Review Comments.pdf - Adobe Acrobat Standard

Here you go!



Stephen S. Sawyer, Jr., PE *Vice President, Transportation*
Office: 207.200.2100 | Direct: 207.200.2082 | Mobile: 207.232.1281
75 John Roberts Rd., Suite 1A, South Portland, ME 04106
ssawyer@sebagotechnics.com | www.sebagotechnics.com
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From: Julie Gordon

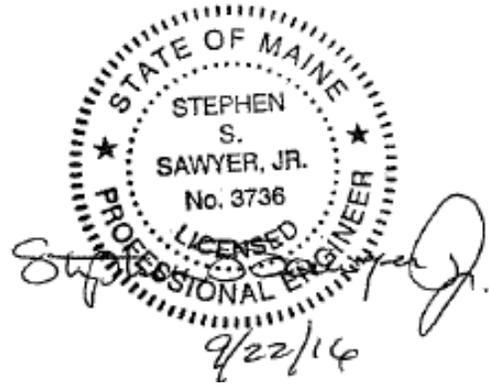
Sent: Thursday, September 22, 2016 2:29 PM

To: Steve Sawyer <ssawyer@sebagotechnics.com>

Subject: 16382.Traffic Peer Review Comments.pdf - Adobe Acrobat Standard

Memorandum

Project: 16382
To: James Seymour
From: Stephen Sawyer
Date: September 22, 2016
Subject: Traffic Peer Review – Cumberland Farms,
Brunswick



Per your request, I have reviewed the proposed site plan and Planning Board application by LHB Enterprises, Inc. for a new Cumberland Farms at the intersection of Old Bath Road and Route 24 at Cooks Corner in Brunswick. My review focused solely on the traffic elements of the application – Part 7 in the application. You also provided me with a set of Plans produced by MFH Design Consultants and stamped on 8/31/16 by Frank C. Monterio, Maine PE 7201. My findings are as follows:

1. The updated Collision Summary Letter by the Traffic Engineer from Greenman-Pederson, Inc. (GPI) dated August 30, 2016 provides data from MaineDOT's files but neglects to indicate the significance of this information in traditional MaineDOT fashion. What is missing is that our information suggests that MaineDOT categorizes both intersections on Old Bath Road (Thomas Point Road and Route 24) as High Crash Locations. There is a minor discrepancy in the number of crashes at the Thomas Point Road intersection (7 vs. 8), but nevertheless this is an area of heightened concern for safety. Given this information, we do not believe that introducing a new point of potential conflict onto Old Bath Road by allowing right turns out of the site entrance onto Old Bath Road (which is not permitted today) is wise.
2. The trip generation provided by GPI dated July 7, 2016 was found to be in order.
3. You mentioned that the Town may have some concerns with queue spillback onto Old Bath Road from gas pump operations. I don't see this as a major issue given the 10 pump positions that are proposed. However, if the right turn out access onto Old Bath Road is allowed by the Town, then it is more likely that queued vehicles exiting the site could impede smooth traffic flow entering the site as their free movement to the pumps would be compromised.

This is the extent of my comments and let me know if you have any follow up questions or concerns.

Julie Erdman

From: John Foster
Sent: Wednesday, October 05, 2016 4:16 PM
To: Jared Woolston
Subject: RE: Cumberland Farms

Jared,

For the record and to confirm our phone conversation, I don't believe a performance guarantee is required for this project as they are not constructing any required public infrastructure improvements. FYI, thanks, John

John Foster, Town Engineer/Public Works Director
Town of Brunswick
207.725.6654

From: Jared Woolston
Sent: Wednesday, October 05, 2016 3:49 PM
To: John Foster
Subject: FW: Cumberland Farms

John: Can you provide some feedback on the question below (re: performance guarantee)? I will have the town attorney look this over and please send me any questions or concerns you may have.

Jared Woolston
Planner
Town of Brunswick
85 Union Street
Brunswick, ME 04011

(207) 725-6660, ext. 4022 (v)
(207) 725-6663 (f)
jwoolston@brunswickme.org
www.brunswickme.org

From: Sandra Guay [<mailto:slg@woodedlaw.com>]
Sent: Wednesday, October 05, 2016 3:46 PM
To: Jared Woolston
Subject: RE: Cumberland Farms

Hi Jared:

The digital copy that you have is the most recent version.

Attached is a draft MOU. The easement deed is attached - note that we have included landscaping in the list of improvements. Cumberland Farms is still reviewing this, but I am anticipating that there will be no changes on our end. Kindly let me know if the Town's attorney has any comments.

I am still waiting to hear back on the traffic review. I believe that GPI has had a conversation with Sebago, and is in the process of preparing a response letter. As soon as it is ready, I will get a copy to you.

Do you have any information on what is required for a draft performance guarantee (and if you want it coming from the engineer or Cumberland Farms)?

Thank you – Sandra

SANDRA L. GUAY, ESQUIRE

Partner

WOODMAN EDMANDS DANYLIK
AUSTIN SMITH & JACQUES, P.A.
234 MAIN ST., P. O. BOX 468
BIDDEFORD, ME 04005
T: [\(207\) 284-4581](tel:(207)284-4581) F: [\(207\)284-2078](tel:(207)284-2078)
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www.woodedlaw.com

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From: Jared Woolston [<mailto:jwoolston@brunswickme.org>]
Sent: Wednesday, October 05, 2016 3:15 PM
To: Sandra Guay
Subject: RE: Cumberland Farms

Sandra: I received the package of application materials today. Do I have the most current digital application for posting online (Friday) or do I need an updated version? How is the draft agreement / traffic review going?

Jared Woolston
Planner
Town of Brunswick
85 Union Street
Brunswick, ME 04011

(207) 725-6660, ext. 4022 (v)
(207) 725-6663 (f)
jwoolston@brunswickme.org
www.brunswickme.org

From: Sandra Guay [<mailto:slg@woodedlaw.com>]
Sent: Monday, October 03, 2016 3:21 PM
To: Jared Woolston
Subject: Cumberland Farms

Hi Jared:

I just want to confirm that other than comments to traffic peer review and finalizing the sidewalk agreement, you have everything you need for planning board. I believe that we are all up to date on submittals, but would appreciate confirmation.

Thank you – Sandra

SANDRA L. GUAY, ESQUIRE

Partner

WOODMAN EDMANDS DANYLIK
AUSTIN SMITH & JACQUES, P.A.
234 MAIN ST., P. O. BOX 468
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TOWN OF BRUNSWICK, MAINE
Bike & Pedestrian Advisory Committee

28 FEDERAL STREET
BRUNSWICK, ME 04011

William Wilkoff, Co-Chair
Richard Cromwell, Co-Chair

PHONE: 207-725-6660
FAX: 207-725-6663

Minutes of July 21, 2016

Council Chambers
85 Union Street

Present: Will Wilkoff (chair), Jared Woolston (Town Planner), Josh Katz, Kathy Wilson (Town Councilor), Josh Katz, Blaine Moore, Ben Walsh

Call to Order: 6:02 PM

New Member – Blaine Moore was introduced as our newest member. His presence at most of our meetings over the last two years had been appreciated and the Committee is eager to have him officially on board.

June 16, 2016 Minutes: Approved without correction

Communications: Rich has three communications two regarding Brunswick Landing from Larry Kline and Alicia Heyburn. One included information that the gate on Brunswick Landing perimeter road just before the Bowdoin Solar Farm has been unlocked. The third was regarding a pedestrian crossing of Bath Rd. at Sunnybrook Farm. Details of these communications will wait until Rich returns at the next meeting.

Development Review:

Case # 16-035 – Cumberland Farms at Cooks Corner – Representatives from the company and the development presented their initial plan. The issues regarding pedestrian and bicycle access were discussed. The bicycle parking seems adequate and the entrance and exist will remain similar. The question of whether sidewalks should be included as is required by the current Cooks Corner plan was discussed. The initial impression of the Committee was that with no existing sidewalks near the development that it might be reasonable to have this requirement waived. However, there was concern that although there was no comprehensive plan for Cooks Corner that requiring sidewalks

might be a place to start. The Committee will have another opportunity to review the plans as they develop.

Case # 16-026 Tau Yuan – Green House and expansion. The Committee felt that the plan included adequate bicycle parking and presented no new pedestrian problems. The Committee did recommend that the parking under the building also include some bicycle parking if feasible.

Case # 16-018 Hair Salon – The Committee felt that bicycle parking planned was adequate and that no new pedestrian issues were anticipated.

Case # 16-030 All Saints Parish – The bicycle parking planned appeared adequate there was some discussion about pedestrian flow through the site and a question of whether the sidewalks on Union St should be coordinated with further rehab of Union St.

Case # 16-028 Hancock Lumber- This is an addition of warehouse that according to current code would trigger more parking and a sidewalk requirement. While the Committee could see the need for some bicycle is warehouse. Given the expected future development (or lack thereof) in the area the Committee felt that sidewalks did not appear to be necessary.

Crossing Flashers State Grant – Jared reported that the money from the State has not been released.

Letters to Savalink and Wayfair: Jared reported that these are still being worked on.

Projects:

BBPAC Project List and Recommendations – Will reported that once the Complete Streets issue is resolved he intends to work on this project that Steve Cohen began.

Bike Friendly Downtown – Ben reported informally prior to the meeting the sticker program was to be launched on July 25.

Bike Friendly Community Application – Josh reported that the application is moving forward with help from Rich. It remains to be worked out on how to include the supporting photos.

Frank Wood Bridge – Josh reported that he and Rich have been involved in the “106” process of evaluation that is mandated because it will have some Federal funding.

Union Street – Will reported that he and Rich will work with Jared to work on scheduling a workshop to develop our recommendation.

Complete Streets – Will presented the draft of the plan that he and the Town Manager have worked on. It clarifies the role of BBPAC, the Town Engineer and the Town Manager. The Committee supported this draft but would like a phrase added that expressed the need for BBPAC to be informed in a timely fashion of any project not included in the CIP. With this wording in place the Town Manager will present it to the Town Council next month.

Jordan Avenue – Will reported on John Foster's response to our proposal of making Jordan Avenue a sharrow from Federal Street to Lishness Field entrance. John agreed with going as far as the railroad bridge, but felt that the road with further east was sufficiently wide to not trigger the need for a sharow. He provided some photos of the striping that included generous shoulders that was done after the street's rehab in 2006. Some of the neighbors apparently felt that this striping made the road look more like a connectors and it was not repainted in the next cycles. Unfortunately, we were not able to see these photos, but Will felt that this might be a reasonable compromise. We will deal with this at our next meeting.

New Business:

Kathy voiced her concern about the sandwich boards on the Maine St. sidewalks that make for tortuous pedestrian paths. Jared reported that the Department's summer intern was doing an audit of the Maine St. sidewalks.

Will reported that the US Bicycle Route One/East Coast Greenway signs are up.

Blaine reported that he is working on Runner Friendly Application.

Adjournment: 7:23 PM



TOWN OF BRUNSWICK, MAINE
Bike & Pedestrian Advisory Committee

BRUNSWICK, ME 04011

William Wilkoff, Co-Chair
Richard Cromwell, Co-Chair

PHONE: 207-725-6660
FAX: 207-725-6663

Minutes of Bike/Pedestrian Advisory Committee
September 15th, 2016

Present: Rich Cromwell (Co-Chair), Jared Woolston (Town Planner), Josh Katz, Kathy Wilson (Town Councilor), and Blaine Moore

Call to Order:

Minutes of August 18, 2016: Approved without correction

Communications:

Rich presented on discussions with Larry Kline about bicycling and walking on Brunswick Landing.

- Possible use of the area surrounding the bunkers as a mountain bike park;
- Western boundary perimeter road;
- Possibility of identifying areas that could currently be developed as multi-use trails; and
- Formation of a 'sub-committee' of local stakeholders to address Brunswick Landing bike/ped issues specifically.

Reference the request for a sub-committee, Jared suggested a sit down meeting with Larry. Josh suggested a preliminary plan should be developed by the interested parties as a basis of discussion and that MRRRA and other stakeholders should be involved early on.

Projects and Progress Reports:

- Rich reported on 'Open Streets Event' in Bath and suggested we consider doing the same in Brunswick next summer on a Sunday morning or afternoon;
- Blaine reported on 'Runner Friendly Community' status and on the process of applying. Kathy will present the idea to Town Council;
- Josh reported on the Frank J. Wood Bridge Section 106 process. Kathy commented on the recent bridge inspection results. Josh pointed out that MDOT is well aware of bike/ped issues. Rich mentioned that the Design Advisory Committee met and Will can report on DAC activity when he returns;
- Rich reported that John Foster has received the BBPAC's Jordan Ave. and Union Street proposals; and
- The Pleasant Street walk will be held September 26th at 4:00 PM.

Staff Report:

- Kevin reported on All Saints' Parish sketch plan of the common development plan and specifically on the multi-use building proposed on the campus. The plan includes bike racks and multiple marked sidewalks. Josh mentioned covered bike racks, but Kevin said that even covered bike parking is complicated by Codes, which could require sprinkling, for example;
- Jared reported on Cumberland Farms project;
- Jared reported on Mid Coast Hospital parking lot expansion, which will include bike racks;
- Jared reported on MDOT technical inspection of our proposed pedestrian flasher locations. There is an easement problem at the Longfellow location which could hold up the whole project. Rich will seek clarification from John on the easement issue.

Old Business:

Discussion of active recruitment for Committee vacancies we know will be occurring next year.

Adjournment:

Meeting adjourned at 6:55 PM.

Next meeting on October 20, 2016.

TOWN OF BRUNSWICK PLANNING BOARD

FINAL PLAN APPLICATION

APPLICANT: LHB Enterprises, Inc.

AGENT: Sandra L. Guay, Esquire
Woodman Edmands Danylik Austin Smith & Jacques, P.A.
234 Main Street, P.O. Box 468
Biddeford, Maine 04005

ENGINEERS: MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, NH 03079

**Final Plan Application of
LHB Enterprises, Inc.**

**Cumberland Farms Project
190 Bath Road**

TABLE OF CONTENTS

1. Final Plan Application
2. Authorization
3. Right Title and Interest
4. Location Maps
5. Abutter's List
6. Project Description
7. Trip-Generation and Collision Summary Letters dated July 7, 2016 and August 30, 2016
8. Waiver Requests
9. Plans (submitted separately)

1

**MAJOR DEVELOPMENT REVIEW
FINAL PLAN APPLICATION**

1. Project Name: Cumberland Farms Project

2. Project Applicant

Name: LHB Enterprises, Inc.
 Address: 3 Battery Wharf, #3411
Boston, MA 02109
 Phone Number: _____

3. Authorized Representative

Name: Sandra L. Guay, Esq.
 Address: 234 Main Street
Biddeford, ME 04005
 Phone Number: (207) 284-4581

4. List of Design Consultants. Indicate the registration number, address and phone number Of any engineer, surveyor, architect, landscape architect or planner used:

1. MHF Design Consultants, Inc., 44 Stiles Road, Ste One, Salem, NH 03079 (603)893-0720
2. _____
3. _____

5. Physical location of property being affected: 190 Bath Road

6. Lot Size: 50,960 sq. ft.

7. Zoning District: CC

8. Indicate the interest of the applicant in the property and abutting property. For example, is the applicant the owner of the property and abutting property? If not, who owns the property subject to this application? Applicant is the owner of the property

9. Assessor's Tax Map CC1 Lot Number 28 of subject property.

10. Brief Description of proposed: Retail Business and Motor Fuel Outlet

11. Describe Specific Physical Improvements to be Done: Please see attached.

Owner Signature: *Sandra L. Guay*, as authorized agent for owner
 Applicant Signature (if different): _____

Required Attachments (by Applicant):

- Final Plan Check List
- Final Plan Requirements for Open Space Developments (if applicable)
- Request for Waivers (if applicable)

- Required Copies of Final Plan

Required Attachment (by Planning and Development Department):

- Listing of all owners of property within 200-foot radius of property under review.

FINAL PLAN REQUIREMENTS

Key: "O" = omit; "S"=submit; "NA"=not applicable; "W" = waiver P=pending

Item	O	S	NA	W	P	Comments
Name of Development		X				
Scale, date, north point, area, number of lots (if subdivision)			X			
Boundaries of all lots and tracts with accurate distances and bearings, locations of all permanent monuments property identified as existing or proposed.		X				
Certification by a professional land surveyor that the land has been surveyed and the boundaries established in accordance with the State of Maine Board of Licensure for Professional Surveyors standards for Category 1 (Standard Boundary Survey), conditions 1, 2, or 3.		X				
Existing zoning district and overlay designation.		X				
Names of engineer and surveyor; and professional registration numbers of those who prepared the plan.		X				
Names of current owner(s) of subject parcel and abutting parcels.		X				
Name, location, width of paving and rights-of-way, profile, cross-section dimensions, curve radii of existing and proposed streets; profiles of center-lines of proposed streets, at a horizontal scale of 1" equals 50' and vertical scale of 1 inch equals 5 feet, with all elevations referred to in U.S.G.S. datum.			X			
A general road plan noting circulation, direction, traffic control devices, street lighting and type of lighting proposed.			X			
Existing and proposed easements associated with the development.		X				
Kind, location, profile and cross-section of all proposed drainage facilities, both within the development and outside of it, and a storm-water management plan which includes the submission requirements listed in the storm-water management checklist available in the Planning Department.		X				
Location of features, natural and artificial, such as water bodies, wetlands, streams, vegetation, railroads, ditches and buildings.		X				

Location of existing and proposed utilities; water, sewer, electrical lines, and profiles of underground facilities. Tentative locations of any private wells.		X			
Existing and proposed location, size, profile and cross section of sanitary sewers; description, plan and location of other means of sewage disposal with evidence of soil suitability.			X		
Topography with counter intervals of not more than 2 feet.		X			
A Class A (high intensity) Soil Survey prepared in accordance with the standards of the Maine Association of Professional Soil Scientists.			X		
Location of all existing trees over 10 inches in diameter, locations of tree stands, and a plan showing all trees to be removed as a result of the development proposal.			X		
Lighting plan showing details of all proposed lighting and the location of that lighting in relation to the site.		X			
Existing locations and proposed locations, widths and profiles of sidewalks.				X	
Location map.		X			
Approximate locations and dimensions of proposed parking areas.		X			
Proposed ownership and approximate location and dimensions of open spaces for conservation and recreation.			X		
Grading, erosion control, and landscaping plan; proposed finished grades, slopes, swells, and ground cover or other means of stabilization.		X			
Reference to special conditions stipulated by the Planning Board, with conditions either set forth in full or on the plan or identified as specific documents filed with the Board.			X		
A wetlands map drawn by a specialist delineating wetland boundaries in accordance with the methods prescribed by the US Army Corps of Engineers.			X		
Dedicated public open spaces, areas protected by conservation easements, and existing and proposed open spaces or recreation areas.			X		

An erosion and sedimentation control checklist prepared by the Cumberland County Soil and Water Conservation District.			X			
A statement from the Brunswick-Topsham Water District of conditions under which water will be provided.					X	
A statement from the Brunswick-Topsham Water District of its review and comments on the proposed use if the project involves development within the Aquifer Protection Zone.			X			
A Statement from the Fire Chief recommending the number, size, and location of hydrants, available pressure levels, road layout and street and project name, and any other fire protection measures to be taken.			X			Reviewed by Fire Chief at Sketch Plan Review. No Comments
A statement from the Superintendent of the Brunswick Sewer District of the conditions under which the Sewer District will provide sewerage disposal service and approval of the sanitary sewers proposed within the development.					X	
Where a septic system is to be used, evidence of soil suitability.			X			
All applicable materials necessary for the reviewing entity to review the proposal in accordance with the Criteria of Section 411.		X				
A plan of all buildings with new construction or expansion of an existing facility, including type, size, and footprint, floor layout, setback, elevation of first floor slab, storage, and loading areas.		X				
An elevation view of all sides of each building proposed indicating height, color, bulk, surface treatment, and signage.		X				
A circulation plan describing all pedestrian and vehicle traffic flow on surrounding road systems.		X				
The size and proposed location of water supply and sewage disposal systems.		X				
A site landscaping plan indicating grade change, vegetation to be preserved, new plantings used to stabilize areas of cut and fill, screening, the size, location and purpose and type of vegetation.		X				

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AUTHORIZATION

I, Lily H. Bentas, on behalf of myself and my company LHB Enterprises, Inc., hereby authorize our attorneys, Woodman Edmands Danylik Austin Smith & Jacques, P.A., Cumberland Farms, Inc., and our engineers, MHF Design Consultants, Inc., to sign any and all State of Maine or Town of Brunswick permit and appeal applications on our behalf with regard to the proposed Cumberland Farms development located at 190 Bath Road, Brunswick, Maine. I further authorize any of the attorneys or engineers in the above referenced firms to appear on our behalf and to represent the company before any department, board, committee or agency of said State or Town, including, but not limited to, the Maine Department of Environmental Protection, Planning Board and the Zoning Board of Appeals.

DATED: July 6, 2016

LHB Enterprises, Inc.



By: Lily H. Bentas
Its: President

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021569

DEEDB95.2

QUITCLAIM DEED

Lily H. Bentas, of 431 Lewis Wharf, Boston, MA 02110,

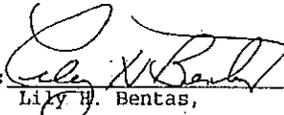
for consideration paid of One Dollar (\$1.00) and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged,

grants to L. H. B. Enterprises, Inc., a Florida corporation with offices at 431 Lewis Wharf, Boston, MA 02110,

the real property located at Route 1 and 24, Brunswick, Cumberland County, Maine, more particularly described in Exhibit A attached hereto and made a part hereof (the "Property") together with any and all servitudes, easements, rights-of-way, licenses and other rights in real property appurtenant thereto, and all improvements located thereon.

Said Property is conveyed subject to any liens for taxes not yet due and payable, provisions of any ordinance, municipal regulation and public or private law, all matters of the public record and any state of facts which an accurate survey of the premises would disclose.

IN WITNESS WHEREOF, Grantor has executed this deed this
29th day of April, 1997.

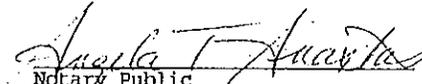
By: 
Lily H. Bentas,

THE COMMONWEALTH OF MASSACHUSETTS

Norfolk SS.

April 29, 1997

Then personally appeared the above Lily H. Bentas, to me known and known by me to be the person executing the foregoing instrument, and she acknowledged said instrument by her executed to be her free act and deed, before me.


Notary Public

My Commission Expires: 3/17/00

SEAL

EXHIBIT A

Certain lots or parcels of land, together with buildings and improvements thereon, situated on the southerly side of Old Route #1 (also being known as the Bath Road), in the Town of Brunswick, County of Cumberland, and State of Maine, bounded and described as follows:

Parcel I. Beginning at an iron pipe situated on the Westerly side of Thomas Point Road, which iron pipe marks the most Northerly corner of land now or formerly owned by one Evrard; thence proceeding South $49^{\circ} 21' 0''$ West along the Northwesterly side of said Evrard Land, a distance of 150.02 feet to an iron pin at the most Westerly corner of said Evrard land, said iron pin also marking the Northeasterly corner of land of McDonald's Corporation, as conveyed to said McDonald's Corporation by Bo-Ed Inc. by deed dated April 30, 1969 and recorded in the Cumberland County Registry of Deeds in Book 3083, Page 284; thence proceeding North $84^{\circ} 57' 34''$ West along the Northerly side of said McDonald's Corporation land a distance of 115 feet to an iron pipe; thence proceeding North $87^{\circ} 46' 39''$ West along the Northerly side of said McDonald's Corporation land a distance of 54.82 feet to an iron pipe situated at the Southeasterly corner of a 20 foot right-of-way; thence proceeding North $2^{\circ} 53' 11''$ West along the Easterly side of said right-of-way, a distance of 192.07 feet to an iron pipe situated on the Southerly side of Old Route #1, so-called also being known as the Bath Road; thence proceeding North $89^{\circ} 19' 26''$ East along the Southerly side of Old Route #1, a distance of 149.57 feet to a granite monument; thence proceeding South $80^{\circ} 53' 37''$ East along the Southerly side of Old Route #1, a distance of 29.43 feet to an iron pipe situated on the westerly side of said Thomas Point Road; thence proceeding South $47^{\circ} 49' 53''$ East along the Westerly side of said Thomas Point Road, a distance of 154.07 feet to an iron pipe in the point of beginning.

Parcel II. Beginning at an iron pipe situated on the Westerly side of Thomas Point Road, which iron pipe marks the most Northerly corner of land now or formerly owned by one Evrard; thence proceeding South $49^{\circ} 21' 0''$ West along the Northwesterly side of said Evrard land, a distance of 150.02 feet to an iron pin at the most Westerly corner of said Evrard land, said iron pin also marking the Northeasterly corner of land of McDonald's Corporation, as conveyed to said McDonald's Corporation by Bo-Ed Inc. by deed dated April 30, 1969 and recorded in the Cumberland County Registry of Deeds in Book 3083, Page 284; thence proceeding North $84^{\circ} 57' 34''$ West along the Northerly side of said McDonald's Corporation land a distance of 115 feet to an iron pipe; thence proceeding North $87^{\circ} 46' 39''$ West along the Northerly side of said McDonald's Corporation land a distance of 54.82 feet to an iron pipe situated at the Southeasterly corner of a twenty foot right-of-way and the point of beginning; thence proceeding North $2^{\circ} 53' 11''$ West 192.07 feet to an iron pipe situated on the Southerly side of Old Route #1, so-called, also being known as the Bath Road; thence proceeding South $89^{\circ} 19' 26''$ West along the Southerly side of Old Route #1, a distance of 20.01 feet to an iron pipe; thence South $2^{\circ} 53' 11''$ East a distance of 191.05 feet to an iron pipe; thence proceeding South $87^{\circ} 46' 39''$ East 20.08 feet to the point of beginning.

Meaning and intending to describe the same premises described in the deed recorded in Book 4183, Page 22.

RECEIVED
RECORDED REGISTRY OF DEEDS

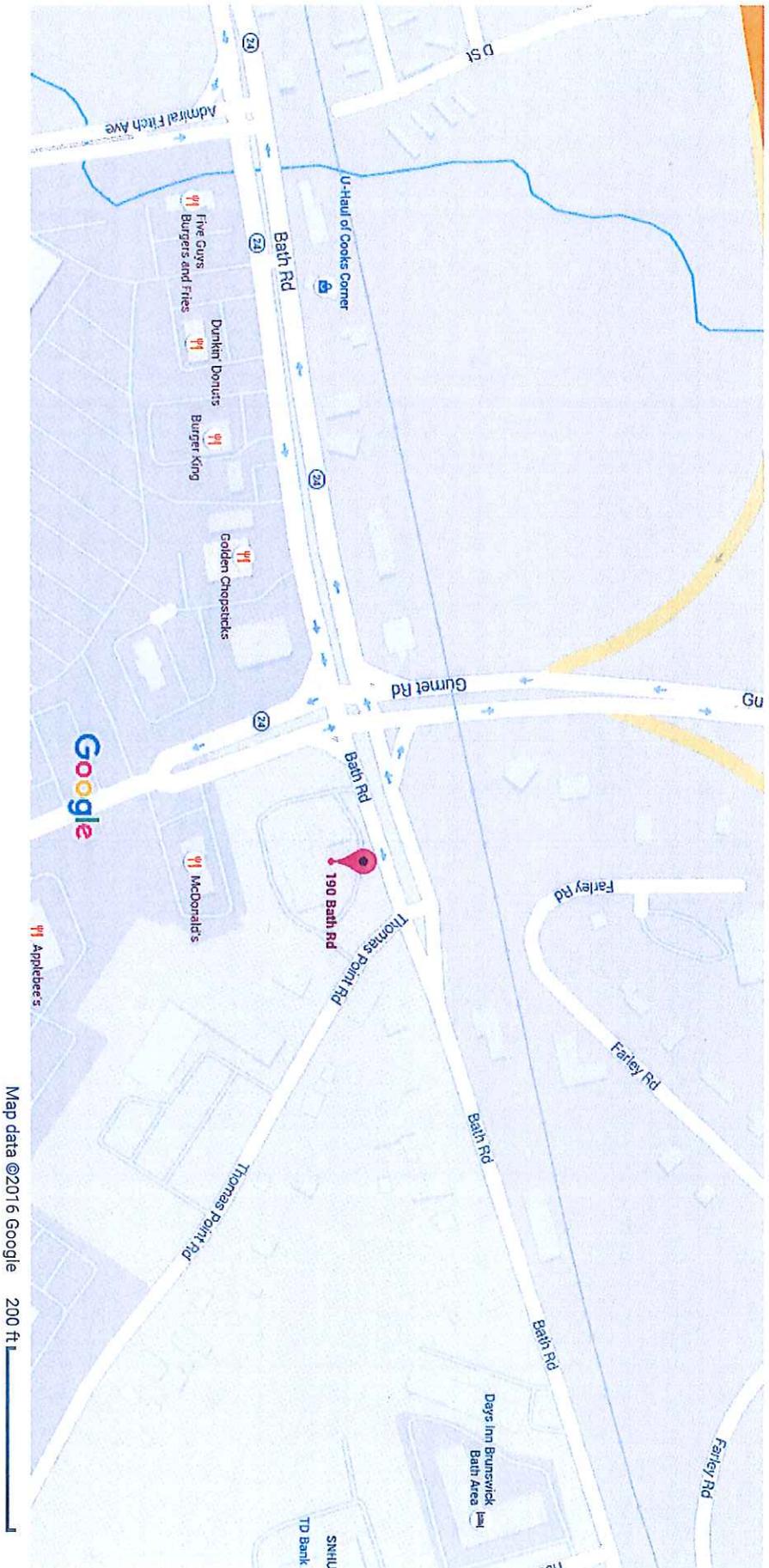
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CUMBERLAND COUNTY

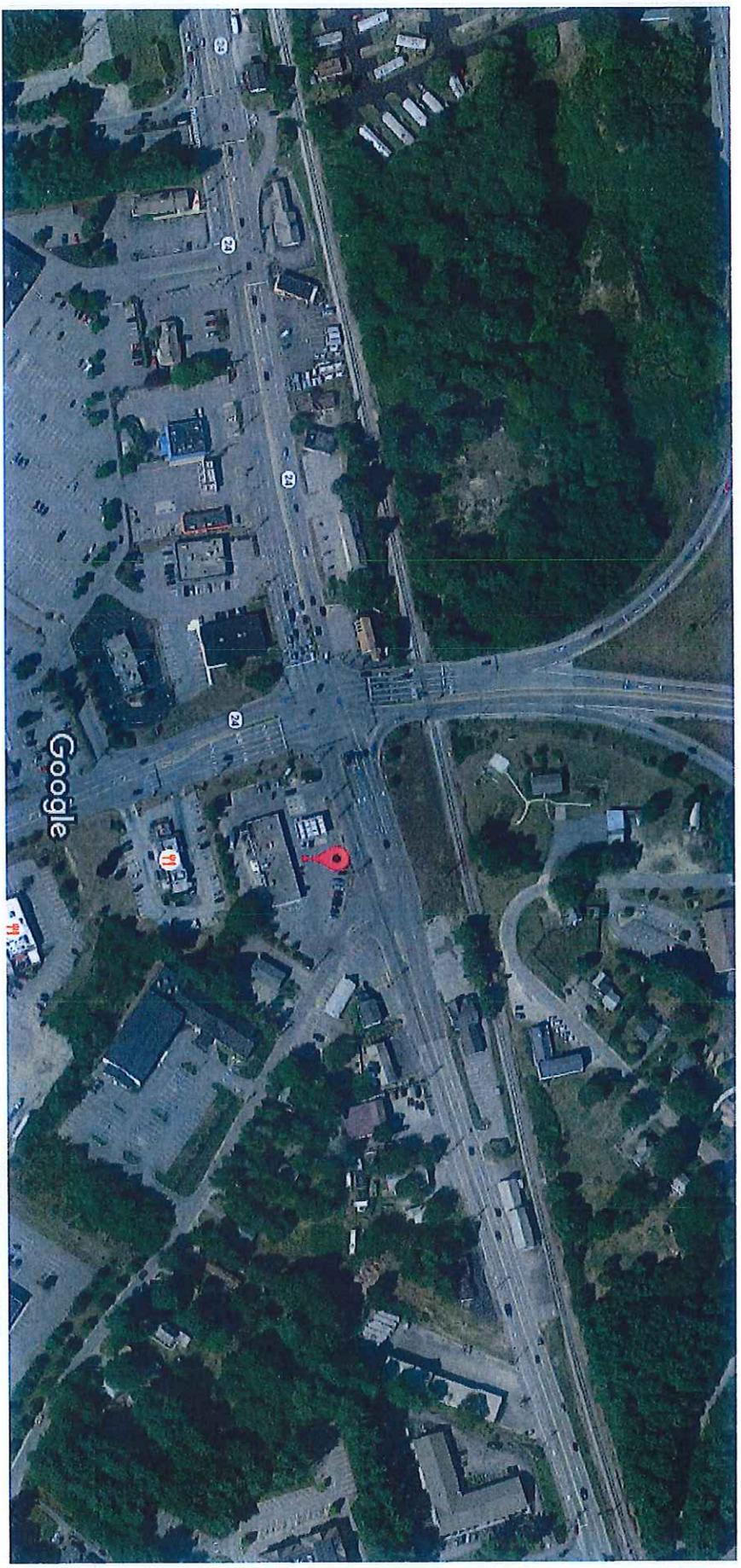
John B. Curran

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Google Maps 190 Bath Rd



Google Maps 190 Bath Rd



Map data ©2016 Google 200 ft

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**Final Plan Application of
LHB Enterprises, Inc.**

**Cumberland Farms Project
19 Bath Road**

LIST OF ABUTTERS

Map	Lot	Owner	Mailing Address
CC1	7	Ricky and Monique Lachapelle	379 Lisbon Street Lewiston, ME 04240
CC1	21	Just Because, LLC	P.O. Box 9340 Auburn, ME 04210
CC1	27	Prompto, Inc.	70 Scott Drive Westbrook, ME 04092
CC1	29A	McDonalds Corporation c/o Joe Breisacher	P.O. Box 653 Brunswick, ME 04011
CC1	31	Developers Diversified Cooks Corner LP	P.O. Box 228042 Beachwood, OH 44122
CC2	9	Crooker Enterprises, Inc. c/o Estes Lobster House	1909 Harpswell Neck Road Harpswell, ME 04079
CC2	11	State of Maine c/o MDOT	16 State House Station Augusta, ME 04333
CC2	29	Linda A. Johnson	206 Bath Road Brunswick, ME 04011
CC2	30	Cook's Corner Group, LLC	204 Bath Road Brunswick, ME 04011
CC2	31	Edward and Samantha Hannan	169 Cedar Pond Road Durham, ME 04222
CC2	32	Sunshine Too, Inc.	200 Bath Road Brunswick, ME 04011
CC2	42	Brunswick Landing Development Group, LLC	2 Main Street, Suite 200 Topsham, ME 04086
CC2	53	State of Maine c/o MDOT	16 State House Station Augusta, ME 04333

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July 11, 2016

Ms. Anna Breinich
Director of Planning and Development
Town of Brunswick
85 Union Street
Brunswick, ME 04011

Re: Cumberland Farms, Inc.
Map CC1 Lot 28
190 Bath Road
Brunswick, ME 04011

Sub: Drainage Memorandum

Dear Ms. Breinich,

MHF Design Consultants, Inc., on behalf of our client, Cumberland Farms Inc., is pleased to provide the following information summarizing the proposed site re-development project relative to the effects it will have on stormwater management on the surrounding areas at the above referenced site. Please refer to the attached Proposed Site Re-Development Plans, dated July 11, 2016, as prepared by this office.

The property is located on a parcel of land that is approximately 1.17 Ac in size and identified as Tax Map CC1, Lot 28 and is located in the Cooks Corner Center zoning district at the southeast intersection of Bath Road and Route 24. The lot is currently occupied by a 6,883 sf Cumberland Farms Convenience Store which includes an AT&T Retail shop and Papa John's Pizza shop. The site also includes a fuel dispensing area with 4 dispensers (8 fueling locations) and an overhead canopy, 16 striped parking spaces and many additional un-striped paved areas accessed by driveways located along both Bath Road and Thomas Point Road.

Cumberland Farms Inc. proposes to raze the existing building and fuel dispensing area and construct a new standalone Cumberland Farms convenience store (4,786 sf), a new fuel canopy with five (5) dispensers (10 fueling locations), new fuel piping and two (2) new 20,000 gallon double wall fiberglass underground fuel storage tanks. Site improvements will also include a reconfigured parking area, new trash enclosure, bike rack and modified entrance driveways. New utility connections including water, gas, electric and sewer are proposed along with new landscaping to enhance the site.

Re-development of this parcel will result in a decrease in onsite impervious coverage of approximately 11,372 sf which will in turn reduce both the peak rate and volume of runoff discharging into the Bath Road and Thomas Point Road closed drainage systems. The proposed on-site stormwater management system includes installing several deep sump, hooded catch basins and the installation of two (2) hydrodynamic separators prior to discharge into the existing closed drainage systems in Bath Road and Thomas Point Road which will significantly improve



the stormwater quality. While there is an existing closed drainage system currently onsite, it appears that the pipes are undersized and that little or no pretreatment of the stormwater is provided before leaving the site.

Based on the reduction in impervious coverage of over 1/4-ac and the implementation of a new stormwater management system, we believe that the owner is improving the current conditions while reducing peak rates of runoff and volume discharging from the site and that there will be no negative impact on the abutting properties as a result of the proposed redevelopment project.

Please review the attached information and should you have any questions, please feel free to call our office at your convenience.

Sincerely,
MHF Design Consultants, Inc.

A handwritten signature in blue ink, appearing to read 'Chris Tymula', is written over a faint, larger version of the signature.

Chris Tymula
Project Manager

F:\Projects\Eng\375115\3751-Drainage Memo.doc

Cc: Dalina Boryszewski - Cumberland Farms, Inc. (EB Upload)
Sandra Guay - Woodman Edmands Danylik Austin Smith & Jacques, P.A.

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GPI Greenman-Pedersen, Inc.

Engineering and Construction Services

REF.: MAX-2016046

August 30, 2016

Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

SUBJECT: Updated Collision Summary Letter
Cumberland Farms Redevelopment
190 Bath Road
Brunswick, Maine

Dear Ms. Sousa:

Greenman-Pedersen Inc. (GPI) has prepared this letter to amend the Trip-Generation and Collision Summary Letter prepared by GPI dated July 7, 2016 for the proposed Cumberland Farms redevelopment located at 190 Bath Road in Brunswick, Maine. As requested by the Town of Brunswick, the collisions at the intersection of Bath Road at Thomas Point Road have been collected. Data was obtained from Maine Department of Transportation (MaineDOT) for the most recent three-year period available (2013 through 2015). A summary of the collision data is provided in an Updated Table 1, attached to this letter.

Based on the collision data, the unsignalized intersection of Bath Road at Thomas Point Road has experienced a total of 7 crashes over the three-year study period (2013 through 2015), 2.33 reported collisions per year on average. Of the 7 collisions, one resulted in injury and six resulted in property damage only. Six of the crashes were angle collisions and one was a rear-end collision. Twenty-nine percent of the reported crashes occurred during the weekday AM or weekday PM commuter peak periods and twenty-nine percent occurred during wet/icy conditions.

Should you have any questions, please contact me at (978) 570-2968.

Sincerely,

GREENMAN - PEDERSEN, INC.



Heather L. Monticup, P.E.
Senior Project Manager

Attachment(s)

UPDATED COLLISION SUMMARY LETTER

Cumberland Farms Redevelopment – Brunswick, Maine

ATTACHMENTS

UPDATED TABLE 1 – COLLISION SUMMARY

UPDATED COLLISION SUMMARY LETTER

Cumberland Farms Redevelopment – Brunswick, Maine

**Updated Table 1
COLLISION SUMMARY**

Location	Number of Collisions		Severity ^a				Collision Type ^b					Percent During	
	Total	Average per Year	PD	PI	F	CM	RE	HO	FO	Cyc	U	Commuter Peak ^c	Wet/Icy Conditions ^d
Bath Road (Rte 24) at Gurnet Road (Rte 24)	44	14.6	36	8	--	9	32	--	1	--	2	48%	14%
Bath Road at Cumberland Farms Dwy	3	1.00	3	--	--	--	3	--	--	--	--	0%	33%
Thomas Point Rd at Cumberland Farms Dwy	1	0.33	1	--	--	1	--	--	--	--	--	100%	100%
Bath Road at Thomas Point Rd	7	2.33	6	1	--	6	1	--	--	--	--	29%	29%

Source: MaineDOT (2013-2015).

^a PD = property damage only; PI = personal injury; F = fatality.

^b CM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; Cyc = cyclist; U = unknown.

^c Percent of vehicle incidents that occurred during the weekday AM and weekday PM commuter peak periods.

^d Represents the percentage of only "known" collisions occurring during inclement weather conditions.

GPI Greenman-Pedersen, Inc.

Engineering and Construction Services

REF.: MAX-2016046

July 7, 2016

Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

SUBJECT: Trip-Generation and Collision Summary Letter
Cumberland Farms Redevelopment
190 Bath Road
Brunswick, Maine

Dear Ms. Sousa:

Greenman-Pedersen Inc. (GPI) has prepared this letter to evaluate the expected trips associated with the proposed Cumberland Farms redevelopment located at 190 Bath Road in Brunswick, Maine. The subject sites consists of a Cumberland Farms facility that includes a $\pm 3,000$ square-foot (sf) convenience store with 4 Multi-Product Dispensers (MPDs) having eight (8) vehicle fueling positions (vfps) as well as $\pm 3,870$ sf of retail space which is occupied by an AT&T store ($\pm 2,250$ sf) and a Papa John's restaurant ($\pm 1,350$ sf). The development consists of razing the existing structures on the site and constructing a 4,786 sf convenience store with 5 MPDs having ten (10) vfps. Access and egress is currently provided via two driveways; one right-in/right-out only driveway on Bath Road and one full access/egress driveway on Thomas Point Road. Access and egress to the site will remain the same as part of the redevelopment, however, the Bath Road driveway will be modified/widened slightly to allow an easier right-turn movement onto Bath Road from the site. The subject site is located on the southeast quadrant of the signalized intersection of Bath Street (Route 24) at Gurnet Road (Route 24), bounded by Bath Road to the north, Thomas Point Road to the east, Gurnet Road (Route 24) to the west and a McDonald's restaurant to the south. The site location in relation to the surrounding roadways is shown on the map on Figure 1.

TRIP-GENERATION AND COLLISION SUMMARY LETTER

Cumberland Farms Redevelopment – Brunswick, Maine

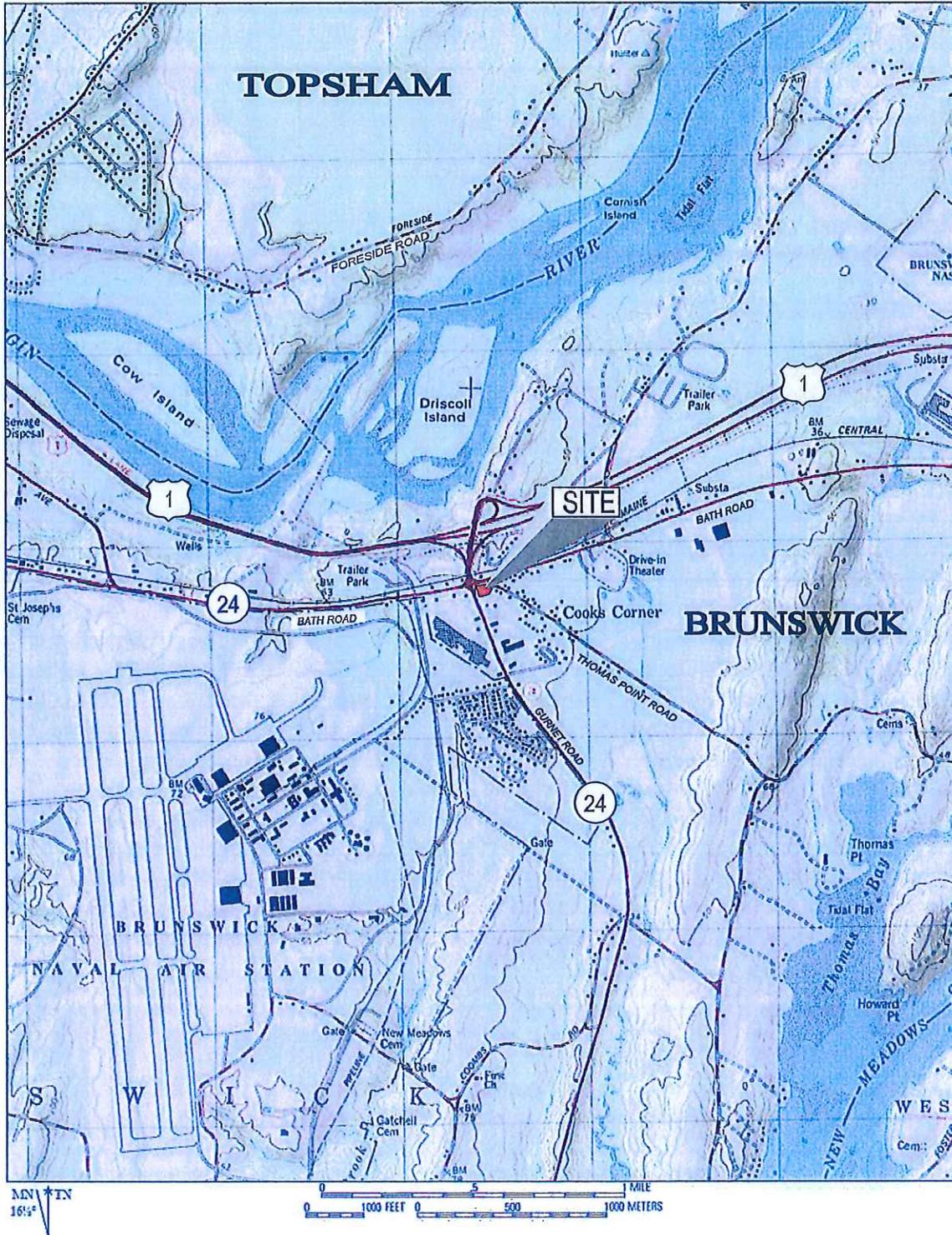


Figure 1
Site Location Map



Ms. Kathleen A. Sousa
July 7, 2016
Page 3 of 7

Existing Conditions

Geometry

Adjacent to the site, Bath Road consists of two general-purpose travel lanes in each direction and one general-purpose lane in each direction beyond the vicinity, with exclusive turn lanes at major intersections. Gurnet Road consists of one general purpose lane in the northbound direction and two general-purpose lanes in the southbound direction with a two-way left-turn lane along the roadway and additional exclusive turn lanes at major intersections. Thomas Point Road consists of one general-purpose lane in each direction with no pavements markings. The posted speed limit along Bath Road and Gurnet Road is 35 miles per hour (mph) and the posted speed limit along Thomas Point Road is 25 mph. Currently, the Cumberland Farms site contains two curb cuts; one on Bath Road and one on Thomas Point Road. Access will remain the same but the Bath Road curb-cut will be modified to provide both right-in and right-out access.

Collision History

Collision data for the study area intersections were obtained from Maine Department of Transportation (MaineDOT) for the most recent three-year period available (2013 through 2015). A summary of the collision data at the intersections is provided in Table 1.

The signalized intersection of Bath Road (Route 24) at Gurnet Road (Route 24) has experienced on average 14.6 reported collisions per year. Of the 44 collisions, eight have resulted in injury and thirty-six resulted in property damage only. Thirty-two of the crashes were rear end collisions, nine were angle collisions, two were reported as other and one was a collision with the curb. Forty-eight percent of the reported crashes occurred during the weekday AM or weekday PM commuter peak periods and fourteen percent occurred during wet conditions.

The intersection of Bath Road at the Cumberland Farms driveway has experienced on average 1 reported collision per year. None of the three collisions resulted in personal injury. All three of the crashes were rear end collisions. None of the reported crashes occurred during the weekday AM or weekday PM commuter peak periods and thirty-three percent occurred during snowy conditions.

The intersection of Thomas Point Road at Cumberland Farms driveway has experienced one collision over the three-year period. The collision did not result in personal injury. The crash was an angle collision and occurred during the AM commuter peak hour. The collision occurred during icy conditions.



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Table 1
COLLISION SUMMARY

Location	Number of Collisions		Severity ^a			Collision Type ^b						Percent During	
	Total	Average per Year	PD	PI	F	CM	RE	HO	FO	Cyc	U	Commuter Peak ^c	Wet/Icy Conditions ^d
Bath Road (Rte 24) at Gurnet Road (Rte 24)	44	14.6	36	8	--	9	32	--	1	--	2	48%	14%
Bath Road at Cumberland Farms Dwy	3	1.00	3	--	--	--	3	--	--	--	--	0%	33%
Thomas Point Rd at Cumberland Farms Dwy	1	0.33	1	--	--	1	--	--	--	--	--	100%	100%

Source: MaineDOT (2013-2015).

^aPD = property damage only; PI = personal injury; F = fatality.

^bCM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; Cyc = cyclist; U = unknown.

^cPercent of vehicle incidents that occurred during the weekday AM and weekday PM commuter peak periods.

^dRepresents the percentage of only "known" collisions occurring during inclement weather conditions.



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Design Conditions

The subject sites consists of a Cumberland Farms facility that includes a $\pm 3,000$ sf convenience store with 4 MPDs having eight (8) vfps as well as $\pm 3,870$ sf of retail space which is occupied by an AT&T store ($\pm 2,250$ sf) and a Papa John's Pizza restaurant ($\pm 1,350$ sf). The development consists of razing the existing structures on the site and constructing a 4,786 sf convenience store with 5 MPDs having ten (10) vfps.

Trip-Generation

For trip-generation purposes, traffic anticipated to be generated by the proposed Cumberland Farms redevelopment project was forecast using the trip rates contained in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*¹ utilizing Land Use Code (LUC) 853 (Convenience Market with Gasoline Pumps). Table 2 provides a trip-generation summary for the traffic anticipated to be generated by the proposed project. All trip-generation data are attached to this letter.

Not all of the vehicle trips expected to be generated by the proposed project represent *new* trips on the study area roadway system. Studies have shown that for developments such as the one proposed, a substantial portion of the site-generated vehicle trips are already present in the adjacent passing stream of traffic or are diverted from another route to the proposed site. Based on information published in the ITE *Trip Generation Handbook*, the average *pass-by* trip percentage is 63 percent during the weekday AM peak hour and 66 percent during the weekday PM peak hour for Convenience Markets with Gasoline Pumps (Land Use Code 853).² Table 2 summarizes the additional peak-hour trips expected to be generated as compared to the existing uses. All trip-generation data are attached to this letter.

¹ *Trip Generation Manual*, 9th Edition; Institute of Transportation Engineers; Washington, DC; 2012.

² *Trip Generation Handbook*; 3rd Edition; Institute of Transportation Engineers; Washington, DC; August 2014.

Table 2
TRIP-GENERATION SUMMARY

Time Period/Direction	Existing Trips			Proposed Trips	Additional Trips		
	Retail ^a	Restaurant ^b	Total ^c	Convenience / Gasoline ^d	Total Trips ^e	Pass-By Trips ^f	New Trips ^g
Weekday Daily	4,100	630	4,730	5,430	700	440	260
Weekday AM Peak Hour:							
<i>Enter</i>	65	26	91	83	(8)	(1)	(7)
<i>Exit</i>	<u>58</u>	<u>21</u>	<u>79</u>	<u>83</u>	<u>4</u>	<u>(1)</u>	<u>5</u>
<i>Total</i>	123	47	170	166	(4)	(2)	(2)
Weekday PM Peak Hour:							
<i>Enter</i>	82	13	95	96	1	0	1
<i>Exit</i>	<u>86</u>	<u>10</u>	<u>96</u>	<u>95</u>	<u>(1)</u>	<u>0</u>	<u>(1)</u>
<i>Total</i>	168	23	191	191	0	0	0
Saturday Daily	1,400	610	2,010	2,040	30	20	10
Saturday Midday Peak Hour:							
<i>Enter</i>	31	25	56	51	(5)	(4)	(1)
<i>Exit</i>	<u>32</u>	<u>23</u>	<u>55</u>	<u>49</u>	<u>(6)</u>	<u>(4)</u>	<u>(2)</u>
<i>Total</i>	63	48	111	100	(11)	(8)	(3)

^a ITE *Trip Generation Handbook*, Procedure for Estimating Multi-Use Trip Generation for ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for 8 vfps and ITE Land Use Code 826 (Specialty Retail Center) for 2,250 sf.

^b ITE *Trip Generation Handbook*, Procedure for Estimating Multi-Use Trip Generation for ITE Land Use Code 933 (Fast-Food Restaurant without Drive-Through Window) for 1,350 sf.

^c Existing Retail Trips (which includes the Convenience/Gasoline Trips) plus Restaurant Trips.

^d ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for 10 vfps.

^e Proposed Trips minus Total Existing Trips.

^f 63 percent of Total Additional Trips during the Weekday, AM peak hour, Saturday, and Saturday midday peak hour; 66 percent of Total Additional Trips during the weekday PM peak hour.

^g Additional Total Trips minus Pass-By Trips.

As shown in Table 2, the proposed Cumberland Farms redevelopment project as compared to the existing uses is expected to generate 2 less vehicle trips (7 less entering and 5 more exiting) during the weekday AM peak hour, the same vehicle trips (1 more entering and 1 less exiting) during the weekday PM peak hour, and 3 less vehicle trips (1 less entering and 2 less exiting) during the Saturday



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midday peak hour. Traffic decreases as a result of the redevelopment are expected to be approximately 1 less vehicle every 20 to 30 minutes during the peak hours.

Since the section of Bath Road where the Cumberland Farms driveway exists is not under state jurisdiction, a MaineDOT Traffic Movement Permit (TMP) and Application for Driveway/Entrance Permit is not expected to be required. Should you have any questions, or require additional information, please contact me at (978) 570-2968.

Sincerely,

GREENMAN – PEDERSEN, INC.

A handwritten signature in blue ink, appearing to read "H. Monticup", is written over the company name.

for Heather L. Monticup, P.E.
Senior Project Manager

Attachment(s)

TRIP-GENERATION AND COLLISION SUMMARY

Cumberland Farms Redevelopment – Brunswick, Maine

ATTACHMENTS

TRIP-GENERATION WORKSHEETS



Trip Generation Summary

Existing Gas Station = 8 Vehicle Fueling Positions
 Existing Retail = 2,250 Square Feet
 Existing Restaurant = 1,350 Square Feet
 Proposed Gas Station = 10 Vehicle Fueling Positions

	Existing Trips (External)			Proposed Trips	Additional Trips		
	Retail	Restaurant	Total Trips	Retail	Total Trips	Pass-By Trips	New Trips
Weekday Daily	4,102	630	4,732	5,426	694	437	257
Weekday AM Peak Hour	65	26	91	83	-8	-1	-7
	<u>58</u>	<u>21</u>	<u>78</u>	<u>83</u>	<u>5</u>	<u>-1</u>	<u>6</u>
	123	47	170	166	-4	-2	-1
Weekday PM Peak Hour	82	13	95	96	1	0	1
	<u>86</u>	<u>10</u>	<u>96</u>	<u>95</u>	<u>-1</u>	<u>0</u>	<u>-1</u>
	168	23	191	191	0	0	0
Saturday Daily	1,401	611	2,012	2,044	32	20	12
Saturday Midday Peak Hour	31	25	56	51	-5	-4	-2
	<u>32</u>	<u>23</u>	<u>55</u>	<u>49</u>	<u>-6</u>	<u>-4</u>	<u>-2</u>
	63	48	111	100	-11	-7	-4

Pass-By
Percentages
 LUC 853
 AM 63%
 PM 66%
 SAT 63%

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 853 - Convenience Market with Gasoline Pumps

Average Vehicle Trips Ends vs: Vehicle Fueling Positions
Independent Variable (X): 8

AVERAGE WEEKDAY DAILY

$$T = 542.60 * (X)$$

$$T = 542.60 * 8$$

$$T = 4340.80$$

$$T = 4,340 \text{ vehicle trips}$$

with 50% (2,170 vpd) entering and 50% (2,170 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 16.57 * (X)$$

$$T = 16.57 * 8$$

$$T = 132.56$$

$$T = 133 \text{ vehicle trips}$$

with 50% (67 vph) entering and 50% (66 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 19.07 * (X)$$

$$T = 19.07 * 8$$

$$T = 152.56$$

$$T = 153 \text{ vehicle trips}$$

with 50% (77 vph) entering and 50% (76 vph) exiting.

SATURDAY DAILY

$$T = 204.47 * (X)$$

$$T = 204.47 * 8$$

$$T = 1635.76$$

$$T = 1,636 \text{ vehicle trips}$$

with 50% (818 vpd) entering and 50% (818 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

$$T = 10.00 * (X)$$

$$T = 10.00 * 8$$

$$T = 80.00$$

$$T = 80 \text{ vehicle trips}$$

with 51% (41 vph) entering and 49% (39 vph) exiting.

Institute of Transportation Engineers (ITE)
Land Use Code (LUC) 826 - Specialty Retail Center

Average Vehicle Trips Ends vs: 1,000 Sq. Feet Gross Leasable Area
 Independent Variable (X): 2.250

AVERAGE WEEKDAY DAILY

$$T = 44.32 * (X)$$

$$T = 44.32 * 2.250$$

$$T = 99.72$$

T = 100 vehicle trips

with 50% (50 vpd) entering and 50% (50 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$\frac{\text{ITE LUC 820 Weekday Morning Trip Rate}}{\text{ITE LUC 820 Weekday Evening Trip Rate}} = \frac{\text{ITE LUC 826 Weekday Morning Trip Rate}}{\text{ITE LUC 826 Weekday Evening Trip Rate}}$$

$$\frac{0.96}{3.71} = \frac{(Y)}{2.71} \quad Y = 0.70123989$$

$$T = Y * 2.250$$

$$T = 1.578$$

T = 2 vehicle trips

with 62% (1 vph) entering and 38% (1 vph) exiting.

(same distribution split as ITE LUC 820 during the weekday morning peak hour of adjacent street traffic)

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 2.40 * (X) + 21.48$$

$$T = 2.40 * 2.250 + (21.48)$$

$$T = 26.88$$

T = 27 vehicle trips

with 44% (12 vph) entering and 56% (15 vph) exiting.

SATURDAY DAILY

$$T = 42.04 * (X)$$

$$T = 42.04 * 2.250$$

$$T = 94.59$$

T = 94 vehicle trips

with 50% (47 vpd) entering and 50% (47 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR

$$\frac{\text{ITE LUC 820 Saturday Midday Trip Rate}}{\text{ITE LUC 820 Saturday Daily Trip Rate}} = \frac{\text{ITE LUC 826 Saturday Midday Trip Rate}}{\text{ITE LUC 826 Saturday Daily Trip Rate}}$$

$$\frac{4.82}{49.97} = \frac{(Y)}{42.04} \quad Y = 4.05508905$$

$$T = Y * 2.250$$

$$T = 9.124$$

T = 9 vehicle trips

with 52% (5 vph) entering and 48% (4 vph) exiting.

(same distribution split as ITE LUC 820 during the Saturday midday peak hour of generator)

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 933 - Fast-Food Restaurant without Drive-Through Window

Average Vehicle Trips Ends vs: 1,000 Sq. Feet Gross Floor Area
Independent Variable (X): 1.350

AVERAGE WEEKDAY DAILY

T = 716.00 * (X)
T = 716.00 * 1.350
T = 966.60
T = 968 vehicle trips
with 50% (484 vpd) entering and 50% (484 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

T = 43.87 * (X)
T = 43.87 * 1.350
T = 59.22
T = 59 vehicle trips
with 60% (35 vph) entering and 40% (24 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

T = 26.15 * (X)
T = 26.15 * 1.350
T = 35.30
T = 35 vehicle trips
with 51% (18 vph) entering and 49% (17 vph) exiting.

SATURDAY DAILY

T = 696.00 * (X)
T = 696.00 * 1.350
T = 939.60
T = 940 vehicle trips
with 50% (470 vpd) entering and 50% (470 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

T = 54.55 * (X)
T = 54.55 * 1.350
T = 73.64
T = 74 vehicle trips
with 49% (36 vph) entering and 51% (38 vph) exiting.

Institute of Transportation Engineers (ITE)

Land Use Code (LUC) 853 - Convenience Market with Gasoline Pumps

Average Vehicle Trips Ends vs: Vehicle Fueling Positions
Independent Variable (X): 10

AVERAGE WEEKDAY DAILY

$$T = 542.60 * (X)$$

$$T = 542.60 * 10$$

$$T = 5426.00$$

T = 5,426 vehicle trips

with 50% (2,713 vpd) entering and 50% (2,713 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 16.57 * (X)$$

$$T = 16.57 * 10$$

$$T = 165.70$$

T = 166 vehicle trips

with 50% (83 vph) entering and 50% (83 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 19.07 * (X)$$

$$T = 19.07 * 10$$

$$T = 190.70$$

T = 191 vehicle trips

with 50% (96 vph) entering and 50% (95 vph) exiting.

SATURDAY DAILY

$$T = 204.47 * (X)$$

$$T = 204.47 * 10$$

$$T = 2044.70$$

T = 2,044 vehicle trips

with 50% (1,022 vpd) entering and 50% (1,022 vpd) exiting.

SATURDAY MIDDAY PEAK HOUR OF GENERATOR

$$T = 10.00 * (X)$$

$$T = 10.00 * 10$$

$$T = 100.00$$

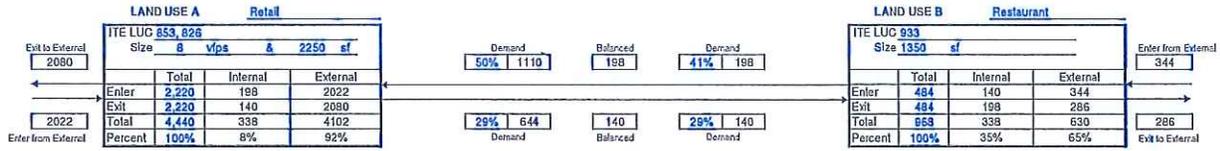
T = 100 vehicle trips

with 51% (51 vph) entering and 49% (49 vph) exiting.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Weekday Daily



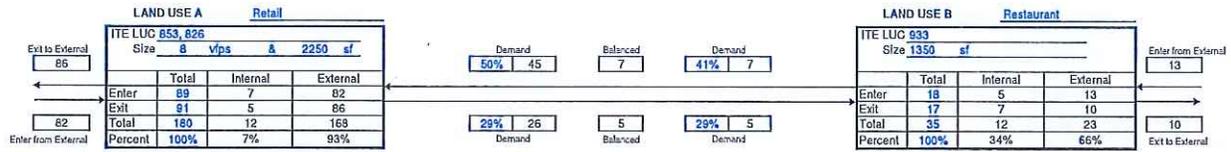
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	2022	2080	4102	4440	
Land Use B	344	286	630	968	
TOTAL	2366	2366	4732	5408	13%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Weekday PM Peak Hour



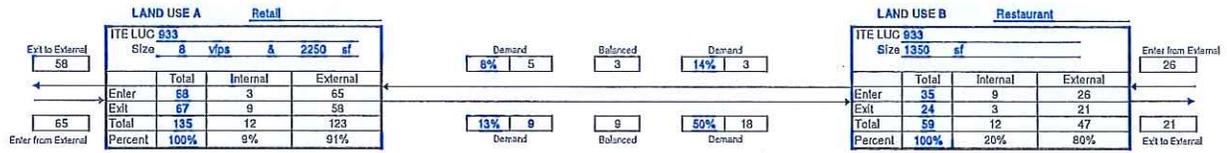
	Enter	Exit	Total	Single-Use Trip Gen Est.	External Capture
Land Use A	82	86	168	180	
Land Use B	13	10	23	35	
TOTAL	95	96	191	215	11%

Based on ITE *Trip Generation Handbook*, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Weekday AM Peak Hour



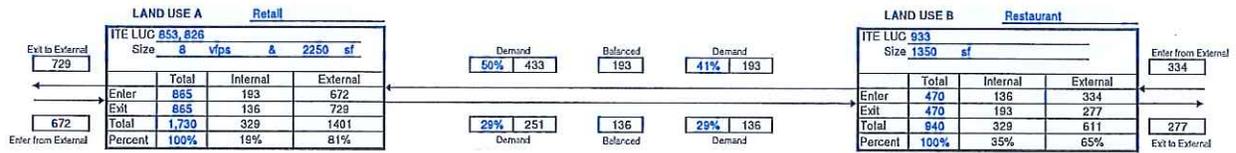
	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	65	58	123	135	
Land Use B	26	21	47	59	
TOTAL	91	78	170	194	13%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Theriault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Saturday Daily



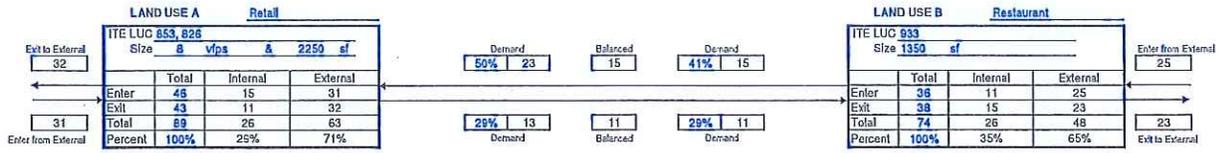
	Enter	Exit	Total	Single-Use Trip Gen Est.	
Land Use A	672	729	1401	1730	
Land Use B	334	277	611	940	Internal Capture
TOTAL	1006	1006	2012	2670	25%

Based on ITE Trip Generation Handbook, June 2004.

Analyst: Susannah E. Therault
 Date: May 12, 2016

**MULTI-USE DEVELOPMENT
 TRIP GENERATION
 AND INTERNAL CAPTURE SUMMARY**

Name of Dvlpt: Cumberland Farms
 Time Period: Saturday Midday Peak Hour



	Enter	Exit	Total	Single-Use Trip Gen Est.	Internal Capture
Land Use A	31	32	63	89	
Land Use B	25	23	48	74	
TOTAL	56	55	111	163	32%

Based on ITE Trip Generation Handbook, June 2004.

**Table F.14 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period
Land Use Code 851—Convenience Market (Open 24 Hours)**

SIZE (1,000 SQ. FT. GFA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
3	Overland Park, KS	Aug. 1987	68	4:30-5:30 p.m.	34	53	13	66	—	—
3	Overland Park, KS	July 1987	68	4:30-5:30 p.m.	28	50	22	72	—	—
~1.9	Billings, MT	1987	461	4:00-6:00 p.m.	62	13	25	38	—	ITE Montana Section Tech Comm
<50.0	Chicago suburbs, IL	1987	72	3:00-6:00 p.m.	28	—	—	72	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	54	3:00-6:00 p.m.	78	—	—	22	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	34	3:00-6:00 p.m.	69	—	—	31	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	100	3:00-6:00 p.m.	63	—	—	37	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	43	3:00-6:00 p.m.	43	—	—	57	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	135	3:00-6:00 p.m.	39	—	—	61	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	74	3:00-6:00 p.m.	53	—	—	47	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	80	3:00-6:00 p.m.	64	—	—	36	—	Kenig, O'Hara, Humes, Flock

Average Pass-By Trip Percentage: 51

"—" means no data were provided

**Table F.15 Pass-By and Non-Pass-By Trips Weekday, AM Peak Period
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. GFA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	7:00-9:00 a.m.	54	11	35	46	1,240	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	7:00-9:00 a.m.	48	17	35	52	1,210	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	47	7:00-9:00 a.m.	62	19	19	38	1,705	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	—	7:00-9:00 a.m.	72	15	13	28	940	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	49	7:00-9:00 a.m.	66	16	18	34	990	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	62	7:00-9:00 a.m.	74	10	16	26	790	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	58	7:00-9:00 a.m.	64	5	31	36	1,255	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	79	7:00-9:00 a.m.	56	6	38	44	635	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	49	7:00-9:00 a.m.	67	4	29	33	1,985	Barton-Aschman Assoc.

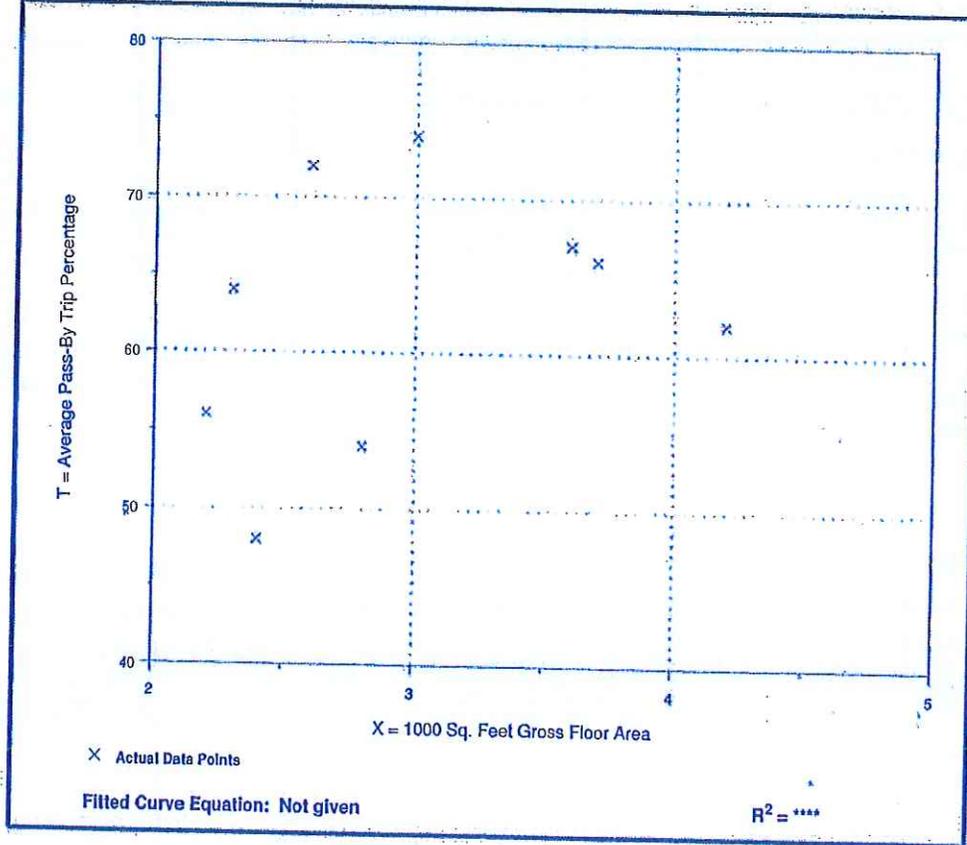
Average Pass-By Trip Percentage: 63

"—" means no data were provided

Figure F.11 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: 1000 Sq. Feet Gross Floor Area
On a: Weekday, A.M. Peak Period
Number of Studies: 9
Average 1000 Sq. Feet GFA: 3

Data Plot



**Table F.16 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. GPA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	4:00–6:00 p.m.	62	11	27	38	2,075	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	4:00–6:00 p.m.	50	13	29	42	2,655	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	61	4:00–6:00 p.m.	58	26	16	42	2,300	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	68	4:00–6:00 p.m.	67	15	18	33	950	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	70	4:00–6:00 p.m.	61	16	23	39	2,175	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	80	4:00–6:00 p.m.	65	15	20	35	1,165	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	67	4:00–6:00 p.m.	57	16	27	43	1,954	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	115	4:00–6:00 p.m.	40	16	36	52	820	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	60	4:00–6:00 p.m.	56	17	27	44	2,505	Barton-Aschman Assoc.
2.6	Seminole Co., FL	1989	82	4:00–6:00 p.m.	73	20	7	27	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	98	4:00–6:00 p.m.	81	15	4	19	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	115	4:00–6:00 p.m.	69	16	15	31	—	Tipton Associates Inc.
2.6	Volusia Co., FL	1989	98	4:00–6:00 p.m.	74	15	11	26	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	4:00–6:00 p.m.	74	24	2	26	—	Tipton Associates Inc.
2.7	Volusia Co., FL	1989	82	4:00–6:00 p.m.	87	8	5	13	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	99	2:00–4:00 p.m.	64	28	8	36	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	2:00–4:00 p.m.	68	21	11	32	—	Tipton Associates Inc.

Average Pass-By Trip Percentage: 66

“—” means no data were provided

Figure F.12 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: 1000 Sq. Feet Gross Floor Area
On a: Weekday, P.M. Peak Period
Number of Studies: 17
Average 1000 Sq. Feet GFA: 3

Data Plot

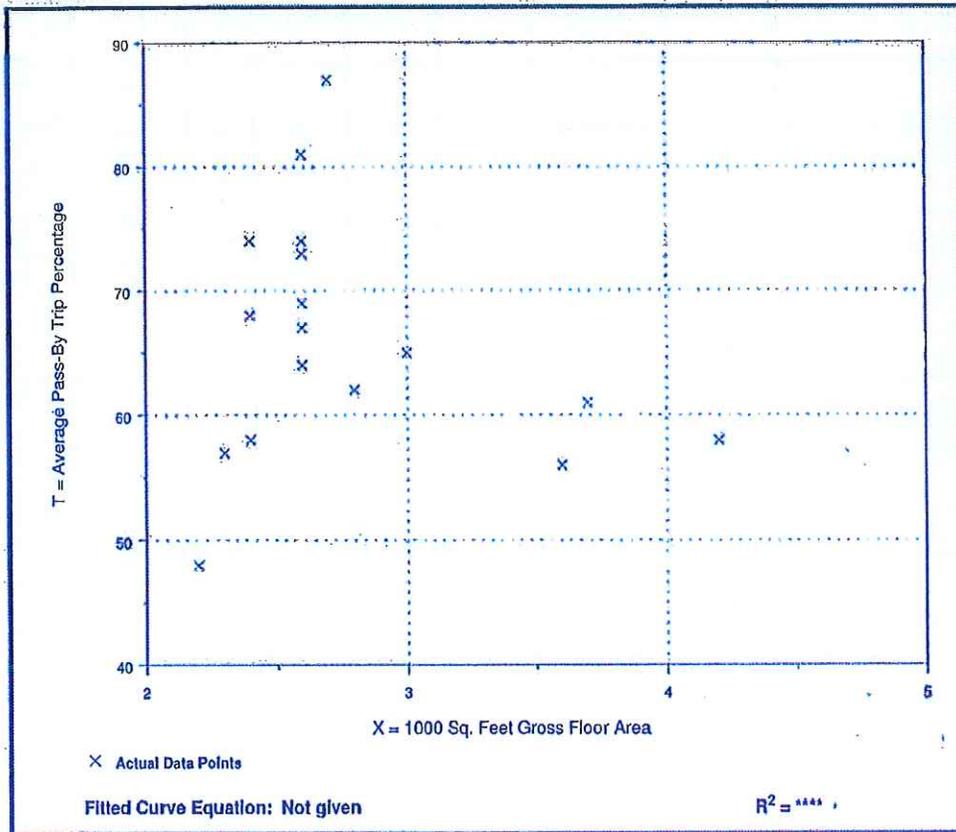
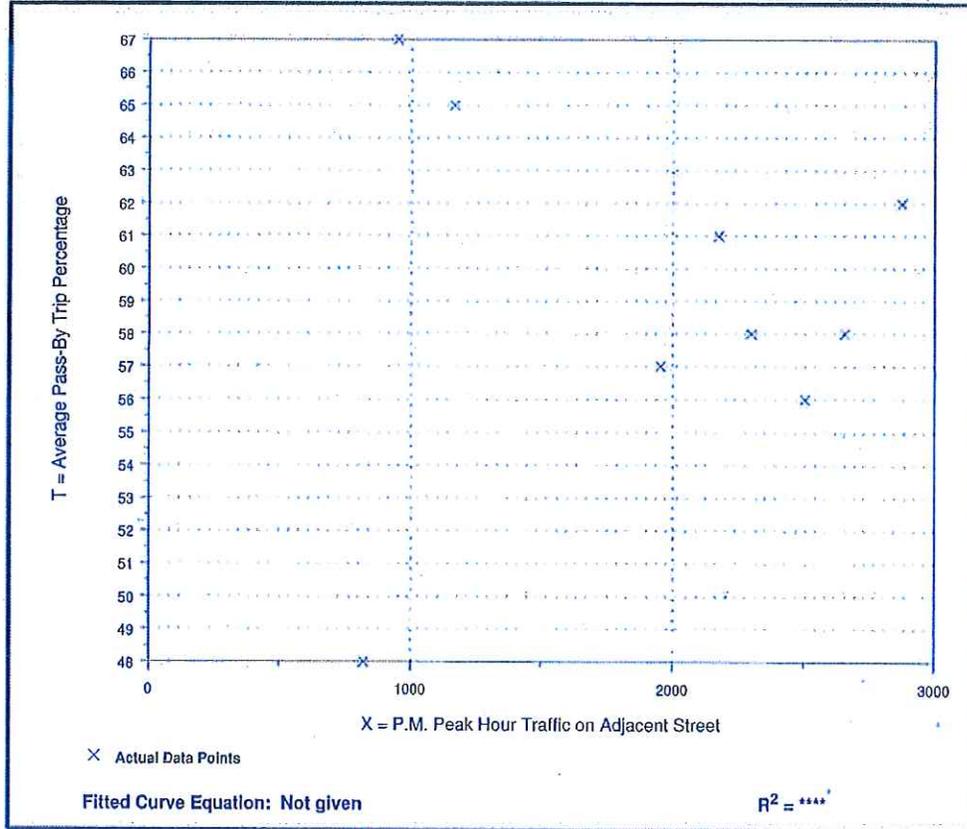


Figure F.13 Convenience Market with Gasoline Pumps (853)

Average Pass-By Trip Percentage vs: P.M. Peak Hour Traffic on Adjacent Street
On a: Weekday, P.M. Peak Period
Number of Studies: 9
Avg. P.M. Peak Hr. Traf. on Adj. Street: 1,933

Data Plot



8

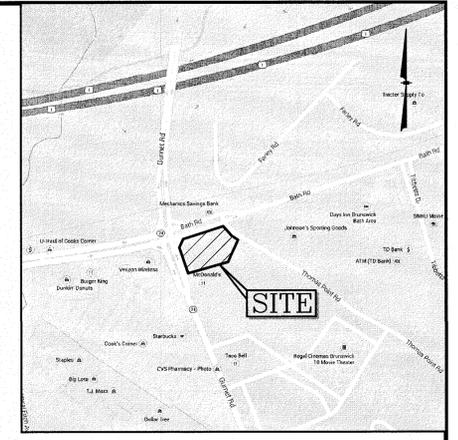
**CUMBERLAND FARMS SITE PLAN APPLICATION
190 BATH ROAD, BRUNSWICK, MAINE**

WAIVER REQUESTS
(Revised)

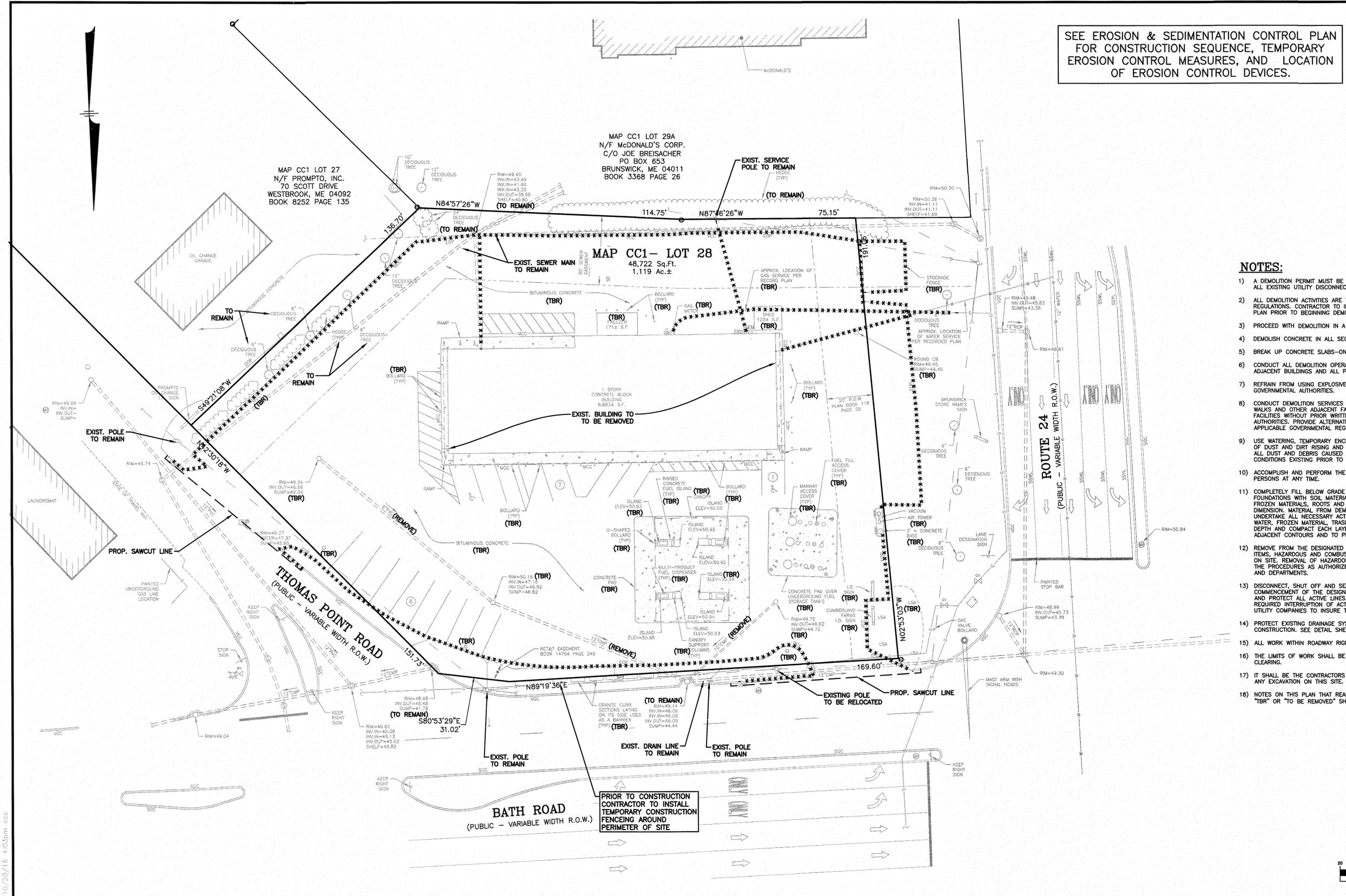
Pursuant to Section 205.3.A. of the Brunswick Zoning Ordinance, and due to the unique shape of the subject parcel, the required location of the fuel service area, and the location of the sewer easement on the south side of the parcel, Cumberland Farms respectfully requests the following waivers from the Cook's Corner Design Standards:

1. Corner Lot Treatment: The proposed redesign meets the required minimum setbacks from all street frontages. Due to the unique shape of the parcel, the location of the sewer easement, and for safety reasons, the fuel service area must however be located in the front of the store with clear visibility from the retail cashier location. As such, Cumberland Farms requests a waiver from the design standard requiring no parking, vehicular travel, or service areas to be located between the building and property lines on both streets.
2. Corner Buildings: The design of the redeveloped structure is two-story with a pitched roof and dormers. Due to the nature of the business as well as required parking, the second story does not have useable floor area and Cumberland Farms requests a waiver from this design standard.
3. Entrance (Corner Lot): Due to the unique shape of the parcel and the necessary location of the placement of the fuel service area within a straight visual line from the retail cashier, Cumberland Farms requests a waiver from the design requirement that the entrance be located on the corner.
4. Side Lot Parking: Due to the unique shape of the parcel, some of the parking spaces along the sideline will extend closer to Thomas Point Road than the front façade of the building, although not closer than the canopy structure for the fuel service area. To the extent that a waiver is required, Cumberland Farms requests a waiver from this design standard.
5. Relationship of Building to Thomas Point Road: Due to the unique shape of the subject parcel, the need to orient the retail building facing Bath Road, and the location of the underground fuel storage tanks, the side of the structure facing Thomas Point Road is further than 25 feet from the property line. The Design Guidelines reference the "front lot line" and Thomas Road is the side lot line for this project. To the extent that a waiver is required, Cumberland Farms request such waiver.
6. Relationship of Building to Bath Road: Due to the unique shape of the parcel, the required location of the fuel service area, and the need for vehicles to safely access the fuel service area, the building is located approximately 112 feet from the front lot line which is further than the maximum 100-foot setback in the Design Guidelines. Cumberland Farms therefore requests a waiver from this Design Standard.
7. Public Sidewalks and Esplanades: Due to the unique shape of the parcel, the required location of the fuel service area, and the location of the sewer easement, there is inadequate room for a sidewalk along the public streets.

SEE EROSION & SEDIMENTATION CONTROL PLAN FOR CONSTRUCTION SEQUENCE, TEMPORARY EROSION CONTROL MEASURES, AND LOCATION OF EROSION CONTROL DEVICES.



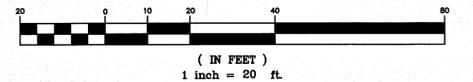
LOCATION MAP (NOT TO SCALE)



NOTES:

- 1) A DEMOLITION PERMIT MUST BE OBTAINED FROM THE TOWN OF BRUNSWICK PRIOR TO COMMENCEMENT OF WORK. ALL EXISTING UTILITY DISCONNECTIONS MUST BE COORDINATED WITH RESPECTIVE UTILITY COMPANIES.
- 2) ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN STRICT ADHERENCE TO ALL FEDERAL, STATE AND LOCAL REGULATIONS. CONTRACTOR TO INSTALL EROSION CONTROL DEVICES IN ACCORDANCE WITH GRADING & DRAINAGE PLAN PRIOR TO BEGINNING DEMOLITION ACTIVITIES.
- 3) PROCEED WITH DEMOLITION IN A SYSTEMATIC MANNER, FROM THE TOP OF THE STRUCTURE(S) TO THE GROUND.
- 4) DEMOLISH CONCRETE IN ALL SECTIONS.
- 5) BREAK UP CONCRETE SLABS-ON-GRADE, UNLESS OTHERWISE DIRECTED BY THE CONSTRUCTION MANAGER.
- 6) CONDUCT ALL DEMOLITION OPERATIONS IN A MANNER THAT WILL PREVENT INJURY, DAMAGE TO STRUCTURES, ADJACENT BUILDINGS AND ALL PERSONS.
- 7) REFRAIN FROM USING EXPLOSIVES WITHOUT PRIOR WRITTEN CONSENT OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES.
- 8) CONDUCT DEMOLITION SERVICES IN SUCH A MANNER TO INSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS AND OTHER ADJACENT FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS OR OTHER OCCUPIED FACILITIES WITHOUT PRIOR WRITTEN PERMISSION OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES. PROVIDE ALTERNATIVE ROUTES AROUND CLOSED OR OBSTRUCTED TRAFFIC WAYS IF REQUIRED BY APPLICABLE GOVERNMENTAL REGULATIONS.
- 9) USE WATERING, TEMPORARY ENCLOSURES AND OTHER SUITABLE METHODS, AS NECESSARY TO LIMIT THE AMOUNT OF DUST AND DIRT RISING AND SCATTERING IN THE AIR. CLEAN ADJACENT STRUCTURES AND IMPROVEMENTS OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. RETURN ALL ADJACENT AREAS TO THE CONDITIONS EXISTING PRIOR TO THE START OF WORK.
- 10) ACCOMPLISH AND PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE UNAUTHORIZED ENTRY OF PERSONS AT ANY TIME.
- 11) COMPLETELY FILL BELOW GRADE AREAS AND VOIDS RESULTING FROM THE DEMOLITION OF STRUCTURES AND FOUNDATIONS WITH SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. STONES USED WILL NOT BE LARGER THAN 6 INCHES IN DIMENSION. MATERIAL FROM DEMOLITION MAY NOT BE USED AS FILL. PRIOR TO PLACEMENT OF FILL MATERIALS, UNDERTAKE ALL NECESSARY ACTION IN ORDER TO INSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROZEN MATERIAL, TRASH, DEBRIS. PLACE FILL MATERIALS LAYERS NOT EXCEEDING 6 INCHES IN LOOSE DEPTH AND COMPACT EACH LAYER AT PLACEMENT TO 95% OPTIMUM DENSITY, GRADE SURFACE TO MEET ADJACENT CONTOURS AND TO PROVIDE SURFACE DRAINAGE.
- 12) REMOVE FROM THE DESIGNATED SITE, AT THE EARLIEST POSSIBLE TIME, ALL DEBRIS RUBBISH, SALVAGEABLE ITEMS, HAZARDOUS AND COMBUSTIBLE SERVICES. REMOVED MATERIALS MAY NOT BE STORED, SOLD OR BURNED ON SITE. REMOVAL OF HAZARDOUS AND COMBUSTIBLE MATERIALS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROCEDURES AS AUTHORIZED BY THE FIRE DEPARTMENT OR OTHER APPROPRIATE REGULATORY AGENCIES AND DEPARTMENTS.
- 13) DISCONNECT, SHUT OFF AND SEAL ALL UTILITIES SERVING THE STRUCTURE(S) TO BE DEMOLISHED BEFORE THE COMMENCEMENT OF THE DESIGNATED DEMOLITION. MARK FOR POSITION ALL UTILITY DRAINAGE AND SANITARY LINES AND PROTECT ALL ACTIVE LINES. CLEARLY IDENTIFY BEFORE THE COMMENCEMENT OF DEMOLITION SERVICES THE REQUIRED INTERRUPTION OF ACTIVE SYSTEMS THAT MAY AFFECT OTHER PARTIES, AND NOTIFY ALL APPLICABLE UTILITY COMPANIES TO INSURE THE CONTINUATION OF SERVICE.
- 14) PROTECT EXISTING DRAINAGE SYSTEM(S) AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING DURING CONSTRUCTION. SEE DETAIL SHEETS FOR EROSION CONTROL DEVICES.
- 15) ALL WORK WITHIN ROADWAY RIGHT-OF-WAYS TO CONFORM TO TOWN OF BRUNSWICK AND MDOT STANDARDS.
- 16) THE LIMITS OF WORK SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO THE START OF CONSTRUCTION OR SITE CLEARING.
- 17) IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO NOTIFY "DIG SAFE" (1-888-344-7233) 72 HOURS PRIOR TO ANY EXCAVATION ON THIS SITE.
- 18) NOTES ON THIS PLAN THAT READ "TBR" REPRESENT FEATURES TO BE REMOVED. ANY FEATURES NOT LABELED "TBR" OR "TO BE REMOVED" SHALL BE CONSIDERED EXISTING TO REMAIN.

GRAPHIC SCALE



LEGEND

○	UTILITY POLE	—	OVERHEAD SERVICE WIRES
⊙	DRAIN MANHOLE	—	DOUBLE SOLID YELLOW LINE
⊙	SEWER MANHOLE	—	SINGLE SOLID WHITE LINE
⊙	TELEPHONE MANHOLE	—	SIGN
⊙	CATCH BASIN	—	OBSERVATION WELL
—	WATER LINE	—	TREELINE
—	WATER VALVE	—	CONTOUR ELEVATION
—	FIRE HYDRANT	—	GAS LINE
—	GAS VALVE	(TBR)	TO BE REMOVED
—		—	TO BE REMOVED



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION
1	8/26/16	CMT	MISC. REVISIONS PER TOWN COMMENTS
2	10/20/16	CMT	REV'S PER TOWN COMMENTS

48,722 FEET
1.119 ACRES
V# L0589
Store# 5585
Gas Station# 1818

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44 Stiles Road, Suite One
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100 CROSSING BLVD.
FRAMINGHAM, MA 01702

190 BATH ROAD
BRUNSWICK, MAINE 04011

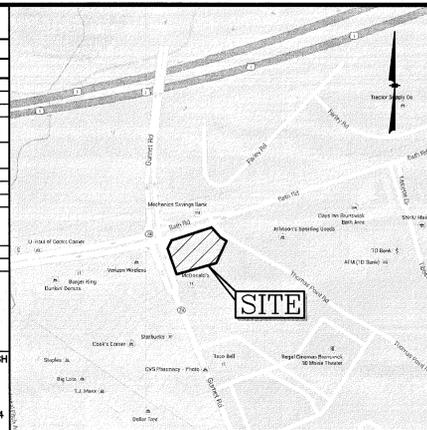
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DEMOLITION PLAN
CFG03.0

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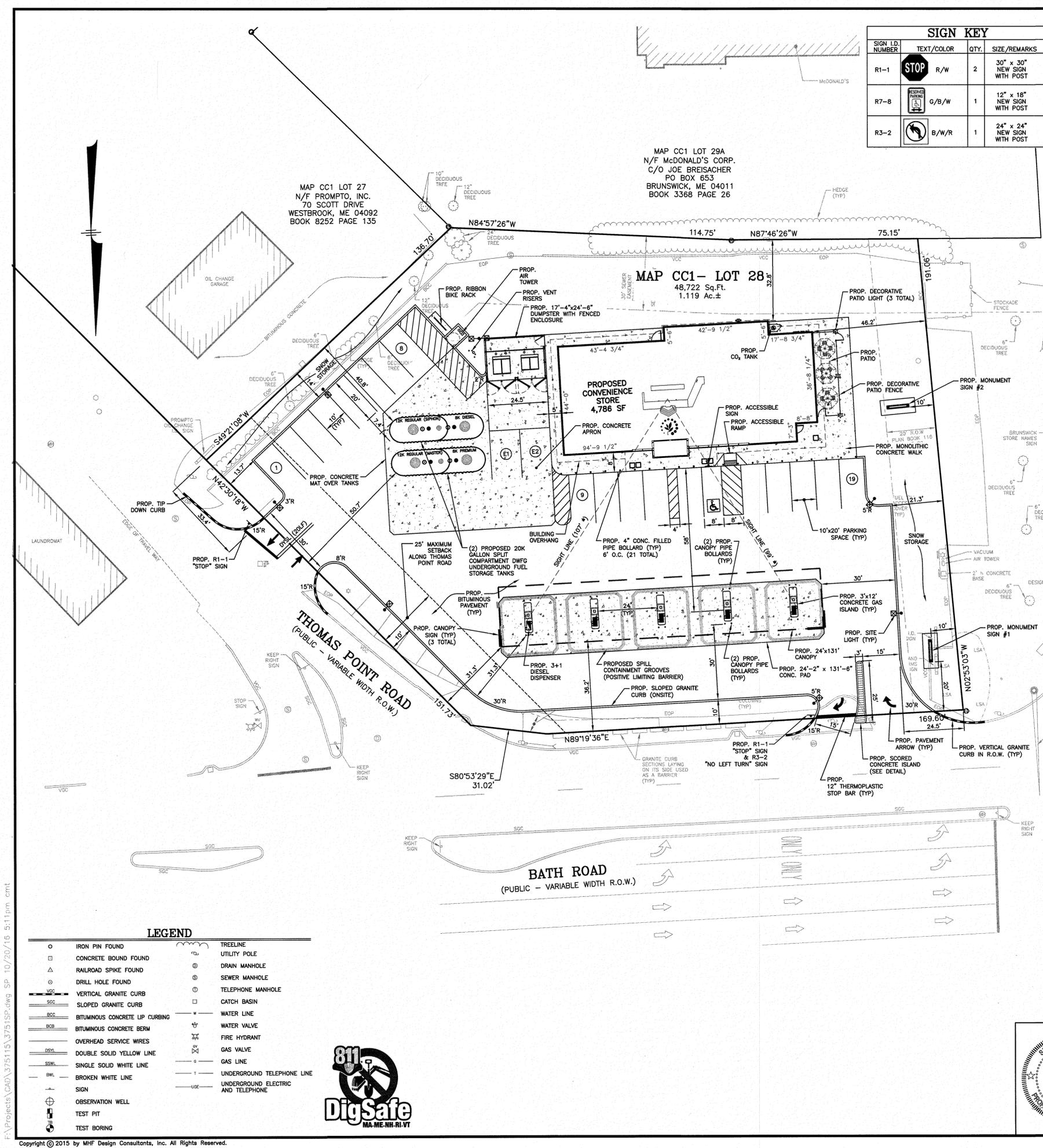
TABLE OF ZONING REGULATIONS - BRUNSWICK, ME

ZONE: COOKS CORNER CENTER DISTRICT (CCD)		
DESCRIPTION	REQUIRED	PROVIDED
MINIMUM LOT AREA	15,000 SF	48,722 SF
MINIMUM LOT FRONTAGE	60'	199.03' (ALONG BATH ROAD)
MINIMUM FRONT YARD BUILDING SETBACK ON THOMAS POINT ROAD	0'	31.3' (CANOPY)
MAXIMUM FRONT YARD BUILDING SETBACK ON THOMAS POINT ROAD	25'	MIN=31.3' (CANOPY) 104.4' (C-STORE)
MAXIMUM FRONT YARD BUILDING SETBACK ON BATH ROAD	100'	(C-STORE) MIN=108.7' (CANOPY) MIN=36.2'
MINIMUM SIDE YARD BUILDING SETBACK	0'	46.2' (BUILDING)
MINIMUM REAR YARD BUILDING SETBACK	0' ON CORNER LOT	32.8' (BUILDING)
PARKING SPACE DIMENSIONS	NOT PROVIDED	10'x20'
MINIMUM NUMBER PARKING SPACES	4 SPACES/1000 SF FLOOR AREA = (4,786 / 1,000) x 4 = 21 SPACES REQUIRED	21 SPACES (INCLUDES 2 EMPLOYEE SPACES)
MAXIMUM BUILDING HEIGHT	40'	32'-10"
MAXIMUM LOT COVERAGE	80%	33,333 SF (68.4%)
FREESTANDING SIGN AREA, HEIGHT & SETBACK	1 SIGN ALLOWED PER 250 LF OF LOT FRONTAGE: (1) MONUMENT SIGN, MAX. 32 SF, 10' HIGH, NO MIN. FRONT SETBACK OR (1) POLE SIGN, MAX. 25 SF, 15' HIGH, 5' SETBACK TO SIDE & REAR PROPERTY LINES & NO MIN. FRONT SETBACK	543.41 LF FRONTAGE (2) 9' HIGH, 31.5 SF MONUMENT SIGNS PROPOSED
WALL SIGN AREA (COOKS CORNER CENTER)	25 SF MAX. WALL SIGNAGE OR 10% OF PRINCIPAL WALL FACADE, WHICHEVER IS GREATER	103.5' WALL FACADE x 13'-10" HIGH = 1,431 SF 10% = 143 SF ALLOWED (1) 37.6 SF SIGN FACING BATH ROAD (1) 24.5 SF SIGN FACING ROUTE 24 TOTAL WALL SIGNS = 62.1 SF
GASOLINE CANOPY SIGNS	15% MAX. SF/SIDE OF CANOPY OR TOTAL AREA SHALL NOT EXCEED 9% OF TOTAL SF OF ENTIRE CANOPY	24' WIDE CANOPY x 3'-8" HIGH = 88 SF 15% = 13.2 SF ALLOWED/SIDE (3) 11 SF SIGNS PROPOSED



SIGN KEY

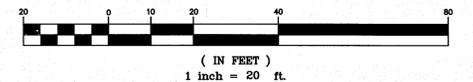
SIGN I.D. NUMBER	TEXT/COLOR	QTY.	SIZE/REMARKS
R1-1	STOP R/W	2	30" x 30" NEW SIGN WITH POST
R7-8	G/B/W	1	12" x 18" NEW SIGN WITH POST
R3-2	B/W/R	1	24" x 24" NEW SIGN WITH POST



NOTES:

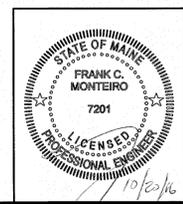
- THE BOUNDARY INFORMATION SHOWN HEREON FOR MAP CC1 LOT 28 IS THE RESULT OF A STANDARD BOUNDARY SURVEY BY THIS OFFICE.
- ZONING DISTRICT: COOKS CORNER CENTER DISTRICT (CCD)
- LOT AREA = 48,722 Sq.Ft.± = 1.119 Ac.±
- EXISTING USE: RETAIL MOTOR FUEL OUTLET WHICH INCLUDES A 6,883 SF CONVENIENCE STORE, A FUEL DISPENSING AREA WITH 4 DISPENSERS (8 FUELING POSITIONS), AN OVERHEAD CANOPY, AN AT&T RETAIL STORE AND A PAPA JOHN'S PIZZA.
PROPOSED USE: RETAIL MOTOR FUEL OUTLET WHICH INCLUDES A 4,786 SF CONVENIENCE STORE AND A FUEL DISPENSING AREA WITH 5 DISPENSERS (10 FUELING POSITIONS) AND AN OVERHEAD CANOPY.
- ALL BUILDINGS AND SITE CONSTRUCTION SHALL COMPLY WITH THE RULES AND REGULATIONS OF THE AMERICANS WITH DISABILITIES ACT (ADA) AS PUBLISHED IN THE FEDERAL REGISTER, VOL. 56, NO. 144, DATED JULY 26, 1991, REVISED JULY 23, 2004.
- THE LOCATIONS OF EXISTING SUBSURFACE UTILITIES SHOWN ON THIS PLAN WERE COMPILED FROM AVAILABLE RECORD DRAWINGS AND ARE NOT WARRANTED TO BE CORRECT. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING SUBSURFACE UTILITIES PRIOR TO PERFORMING ANY WORK.
- WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIGSAFE 1-888-344-7233 PRIOR TO ANY EXCAVATION.
- ALL CONSTRUCTION SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF BRUNSWICK AND THE STATE OF MAINE.
- THE SURVEY TRACT IS LOCATED IN ZONE "X", AREA OF MINIMAL FLOODING, AS SHOWN ON THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL #230042 0015 B FOR THE TOWN OF BRUNSWICK, MAINE CUMBERLAND COUNTY. EFFECTIVE DATE: JANUARY 3, 1986.
- ALL CONSTRUCTION SHALL CONFORM TO THESE PLANS AND THE STANDARD CONSTRUCTION DRAWINGS AS SUPPLIED BY CUMBERLAND FARMS.
- A SIGN PERMIT SHALL BE OBTAINED PRIOR TO INSTALLATION. SIGNAGE SHALL COMPLY WITH THE LAND USE CODE.
- THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THESE PLANS, PREPARED BY MHF DESIGN CONSULTANTS, INC., DO NOT EXTEND TO OR INCLUDE SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR THEIR EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF THE SURVEYOR AND/OR ENGINEER AS INCLUDED IN THE PLAN SET DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE AND/OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND/OR LOCAL REGULATIONS.
- ALL UNDERGROUND STORAGE TANKS, PRODUCT PIPING AND VENT LINES SHALL COMPLY WITH CURRENT STATE AND E.P.A. REGULATIONS.
- ALL SNOW SHALL BE STORED IN THE AREA(S) DEPICTED ON THIS PLAN AS SNOW STORAGE AREAS. IN THE EVENT THAT THE AREA(S) APPROVED FOR SNOW STORAGE BECOME FULL, THE OWNER SHALL REASONABLY REMOVE EXCESS SNOW FROM THE SITE, AND SHALL NOT ALLOW SNOW TO BE STORED WITHIN PARKING LOTS OR TRAVEL AISLES.
- ALL WASTE MATERIALS AND RECYCLABLES SHALL BE CONTAINED WITHIN THE BUILDING(S) OR APPROVED STORAGE FACILITIES AND SHALL NOT BE OTHERWISE STORED ON PROPERTY.
- EXISTING IMPERVIOUS COVERAGE = 44,566 SF± (91.5%)
PROPOSED IMPERVIOUS COVERAGE = 33,333 SF± (68.4%)

GRAPHIC SCALE



LEGEND

○	IRON PIN FOUND	—	TREELINE
□	CONCRETE BOUND FOUND	—	UTILITY POLE
△	RAILROAD SPIKE FOUND	—	DRAIN MANHOLE
⊙	DRILL HOLE FOUND	—	SEWER MANHOLE
—	VERTICAL GRANITE CURB	—	TELEPHONE MANHOLE
—	SLOPED GRANITE CURB	—	CATCH BASIN
—	BITUMINOUS CONCRETE LIP CURBING	—	WATER LINE
—	BITUMINOUS CONCRETE BERM	—	WATER VALVE
—	OVERHEAD SERVICE WIRES	—	FIRE HYDRANT
—	DOUBLE SOLID YELLOW LINE	—	GAS VALVE
—	SINGLE SOLID WHITE LINE	—	GAS LINE
—	BROKEN WHITE LINE	—	UNDERGROUND TELEPHONE LINE
—	SIGN	—	UNDERGROUND ELECTRIC AND TELEPHONE
—	OBSERVATION WELL		
—	TEST PIT		
—	TEST BORING		



REVISIONS

NO.	DATE	REV. BY.	DESCRIPTION
1	8/26/16	CMT	MISC. REVISIONS PER TOWN COMMENTS
2	10/20/16	CMT	REV'S PER TOWN COMMENTS

48,722 FEET
1.119 ACRES
V# L0589
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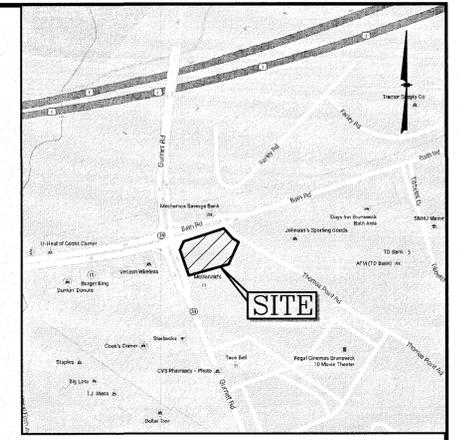
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CHECKED BY: FCM

SITE PLAN
CFG04.0

MHF PROJECT NO. 375115 SHEET 4 OF 12

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FROM: STRUCTURE NUMBER	PIPE SIZE (inches)	TYPE OF PIPE	APPROX. PIPE LENGTH (feet)	SLOPE OF PIPE (ft./ft.)	TO: STRUCTURE NUMBER
CB-2	12	HDPE	58	0.005	CB-1
CB-1	12	HDPE	58	0.005	DMH-1
CB-5	12	HDPE	63	0.005	CB-4
CB-4	12	HDPE	103	0.005	CB-3
CB-3	12	HDPE	65	0.005	DMH-1
DMH-1	12	HDPE	15	0.005	EX. CB-A



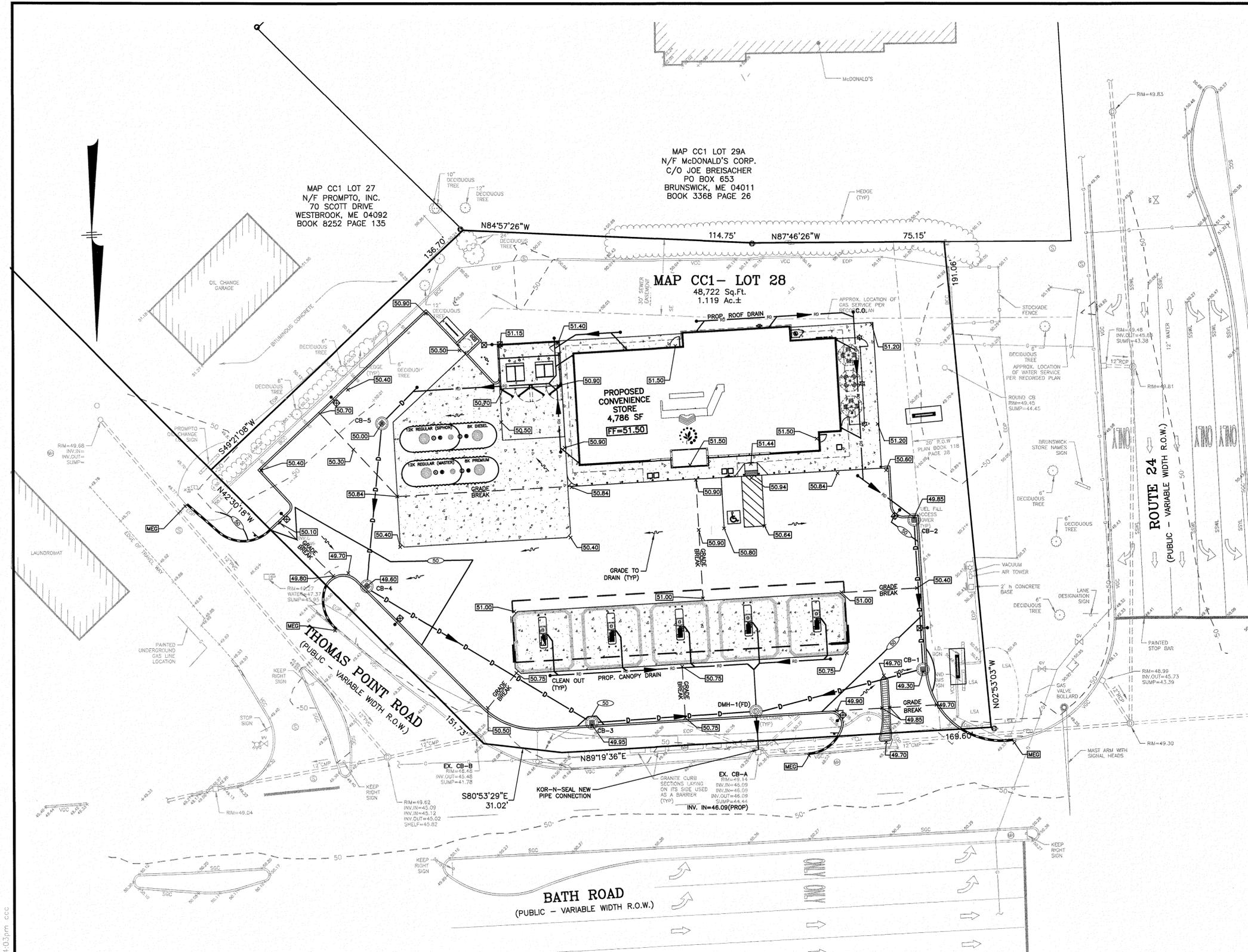
DRAINAGE STRUCTURES

- CB-1 RIM=49.30 INV.IN=46.60 INV.OUT=46.50
- CB-2 RIM=49.85 INV.IN=47.35(RD) INV.OUT=46.85
- CB-3 RIM=49.95 INV.IN=46.60 INV.OUT=46.50
- CB-4 RIM=49.60 INV.IN=47.20 INV.OUT=47.10
- CB-5 RIM=50.00 INV.IN=48.00(RD) INV.OUT=47.50
- DMH-1 (FD) RIM=50.35 INV.IN=46.67 INV.S IN=46.17 (CB'S) INV.OUT=46.17

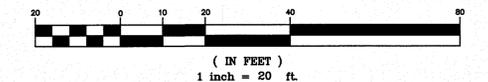
FD = FIRST DEFENSE UNIT
 * = LOW PROFILE FRAME, GRATE & TOP SLAB

NOTES:

- ALL SITE DRAINAGE PIPE SHALL BE CORRUGATED HIGH-DENSITY POLYETHYLENE PIPE WITH STANDARD JOINTS, DUAL-WALL, SMOOTH INTERIOR, AS MANUFACTURED BY ADS, INC., OR APPROVED EQUAL, UNLESS OTHERWISE NOTED ON PLAN.
- ALL ROOF AND CANOPY DRAIN PIPE SHALL BE 6" PVC (SDR-35).
- ELEVATIONS ARE BASED ON NAVD 1988 DATUM.
- ALL PROPOSED ELEVATIONS AS SHOWN ARE BOTTOM OF CURB ELEVATIONS, UNLESS OTHERWISE NOTED.
- ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER OF ANY DISCREPANCIES. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND INSTALLATIONS SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (811) PRIOR TO COMMENCING ANY EXCAVATION.
- ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO MDOT AND THE TOWN OF BRUNSWICK, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- ALL ADA ACCESSIBLE WALKWAYS CANNOT EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE. RAMPS CANNOT EXCEED 8.33% RUNNING SLOPE AND 2% CROSS SLOPE, AND HC PARKING STALLS AND ACCESS AISLES CANNOT EXCEED 2% SLOPE IN ANY DIRECTION. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.
- SEE UTILITY PLAN FOR DETAILED UTILITY LAYOUT.
- CONTRACTOR IS TO FIELD ADJUST GRADES FOR THE TANK PAD TO SHED WATER.
- CONTRACTOR TO INCORPORATE RECOMMENDATIONS MADE IN THE GEOTECHNICAL ENGINEERING EXPLORATION EVALUATION AS PREPARED BY ENVIRONMENTAL COMPLIANCE SERVICES, INC. (ECS) DATED JULY 16, 2015.

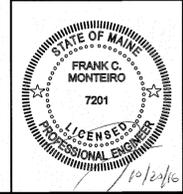


GRAPHIC SCALE



LEGEND

- | | |
|-----------------------------------|--------------------------------------|
| ○ IRON PIN FOUND | ○ TREETLINE |
| □ CONCRETE BOUND FOUND | ○ UTILITY POLE |
| △ RAILROAD SPIKE FOUND | ○ DRAIN MANHOLE |
| ○ DRILL HOLE FOUND | ○ SEWER MANHOLE |
| — VERTICAL GRANITE CURB | ○ TELEPHONE MANHOLE |
| — SLOPED GRANITE CURB | □ CATCH BASIN |
| — BITUMINOUS CONCRETE LIP CURBING | — WATER LINE |
| — BITUMINOUS CONCRETE BERM | — WATER VALVE |
| — OVERHEAD SERVICE WIRES | — FIRE HYDRANT |
| — DOUBLE SOLID YELLOW LINE | — GAS VALVE |
| — SINGLE SOLID WHITE LINE | — GAS LINE |
| — BROKEN WHITE LINE | — UNDERGROUND TELEPHONE LINE |
| — SIGN | — UNDERGROUND ELECTRIC AND TELEPHONE |
| — OBSERVATION WELL | — WETLAND LINE |
| — TEST PIT | — SPOT ELEVATION |
| — TEST BORING | — CONTOUR ELEVATION |



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2	10/20/16	CMT	REV'S PER TOWN COMMENTS

48,722 FEET
 1.119 ACRES
 V# L0589
 Store# 5585
 Gas Station# 1818

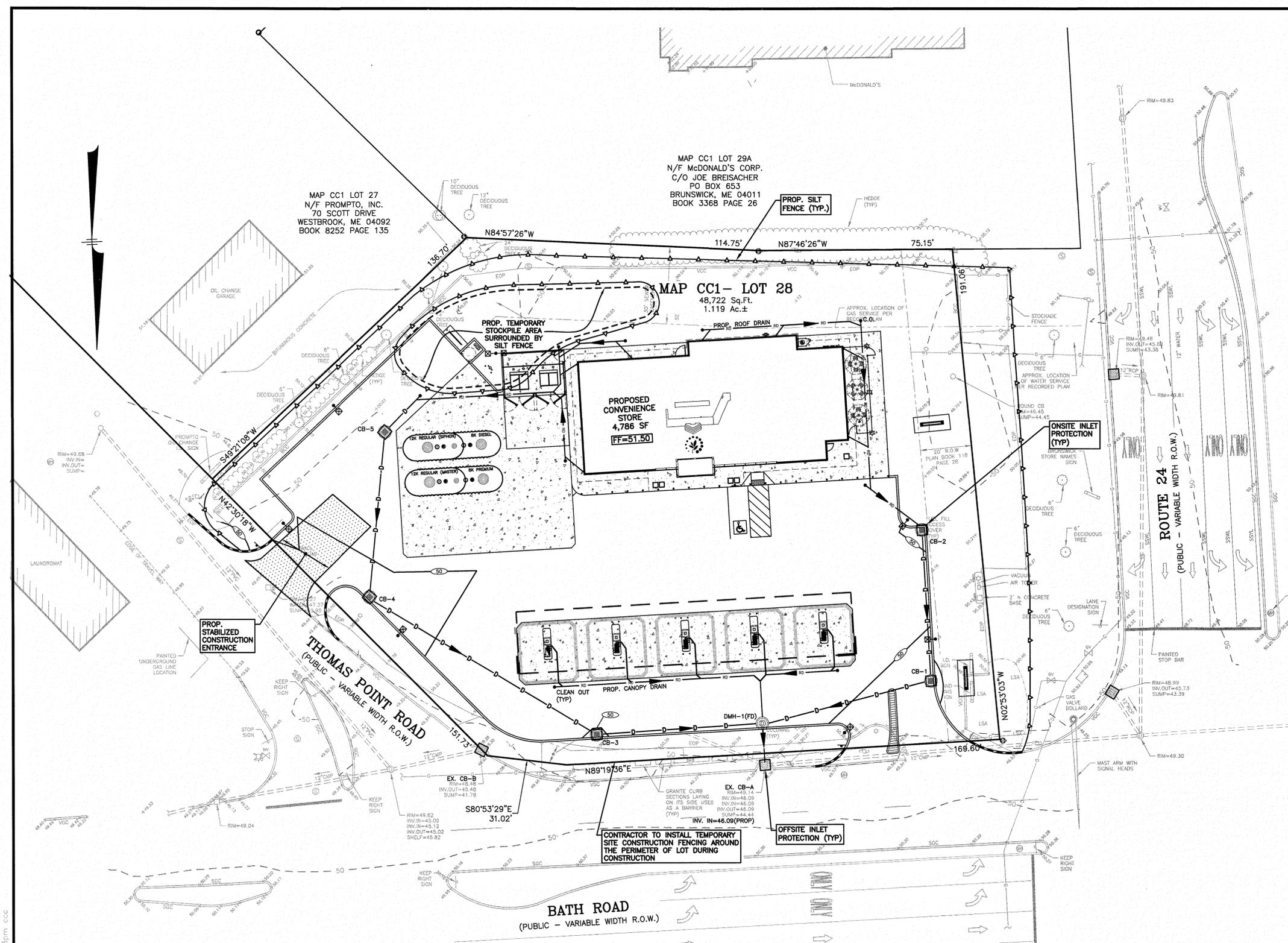
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GRADING & DRAINAGE PLAN
 CFG05.0

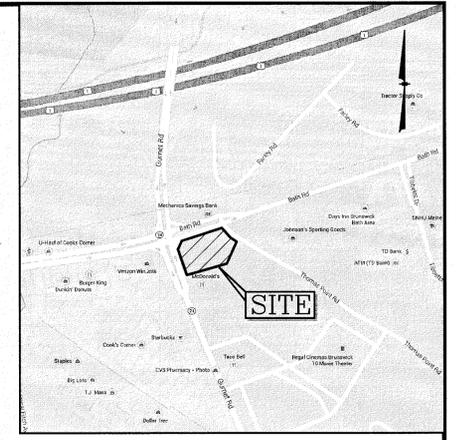
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CONSTRUCTION SEQUENCE:

- CUT AND STUMP AREAS OF PROPOSED CONSTRUCTION.
- INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS REQUIRED. CONSTRUCT CONSTRUCTION ENTRANCE AS SHOWN ON PLAN.
- REMOVE AND STOCKPILE TOPSOIL. STOCKPILE SHALL BE SEED TO PREVENT EROSION. THE DOWN GRADIENT PORTION OF THE STOCKPILE SHALL BE ENCLOSED WITH EITHER SILT FENCE OR A STRAW OR HAYBALE BARRIER INSTALLED ACCORDING TO DETAILS SHOWN ON THIS PLAN.
- CONSTRUCT DETENTION BASIN AND CLOSED DRAINAGE SYSTEM. PROTECT CULVERT INLETS AND CATCH BASINS WITH SEDIMENTATION BARRIERS.
- PERFORM SITE GRADING, PLACING HAY BALES AND SILTATION FENCES AS REQUIRED TO CONTROL SOIL EROSION.
- INSTALL UNDERGROUND UTILITIES.
- BEGIN TEMPORARY AND PERMANENT SEEDING AND MULCHING. ALL CUT AND FILL SLOPES SHALL BE SEED OR MULCHED IMMEDIATELY AFTER THEIR CONSTRUCTION.
- DAILY OR AS REQUIRED, CONSTRUCT, INSPECT, AND IF NECESSARY, RECONSTRUCT TEMPORARY BERMS, DITCHES, SILT FENCES AND SEDIMENT TRAPS INCLUDING MULCHING AND SEEDING. SEE "LANDSCAPE PLAN FOR MULCHING SPECIFICATIONS.
- BEGIN EXCAVATION FOR AND CONSTRUCTION OF BUILDINGS.
- FINISH PAVING ALL DRIVES AND PARKING AREAS.
- COMPLETE PERMANENT SEEDING AND LANDSCAPING.
- NO FLOW SHALL BE DIVERTED TO ANY WETLANDS UNTIL A HEALTHY STAND OF GRASS HAS BEEN ESTABLISHED IN UPGRADED AREAS.
- AFTER GRASS HAS BEEN FULLY GERMINATED IN ALL SEEDED AREAS, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.

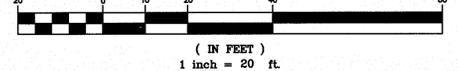


LOCATION MAP
(NOT TO SCALE)

EROSION CONTROL MEASURES & MAINTENANCE

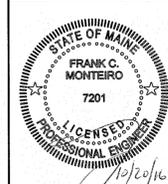
- DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED:
- INSTALLATION OF STONE CHECK DAMS, SILTATION FENCES AND ORANGE CONSTRUCTION FENCE SHALL BE COMPLETED PRIOR TO THE START OF THE SITE WORK IN ANY GIVEN AREA. PREFABRICATED SILTATION FENCES SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
- STONE CHECK DAMS, SILTATION FENCES, AND ORANGE CONSTRUCTION FENCE SHALL BE KEPT CLEAN DURING CONSTRUCTION AND REMOVED WHEN ALL SLOPES HAVE A HEALTHY STAND OF VEGETATIVE COVER. EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EVERY 0.5" OF RAINFALL.
- EXISTING VEGETATION IS TO REMAIN UNDISTURBED WHEREVER POSSIBLE.
- THE AREA OF LAND EXPOSED AND THE TIME OF EXPOSURE SHALL BE MINIMIZED. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 72 HOURS AFTER FINAL GRADING. WITH EITHER PERMANENT OR TEMPORARY MEASURES OR TEMPORARY MEASURES. IN NO CASE SHALL ANY DISTURBED AREAS BE LEFT UNSTABILIZED WITH EITHER PERMANENT OR TEMPORARY EROSION CONTROL MEASURES FOR MORE THAN 72 HOURS, OR AS NECESSARY TO ENSURE SUFFICIENT STABILIZATION DUE TO WEATHER OR OTHER CONDITIONS. SIMULTANEOUS WORK IN MULTIPLE AREAS MAY BE PERMITTED AS NEEDED, SUBJECT TO THE ABOVE, HOWEVER THE CONTRACTOR SHALL NOT DISTURB AREAS THAT CANNOT REASONABLY BE PROPERLY STABILIZED AND MAINTAINED WITHIN 72 HOURS.
- ALL DISTURBED AREAS SHALL HAVE A MINIMUM OF 6" OF LOAM INSTALLED WITH MIX. SEED MIXTURE SHALL BE:
 - A. SLOPE AND DETENTION MIX - 1.8LBS/1000SF (TYPE 2):
 - PERENNIAL SWEET PEA - 20%
 - WILD LUPINE - 50%
 - SNOWY TICK TREFOL - 15%
 - WHITE CLOVER - 15%
 - B. MIX LAWN: MIX - 3LBS/1000SF (TYPE 1):
 - PERENNIAL RYEGRASS - 10%
 - ANNUAL RYEGRASS - 5%
 - KENTUCKY BLUEGRASS - 25%
 - CREeping RED FESCUE - 45%
 - CHEWINGS RYEGRASS - 15%
- LIME AND FERTILIZER SHALL BE INCORPORATED INTO THE SOIL PRIOR TO OR AT THE TIME OF SEEDING. A MINIMUM OF 2 TONS PER ACRE OF AGRICULTURAL LIMESTONE AND 300 LBS. PER ACRE OF 10-10-10 FERTILIZER SHALL BE APPLIED. SEEDING PRACTICES SHALL COMPLY WITH LOCAL USDA SOIL CONSERVATION SERVICES RECOMMENDATIONS.
- HAY MULCH AND SLOPE PROTECTION BLANKET SHALL BE USED WHERE INDICATED ON THE PLANS. A MINIMUM OF 1.5 TONS OF MULCH PER ACRE SHALL BE APPLIED. MULCH SHALL BE ANCHORED IN PLACE WHERE NECESSARY. SLOPE PROTECTION BLANKET SHALL BE LAID IN THE DIRECTION OF RUNOFF FLOW AND APPLIED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS AND/OR DETAILS.
- PERMANENT OR TEMPORARY COVER MUST BE IN PLACE BEFORE THE GROWING SEASON ENDS. WHEN SEEDING AREAS ARE MULCHED, PLANTINGS MAY BE MADE FROM EARLY SPRING TO EARLY OCTOBER. WHEN SEEDING AREAS ARE NOT MULCHED, PLANTINGS SHOULD BE MADE FROM APRIL 15 TO MAY 20 OR FROM AUGUST 15 TO SEPTEMBER 15. NO DISTURBED AREA SHALL BE LEFT EXPOSED DURING WINTER MONTHS.
- PRIOR TO CONSTRUCTION OF IMPERVIOUS AREAS, ALL DRAINAGE STRUCTURES AND PIPES SHALL BE INSTALLED AND INSPECTED FOR PROPER FUNCTION. DURING CONSTRUCTION OF OTHER SITE FEATURES, ALL DRAINAGE FACILITIES SHALL BE INSPECTED ON A DAILY BASIS AND CLEANED/REPAIRED IMMEDIATELY UPON DISCOVERY OF SEDIMENT BUILD-UP OR DAMAGE.
- EROSION CONTROLS ARE TO BE INSPECTED AND MAINTAINED ON A DAILY BASIS. DISCOVERY OF SILT BUILD-UP IN CATCH BASIN SUMPS OR ANY OTHER STRUCTURE SHALL REQUIRE CLEANING.
- ALL EXPOSED SOILS SHALL BE IMMEDIATELY STABILIZED WITH A LAYER OF HAY.
- UPON INSTALLATION OF CATCH BASINS, INLET PROTECTION - AS DESCRIBED ON AFOREMENTIONED PLAN - SHALL BE INSTALLED AND MAINTAINED UNTIL READY FOR PAVING.
- CALCIUM CHLORIDE AND/OR WATER SHALL BE USED FOR DUST CONTROL IN APPROPRIATE AREAS.
- IF, DURING CONSTRUCTION, IT COMES APPARENT THAT ADDITIONAL EROSION CONTROL MEASURES ARE REQUIRED TO STOP ANY EROSION ON THE CONSTRUCTION SITE DUE TO ACTUAL SITE CONDITIONS, THE CONTRACTOR SHALL BE REQUIRED TO INSTALL THE NECESSARY EROSION PROTECTION AT NO EXPENSE TO THE CITY OR OWNER.
- OVER WINTER STABILIZATION (IF NEEDED):
 - A. PERMANENT STABILIZATION CONSISTS OF AT LEAST 85% VEGETATION, PAVEMENT/GRAVEL BASE OR RIPRAP
 - B. DO NOT EXPOSE SLOPES OR LEAVE SLOPES EXPOSED OVER THE WINTER OR FOR ANY OTHER EXTENDED TIME OF WORK SUSPENSION UNLESS FULLY PROTECTED WITH MULCH.
 - C. APPLY HAY MULCH AT TWICE THE STANDARD RATE (150 LBS. PER 1,000 SF). THE MULCH MUST BE THICK ENOUGH SUCH THAT THE GROUND SURFACE WILL NOT BE VISIBLE AND MUST BE ANCHORED.
 - D. USE MULCH AND MULCH NETTING OR AN EROSION CONTROL BLANKET OR MIX FOR ALL SLOPES GREATER THAN 3% OR OTHER AREAS EXPOSED TO DIRECT WIND.
 - E. INSTALL EROSION CONTROL BLANKETS IN ALL DRAINAGE WAYS (BOTTOM AND SIDES) WITH A SLOPE GREATER THAN 3%.
 - F. SEE THE VEGETATION MEASURES FOR MORE INFORMATION ON SEEDING DATES AND TYPES.
- WINTER CONSTRUCTION (IF NEEDED):
 - A. WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED AS SUCH NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME.
 - B. AN AREA WITHIN 100 FEET OF A PROTECTED NATURAL RESOURCE MUST BE PROTECTED WITH A DOUBLE ROW OF SEDIMENT BARRIER.
 - C. TEMPORARY MULCH MUST BE APPLIED WITHIN 7 DAYS OF SOIL EXPOSE OR PRIOR TO ANY STORM EVENT, BUT AFTER EVERY WORKDAY IN AREAS WITHIN 100 FEET FROM A PROTECTED NATURAL RESOURCE.
 - D. AREAS THAT HAVE BEEN BROUGHT TO FINAL GRADE MUST BE PERMANENTLY MULCHED THE SAME DAY.
 - E. IN THE EVENT OF A SNOWFALL GREATER THAN 1 INCH (FRESH OR CUMULATIVE), THE SNOW SHALL BE REMOVED FROM AREAS DUE TO BE SEEDED OR STABILIZED.
 - F. LOAM SHALL BE FREE OF FROZEN CLUMPS BEFORE IT IS APPLIED.
 - G. A DITCH THAT WILL BE CONSTRUCTED DURING THE WINTER MUST BE STABILIZED WITH RIPRAP.
 - H. ALL DITCHES OR SWALES WHICH DO NOT SNOW A MIN. OF 60% VEGETATION BY OCT. 15 OR WHICH ARE DISTURBED AFTER OCT. 15 SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOWS.
 - I. AFTER NOV. 15 INCOMPLETE ROAD OR PARKING SURFACES WHERE WORK HAS STOPPED FOR THE WINTER SEASON SHALL BE PROTECTED WITH A MINIMUM OF 3" OF CRUSHED GRAVEL PER FOOT.

GRAPHIC SCALE



LEGEND

○	IRON PIN FOUND	—	TREELINE
□	CONCRETE BOUND FOUND	—	UTILITY POLE
△	RAILROAD SPIKE FOUND	—	DRAIN MANHOLE
○	DRILL HOLE FOUND	—	SEWER MANHOLE
—	VERTICAL GRANITE CURB	—	TELEPHONE MANHOLE
—	SLOPED GRANITE CURB	—	CATCH BASIN
—	BITUMINOUS CONCRETE LIP CURBING	—	WATER LINE
—	BITUMINOUS CONCRETE BERM	—	WATER VALVE
—	OVERHEAD SERVICE WIRES	—	FIRE HYDRANT
—	DOUBLE SOLID YELLOW LINE	—	GAS VALVE
—	SINGLE SOLID WHITE LINE	—	GAS LINE
—	BROKEN WHITE LINE	—	UNDERGROUND TELEPHONE LINE
—	SIGN	—	UNDERGROUND ELECTRIC AND TELEPHONE
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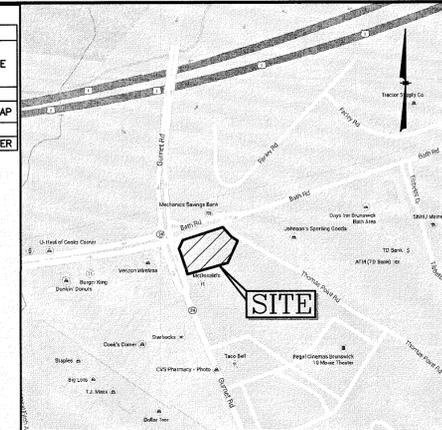
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DATE: JULY 11, 2016
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CHECKED BY: FCM

EROSION & SEDIMENTATION CONTROL PLAN CFG06.0

FROM: STRUCTURE NUMBER	PIPE SIZE (inches)	TYPE OF PIPE	APPROX. PIPE LENGTH (feet)	SLOPE OF PIPE (ft./ft.)	TO: STRUCTURE NUMBER
BLDG. SEWER	4	SDR 35 PVC	42	0.020	WYE #1
BLDG. GR. TRAP	4	SDR 35 PVC	15	0.020	GREASE TRAP
GR. TRAP	4	SDR 35 PVC	8	0.020	WYE #1
WYE #1	6	SDR 35 PVC	41	0.1540	EXIST. SEWER

SEWER STRUCTURES

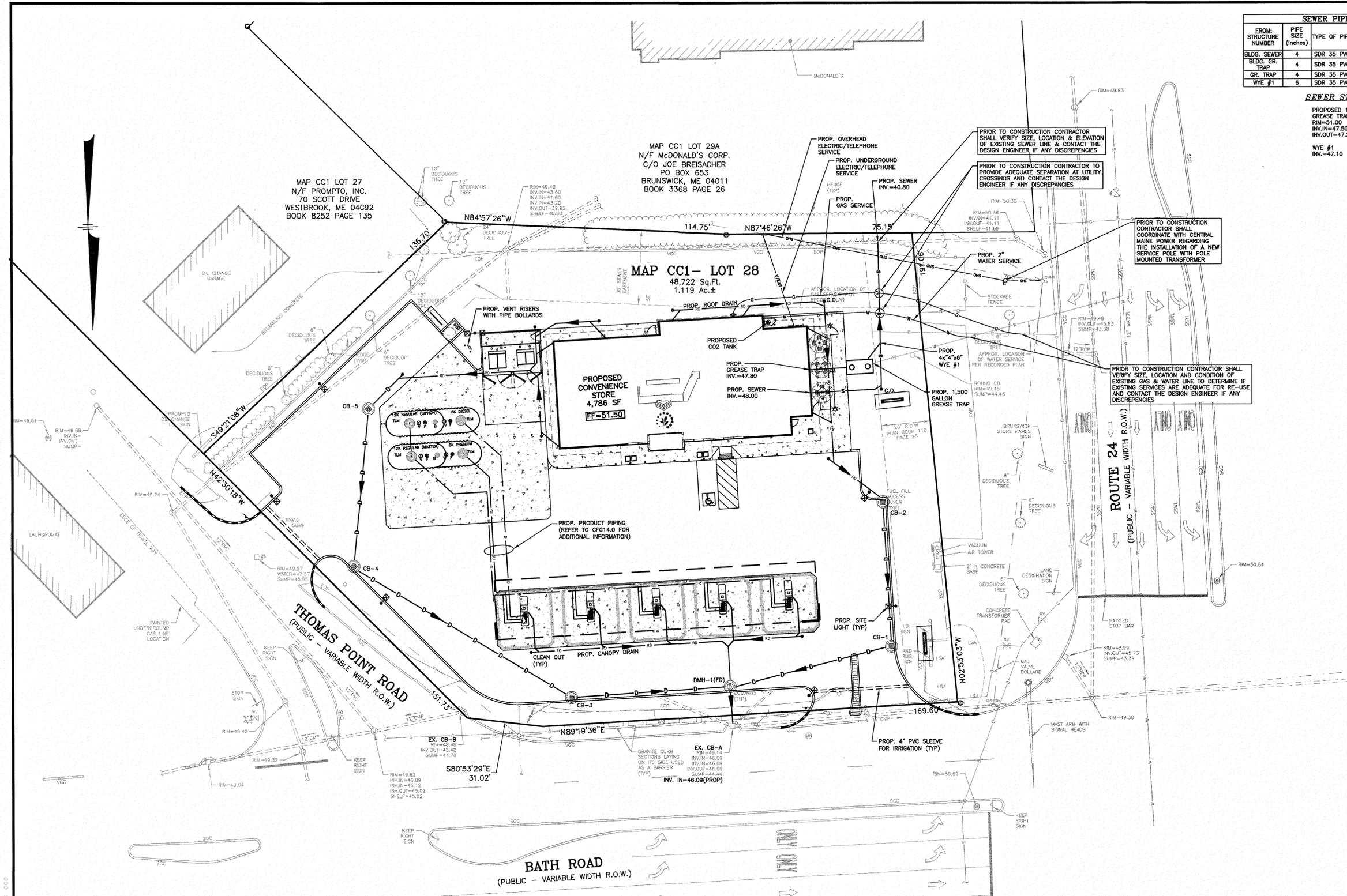
PROPOSED 1,500 GALLON GREASE TRAP
 RIM=51.00
 INV.IN=47.50
 INV.OUT=47.25
 WYE #1
 INV.=47.10



LOCATION MAP
(NOT TO SCALE)

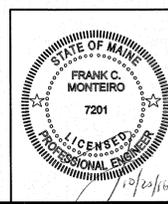
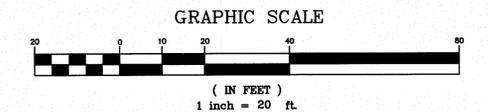
NOTES:

- 1) ALL SANITARY SEWER PIPE SHALL BE PVC (SDR-35), UNLESS OTHERWISE NOTED.
- 2) ALL WATER PIPE SHALL BE COPPER (TYPE K), UNLESS OTHERWISE NOTED.
- 3) ELEVATIONS ARE BASED ON NAVD 1988 DATUM.
- 4) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- 5) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER IF ANY DISCREPANCIES.
- 6) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- 7) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
- 8) ALL WATER, SEWER AND DRAINAGE CONSTRUCTION SHALL CONFORM TO THE TOWN OF BRUNSWICK STANDARDS AND SPECIFICATIONS.
- 9) ALL ELECTRIC, TELEPHONE AND CABLE TV LINES ARE TO BE UNDERGROUND AND INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY CO. SPECIFICATIONS.
- 10) ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY UTILITY COMPANY AND LOCAL DPW.
- 11) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO MAINE DOT AND BRUNSWICK DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- 12) SEE GRADING & DRAINAGE PLAN FOR DETAILED DRAINAGE INFORMATION.
- 13) ELECTRICAL CONDUIT WITHIN 20' OF TANKS OR DISPENSERS MAY NEED TO BE RIGID METAL CONDUIT WITH CONCRETE ENCASEMENT. CONTRACTOR TO COORDINATE WITH UTILITY COMPANY AND/OR TOWN ELECTRICAL INSPECTOR AS REQUIRED.



LEGEND

○ IRON PIN FOUND	○ TREELINE
□ CONCRETE BOUND FOUND	○ UTILITY POLE
△ RAILROAD SPIKE FOUND	○ DRAIN MANHOLE
○ DRILL HOLE FOUND	○ SEWER MANHOLE
— VERTICAL GRANITE CURB	○ TELEPHONE MANHOLE
— SLOPED GRANITE CURB	□ CATCH BASIN
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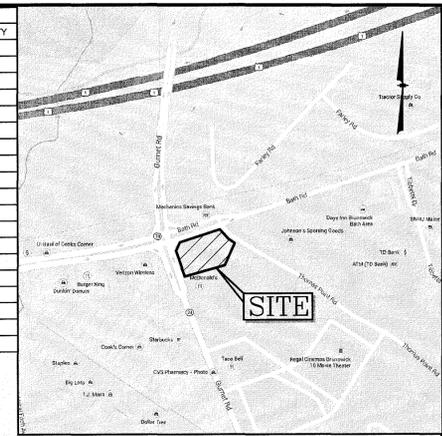
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UTILITY PLAN
 CFG07.0

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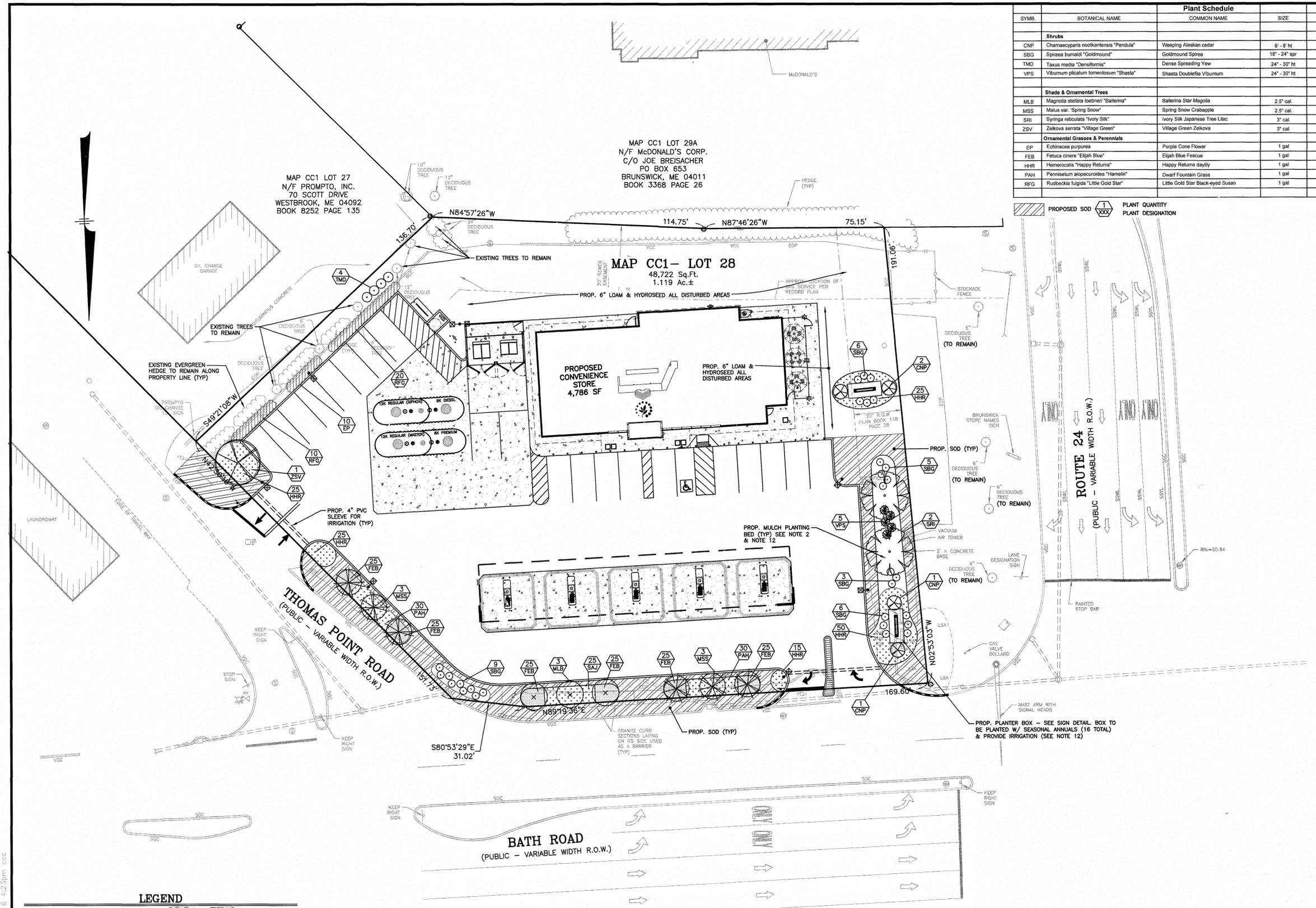
Plant Schedule					
SYMB.	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS	QUANTITY
Shrubs					
CNP	Chamaecyparis nootkanensis "Pendula"	Weeping Alaskan cedar	6' - 8' ht	B & B	4
SBG	Spiraea bumalda "Goldmound"	Goldmound Spiraea	18" - 24" spr	3 gal	29
TMD	Taxus media "Densiflora"	Dense Spreading Yew	24" - 30" ht	B & B or 5 gal.	4
VPS	Viburnum plicatum tomentosum "Shasta"	Shasta Doublefile Viburnum	24" - 30" ht	B & B or 5 gal.	5
Shade & Ornamental Trees					
MLB	Magnolia stellata (lobster) "Ballerina"	Ballerina Star Magnolia	2.5' cal.	B & B	3
MSS	Malus var. "Spring Snow"	Spring Snow Crabapple	2.5' cal.	B & B fruitless	6
SRI	Syringa reticulata "Ivory Silk"	Ivory Silk Japanese Tree Lilac	3' cal.	B & B	2
ZSV	Zelkova serrata "Village Green"	Village Green Zelkova	3' cal.	B & B	1
Ornamental Grasses & Perennials					
EP	Echinacea purpurea	Purple Cone Flower	1 gal	24" o.c.	10
FEB	Fetuca cinerea "Elijah Blue"	Elijah Blue Fescue	1 gal	12" o.c.	150
HHR	Hemerocallis "Happy Returns"	Happy Returns daylily	1 gal	18" o.c.	140
PAH	Pennisetum alopecuroides "Harelin"	Dwarf Fountain Grass	1 gal	18" o.c.	100
RFG	Rudbeckia fulgida "Little Gold Star"	Little Gold Star Black-eyed Susan	1 gal	23" o.c.	30



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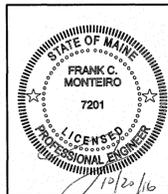
NOTES:

- ALL PLANT STOCK SHALL CONFORM TO ANSI Z260.1 - NURSERY STOCK, LATEST EDITION (AMERICAN ASSOCIATION OF NURSERYMEN, INC.).
- 4" AGED PINEBARK MULCH AND A WEED BARRIER (TY-PAR FABRIC OR APPROVED EQUAL) SHALL BE APPLIED TO ALL SHRUB AND GROUNDCOVER BEDS. INSTALL WEED BARRIER AS PER MANUFACTURERS RECOMMENDATIONS.
- PLANT PIT BACKFILL SHALL BE MIXED AT A RATE OF 7 PARTS OF TOPSOIL TO 2 PART OF DEHYDRATED COW MANURE. SLOW RELEASE FERTILIZER SHALL BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS. USE EXISTING ON-SITE TOPSOIL AS PART OF BACKFILL WHEN AVAILABLE.
- ALL LANDSCAPED AREAS NOT PLANTED WITH TREES, SHRUBS OR GROUNDCOVER SHALL BE RESTORED WITH SEED OR SOD AS INDICATED ON PLANS.
- ALL SOD, SEED, SHRUB AND TREE AREAS SHALL RECEIVE 6" PH CORRECTED TOPSOIL. AFTER TOPSOIL IS SPREAD EVENLY OVER ENTIRE AREA, ALL CLODS, LUMPS, STONES AND OTHER DELETERIOUS MATERIAL SHALL BE RAKED UP AND REMOVED.
- APPLICATION OF GRASS SEED, FERTILIZERS AND MULCH SHALL BE ACCOMPLISHED BY BROADCAST SEEDING OR HYDROSEEDING AT THE RATES OUTLINED BELOW:
 LIMESTONE: 100 LBS./1,000 SQUARE FEET.
 FERTILIZER: 500 LBS./ACRE OF 10-20-20 OR 1000 LBS./ACRE OF 5-10-10.
 MULCH: HAY MULCH APPROXIMATELY 3 TONS/ACRE
 SEED MIX (SLOPES LESS THAN 4:1) LBS./ACRE
 CREEPING RED FESCUE 20
 TALL FESCUE 15
 PERENNIAL RYEGRASS 5
 TOTAL 40
 SLOPE MIX (SLOPES GREATER THAN 4:1) LBS./ACRE
 CREEPING RED FESCUE 20
 TALL FESCUE 20
 BIRDSFOOT TREEFOIL 8
 TOTAL 48
- FOR TEMPORARY EROSION CONTROL NOTES, SEE EROSION & SEDIMENT CONTROL PLAN.
- NEWLY GRADED AREAS REQUIRING SLOPE PROTECTION OUTSIDE OF NORMAL SEEDING SEASON SHALL RECEIVE STRAW MULCH AT THE APPROXIMATE RATE OF NO MORE THAN 3 TONS PER ACRE.
- ANY CHANGES IN PLANT LOCATIONS OR TYPES SHALL BE APPROVED BY THE DEVELOPER AND TOWN PRIOR TO INSTALLATION.
- PLANTINGS SHALL BE GUARANTEED BY THE CONTRACTOR FOR ONE YEAR AFTER WRITTEN ACCEPTANCE OF THE DEVELOPER.
- EXPOSED SOILS SHALL BE SEEDED OR HAY MULCHED WITHIN 72 HOURS OF FINAL GRADING.
- THE CONTRACTOR SHALL INSTALL AN IRRIGATION SYSTEM TO PROVIDE COMPLETE COVERAGE OF ALL SEED, SOD AREAS AND SHRUB BEDS. THE SYSTEM SHALL INCLUDE A TIMER WITH RAIN SENSOR AND SHALL BE INSTALLED IN ACCORDANCE WITH LOCAL CODES. IRRIGATION SYSTEM SHALL BE ROUTED TO Pylon SIGN PLANTER BED AND PLANTER BED ALONG BUILDING.



LEGEND

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□ CONCRETE BOUND FOUND	— UTILITY POLE
△ RAILROAD SPIKE FOUND	⊙ DRAIN MANHOLE
⊙ DRILL HOLE FOUND	⊙ SEWER MANHOLE
— VERTICAL GRANITE CURB	⊙ TELEPHONE MANHOLE
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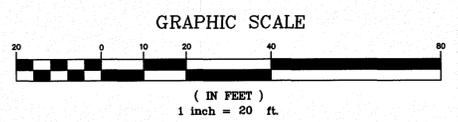
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FRAMINGHAM, MA 01702

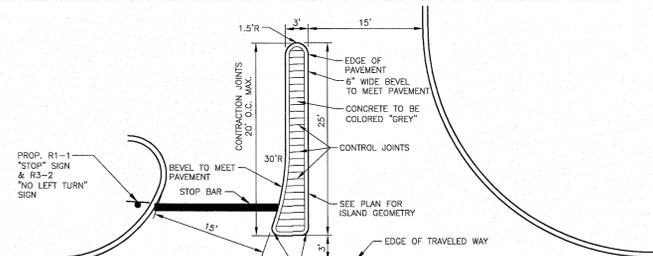
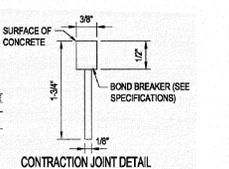
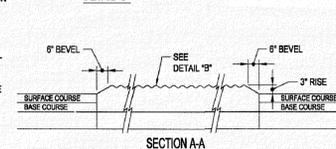
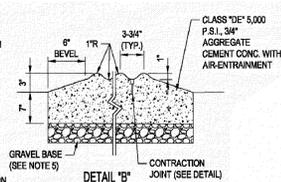
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DATE: JULY 11, 2016
FILE: 3751SP.dwg
DRAWN BY: CMT
CHECKED BY: FCM

LANDSCAPE PLAN CFG08.0

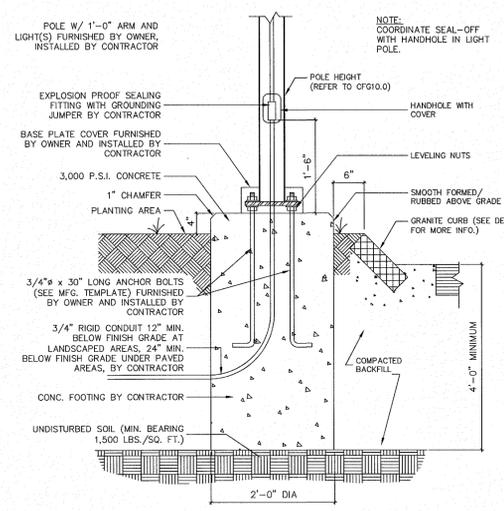


NOTES:

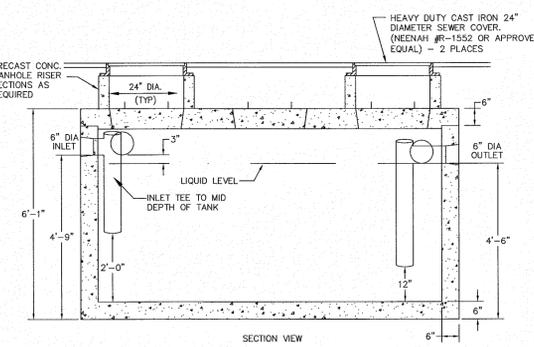
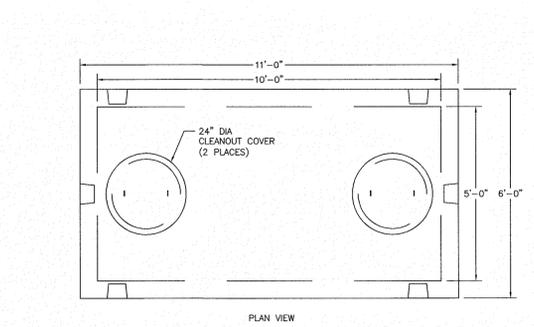
- CONTRACTION JOINTS ARE TO BE SPACED AT A MAXIMUM OF 20 FEET APART.
- THE JOINTS ARE TO BE SAWN AND LOCATED IN THE DEPRESSIONS OF THE CORRUGATIONS (SEE DETAIL OF CORRUGATIONS).
- END OF CORRUGATED RIDGES TO BE BEVELED.
- FOR DESCRIPTION OF MATERIAL AND CONSTRUCTION METHODS SEE CONDUIT STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
- THE DEPTH OF THE GRAVEL IS TO BE SUCH THAT ITS BOTTOM LINE MEETS THE BOTTOM OF THE GRAVEL LINE OF THE CONTIGUOUS PAVEMENT.



BATH ROAD
DETAIL "A"
RAISED/MOUNTABLE CONCRETE ISLAND
NOT TO SCALE

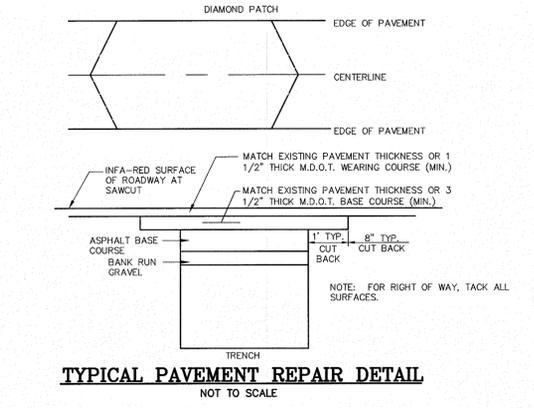


LIGHT POLE DETAIL
NOT TO SCALE

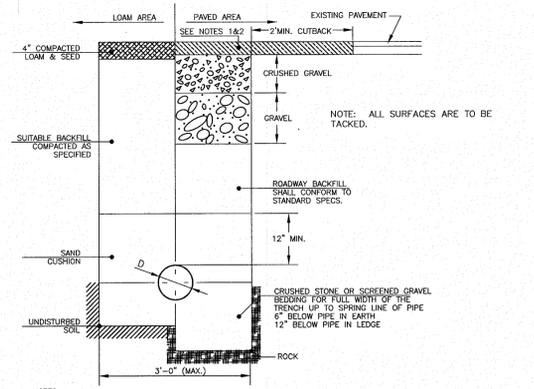


- NOTES:
- CONCRETE, 5,000 PSI MINIMUM AFTER 28 DAYS.
 - FINAL DESIGN OF GREASE TRAP TO MEET ALL LOCAL AND STATE REQUIREMENTS.
 - ALL REINFORCEMENT PER ASTM C1227-93.
 - DESIGNED FOR H-20 LOADING.
 - TONGUE & GROOVE JOINT SEALED WITH BUTYL RESIN.
 - TEES AND BAFFLES SOLD SEPARATELY.
 - GREASE TRAP AS MANUFACTURED BY SHEA CONCRETE PRODUCTS OR APPROVED EQUAL.
 - CONTRACTOR TO PROVIDE DESIGN ENGINEER SHOP DRAWINGS PRIOR TO CONSTRUCTION.

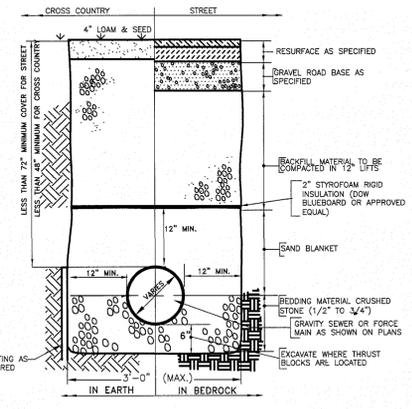
1,500 GALLON GREASE TRAP DETAIL
NOT TO SCALE



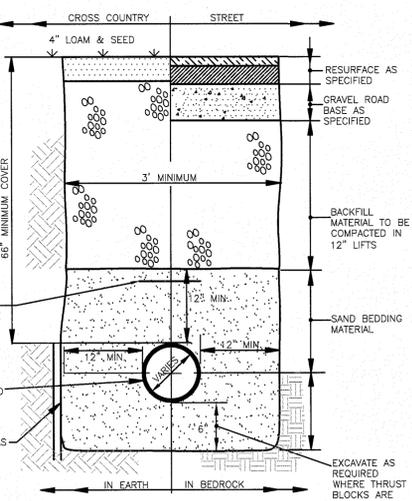
TYPICAL PAVEMENT REPAIR DETAIL
NOT TO SCALE



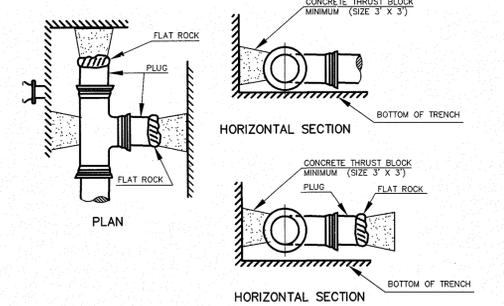
TYPICAL SEWER TRENCH DETAIL
NOT TO SCALE



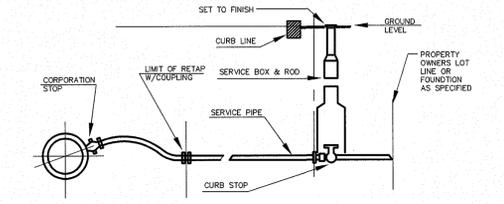
TYPICAL TRENCH SECTION FOR SANITARY SEWER SERVICE WITH LESS THAN 6' OF COVER
NOT TO SCALE



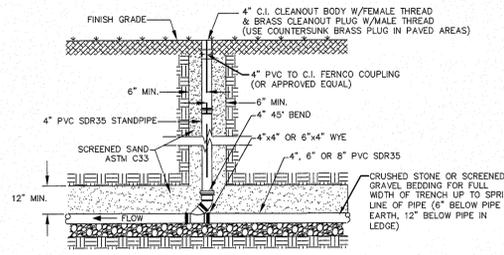
TYPICAL TRENCH SECTION FOR WATER SERVICE
NOT TO SCALE



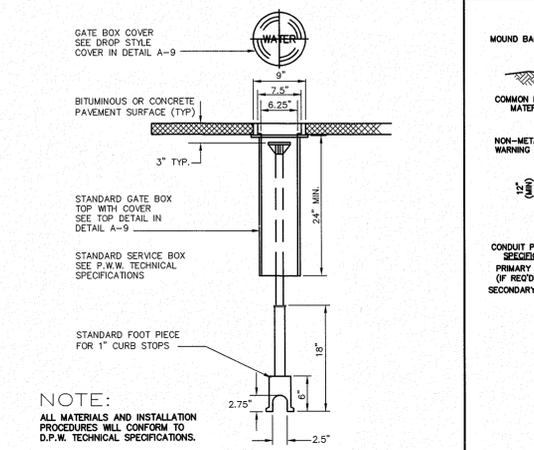
THRUST BLOCK DETAILS
NOT TO SCALE



TYPICAL SERVICE CONNECTION
NOT TO SCALE

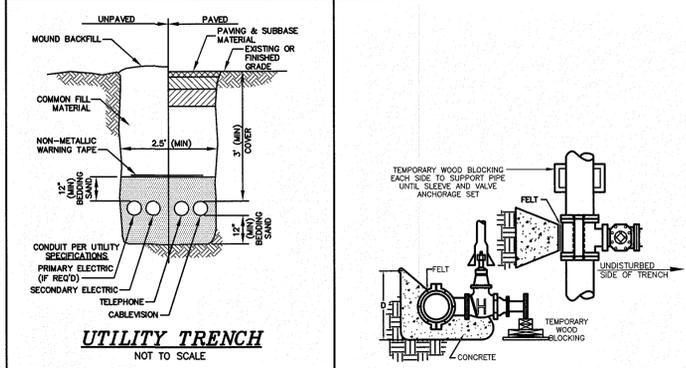


TYPICAL SEWER SERVICE CLEANOUT
NOT TO SCALE

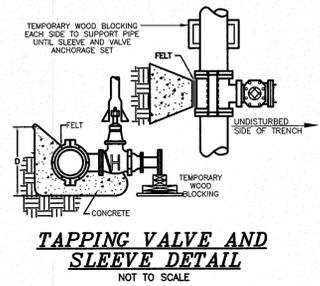


- NOTE:
- ALL MATERIALS AND INSTALLATION PROCEDURES WILL CONFORM TO D.P.W. TECHNICAL SPECIFICATIONS.

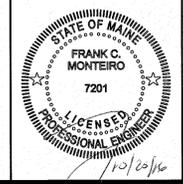
SERVICE BOX INSTALLATION IN PAVEMENT
NOT TO SCALE



UTILITY TRENCH
NOT TO SCALE



TAPPING VALVE AND SLEEVE DETAIL
NOT TO SCALE



REVISIONS			
NO.	DATE	REV. BY	DESCRIPTION
1	8/26/16	CMT	MISC. REVISIONS PER TOWN COMMENTS
2	10/20/16	CMT	REV'S PER TOWN COMMENTS

48,722 FEET
1.119 ACRES
V# L0589
Store# 5585
Gas Station# 1818

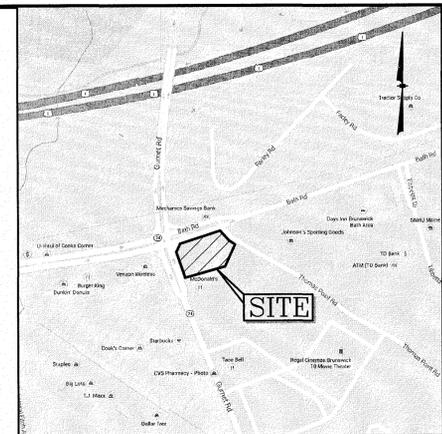
MHF Design Consultants, Inc.
44 Stiles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

Cumberland Farms
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

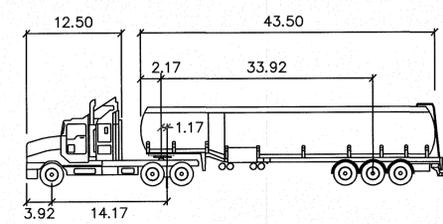
190 BATH ROAD
BRUNSWICK, MAINE 04011

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DATE: JULY 11, 2016
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DRAWN BY: CMT
CHECKED BY: FCM
CFG09.1

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LOCATION MAP
(NOT TO SCALE)

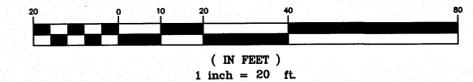


Fuel Tanker D (IN FEET)

Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 40.0
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		

CUMBERLAND FARMS FUEL TANKER

GRAPHIC SCALE



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MHF Design Consultants, Inc.
190 BATH ROAD
BRUNSWICK, MAINE 04011

CUMBERLAND FARMS
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

TRUCK TURN PLAN
CFG16.0

MAP CC1 LOT 29A
N/F McDONALD'S CORP.
C/O JOE BREISACHER
PO BOX 653
BRUNSWICK, ME 04011
BOOK 3368 PAGE 26

MAP CC1 LOT 27
N/F PROMPTO, INC.
70 SCOTT DRIVE
WESTBROOK, ME 04092
BOOK 8252 PAGE 135

MAP CC1- LOT 28
48,722 Sq.Ft.
1.119 Ac.±

PROPOSED CONVENIENCE STORE
4,786 SF

ROUTE 24
(PUBLIC - VARIABLE WIDTH R.O.W.)

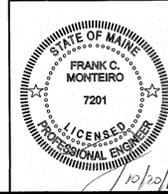
BATH ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

THOMAS POINT ROAD
(PUBLIC - VARIABLE WIDTH R.O.W.)

MAP CC2 LOT 53
N/F STATE OF MAINE C/O
DEPT OF TRANSPORTATION
16 STATE STATION HOUSE
AUGUSTA, ME 04333
BOOK PAGE

LEGEND

- | | |
|-----------------------------------|--------------------------------------|
| ○ IRON PIN FOUND | ○ TREETLINE |
| □ CONCRETE BOUND FOUND | ○ UTILITY POLE |
| △ RAILROAD SPIKE FOUND | ○ DRAIN MANHOLE |
| ○ DRILL HOLE FOUND | ○ SEWER MANHOLE |
| — VERTICAL GRANITE CURB | ○ TELEPHONE MANHOLE |
| — SLOPED GRANITE CURB | □ CATCH BASIN |
| — BITUMINOUS CONCRETE LIP CURBING | — WATER LINE |
| — BITUMINOUS CONCRETE BERM | — WATER VALVE |
| — OVERHEAD SERVICE WIRES | — FIRE HYDRANT |
| — DOUBLE SOLID YELLOW LINE | — GAS VALVE |
| — SINGLE SOLID WHITE LINE | — GAS LINE |
| — BROKEN WHITE LINE | — UNDERGROUND TELEPHONE LINE |
| — SIGN | — UNDERGROUND ELECTRIC AND TELEPHONE |
| — OBSERVATION WELL | |
| — TEST PIT | |
| — TEST BORING | |



REVISIONS			
NO.	DATE	REV. BY.	DESCRIPTION
1	8/26/16	CMT	MISC. REVISIONS PER TOWN COMMENTS
2	10/20/16	CMT	REV'S PER TOWN COMMENTS

48,722 FEET
1.119 ACRES
V# L0589
Store# 5585
Gas Station# 1818

DRAFT FINDINGS OF FACT
Major Development Review
8 Business Parkway
Planning Board Review Date: October 11, 2016

Project Name: 8 Business Parkway (Change of Use)
Address: 8 Business Parkway
Brunswick, ME 04011
Case Number: 16-047 (formerly Case #01-081)
Tax Map: Assessor's Map 17, Lot 68
Zoning: I2 (Church Road Industrial Park)
Overlay Zone: Natural Resource Protection Zone (NRPZ)
Applicant: Allied Composite Center, LLC
2 Main Street
Topsham, ME 04086

Authorized Representative: Sitelines, PA
Kevin Clark, PLS
8 Cumberland Street
Brunswick, ME 04011

Staff reviewed the application and has made a determination of completeness.

PROJECT SUMMARY

Staff review is based on the Major Development Review application prepared by Sitelines, PA as most recently revised on October 13, 2016. The proposed development area is depicted on a plan that is referenced as Sheet 1 of 1 prepared by Sitelines, PA entitled, "Site Plan" dated October 3, 2016.

The proposed project consists of the change of use of an existing building consisting of 28,000 square feet of floor area from Industry, Class 2 to Service Business, Class 2. The proposed change of use requires development review because it affects more than 10,000 square feet of gross floor area. The Brunswick Zoning Ordinance indicates at Section 702.3, if no alterations are proposed for the site related to such Change of Use, the applicant may request that a significant portion of the plan requirements be waived. Further, such an application should be limited to relevant information pertaining to the new use, including but not limited to hours of operation, necessary mechanical devices, screening and parking. The applicant found many of the submittals were not applicable but provided information for the proposed use of the site, and existing conditions based on the required criteria at Chapter 5 of the Brunswick Zoning Ordinance rather than requesting waivers.

Review Standards from Section 411 of the Town of Brunswick Zoning Ordinance

411.1 Ordinance Provisions

The proposed development complies with all applicable provisions and standards of the I2 (Church Road Industrial Park) Zoning District. A portion of the Natural Resource Protection Zone (NRPZ) associated with a stream (ponded stream) exists at the southeast edge of the subject parcel. No activities are proposed within the NRPZ. Further, no exterior changes are proposed at

this time and as built, and based on the available information the existing development conforms with all applicable ordinance provisions. Therefore, the proposed development complies with all applicable provisions and requirements of the Zoning Ordinance. *The Board finds that the provisions of Section 411.1 are satisfied.*

411.2 Preservation of Natural Features

The proposed development avoids all impacts to natural features by utilizing existing site conditions and proposing no additional site development. The proposed development maximizes the preservation of natural features of the landscape, and does not occur within or cause harm to any land which is not suitable for development. *The Board finds that the provisions of Section 411.2 are satisfied.*

411.3 Surface Waters, Wetlands and Marine Resources

Surface waters and marine resources are not proposed to be disturbed within the proposed project area. The proposed development will not adversely affect the water quality of Casco Bay or its estuaries. *The Board finds that the provisions of Section 411.3 are satisfied.*

411.4 Flood Hazard Areas

The FEMA Flood Insurance Rate Map indicates that the project is not located within a FEMA flood hazard area; therefore there is minimal risk of flooding. *The Board finds that the provisions of Section 411.4 are satisfied.*

411.5 Stormwater Management

The proposed development results in no exterior changes, and therefore the recommended storm water quality standards described in Storm Water Management for Maine: Best Management Practices, published by the State of Maine Department of Environmental Protection, November, 1995 as amended are not applicable. *The Board finds that the provisions of Section 411.5 are satisfied.*

411.6 Groundwater

The Board finds that the development will not alone or in conjunction with existing activities adversely affect the quality or quantity of groundwater. *The Board finds that the provisions of Section 411.6 are satisfied.*

411.7 Erosion and Sedimentation Control

The proposed development results in no exterior changes, and therefore Best Management Practices that avoid unreasonable soil erosion or a reduction in the land's capacity to hold water so that a dangerous or unhealthy situation results are not required. *The Board finds that the provisions of Section 411.7 are satisfied.*

411.8 Sewage Disposal

The proposed development is eligible to participate in municipal sewer service subject to an approved sewer permit. *The Board finds that the provisions of Section 411.8 are satisfied.*

411.9 Water Supply

The proposed development has a water source that is adequate to serve the proposed development, and that will have no adverse impact on existing water supplies. *The Board finds that the provisions of Section 411.9 are satisfied.*

411.10 Aesthetic, Cultural and Natural Values

The proposed project results in no exterior site modifications. The proposed development will have no undue adverse effect on the scenic or natural beauty of the area, historic sites, significant wildlife habitats identified by the Maine Department of Environmental Protection or by the Town Of Brunswick, or rare and irreplaceable natural areas or any public rights for physical or visual access to a shoreline. *The Board finds that the provisions of Section 411.10 are satisfied.*

411.11 Community Impact

No new impacts to traffic, the public school system, recreation, public safety, and public works resources have been identified. *The Board finds that the provisions of Section 411.11 are satisfied.*

411.12 Traffic

The Code Enforcement Officer (CEO) calculated the required parking for the proposed use and indicated the parking requirements are considerably more than the amount of parking that is proposed. In response, the applicant agreed to provide a provisional parking analysis that indicates the proposed use intensity supports the proposed parking spaces. The proposed project is not anticipated to cause unreasonable public road congestion or unsafe conditions. *The Board finds that the provisions of Section 411.12 are satisfied provided the CEO is satisfied with the provisional parking analysis provided by the applicant.*

411.13 Pedestrian and Bicycle Access and Safety

The proposal results in no additional use and results in no exterior site modifications. The development contains sufficient accommodations for bicyclists and pedestrians access, safety and circulation both within the site and to points outside of the site. *The Board finds that the provisions of Section 411.13 are not applicable.*

411.14 Development Patterns

The proposed development will be located at an existing developed site within the growth area. The proposed development is respectful of Brunswick's historic development patterns and will have no adverse impact on areas which are primarily residential. *The Board finds that the provisions of Section 411.14 are satisfied.*

411.15 Architectural Compatibility

The proposed commercial building is typical for its use, and provides visual screening of the development site with existing vegetation. The proposed development is compatible with its surroundings. *The Board finds that the provisions of Section 411.15 are satisfied.*

411.16 Municipal Solid Waste Disposal

The proposed reuse of the subject parcel is not expected to result in an increased solid waste impact. Therefore, the Solid Waste Impact Fee is not applicable. *The Board finds that the provisions of Section 411.16 are satisfied.*

411.17 Recreation Needs

A recreation impact fee is not required for the proposed commercial use. *The Board finds that the provisions of Section 411.17 are not applicable.*

411.18 Access for Persons with Disabilities

The application indicates that the proposed development will accommodate ADA requirements. *The Board finds that the provisions of Section 411.18 are satisfied.*

411.19 Financial Capacity and Maintenance

The applicant is an established business at the proposed development site that has demonstrated financial capacity and ability to maintain itself. The developer has adequate financial and technical capacity to complete the project, and once it is completed, the project is expected to have adequate resources to maintain itself. *The Board finds that the provisions of Section 411.19 are satisfied.*

411.20 Noise and Dust

No exterior construction is proposed and therefore the applicant’s consideration of reasonable construction times and decibel levels in accordance with Section 524.1 of the Brunswick Zoning Ordinance is not required. *The Board finds that the provisions of Section 411.20 are satisfied.*

411.21 Right, Title and Interest

The applicant has sufficient right, title and interest to develop the land. *The Board finds that the provisions of Section 411.21 are satisfied.*

411.22 Payment of Application Fees

The applicant has paid all applicable development review application fees. *The Board finds that the provisions of Section 411.22 are satisfied.*

**DRAFT MOTIONS
8 Business Parkway (Change of Use)
PLANNING BOARD APPROVAL DATE: October 25, 2016
CASE NUMBER: 16-047**

Motion 1: That the Major Development Review Final Site Plan application is deemed complete.

Motion 2: That the Major Development Review Final Site Plan is approved with the following conditions:

1. That the Board’s review and approval does hereby refer to these findings of fact, the plans and materials submitted by the applicant and the written and oral comments of the applicant, its representatives, reviewing officials, and members of the public as reflected in the public record. Any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require a review and approval in accordance with the Brunswick Zoning Ordinance.
2. Prior to issuance of a building permit, a provisional parking analysis, and parking plan shall be approved by the Code Enforcement Officer (CEO).
3. Prior to issuance of a building permit, the business operating hours shall be approved by the Code Enforcement Officer (CEO).

** Please note that Development Review Site Plan approvals by the Planning Board or Staff Review Committee shall expire at the end of two years after the date of final approval unless all construction has been completed by that date (Section 407.4.B of the Brunswick Zoning Ordinance).*

October 19, 2016

STAFF REVIEW COMMITTEE NOTES

Staff present:

John Foster (Public Works Director, Town Engineer), Rob Pontau, (Brunswick Sewer District), Jeff Emerson (Deputy Fire Chief), Jeff Hutchinson (Code Enforcement Officer), Anna Breinich (Planning and Development Director) **Non-voting staff:** Jared Woolston (Planner), **Applicants Present:** Kevin Clark (Sitelines), Don Borkowski (Bowdoin College) **Public present:** Birgitta Peterson

1. **Case # 16-047, Change of Use:** The Committee will review and provide a recommendation to the Planning Board on a combination **Sketch/Final Plan Major Development Review** application submitted by authorized representatives from Sitelines, P.A. for Allied Composite Center LLC, to change the use of a 28,000 square foot building to Service Business, Class 2. The proposed development is located at Lot 4 of the Brunswick Industrial Park Expansion at 8 Business Parkway in the **I2 (Church Road Industrial Park) Zoning District (Map 17, Lot 68).**

Kevin Clark (Sitelines):

- He is representing Allied Composite Center and Jim Howard. Based on consultations with staff, in order to have potential leases in this building, the approved use needs to be changed from Industry Class 2 to Service Business Class 2. No new exterior construction is proposed. The building is already constructed; the site is already complete and stable. Any work would be inside for potential tenants. No expected increase in water or sewer use.

Rob Pontau (Sewer District):

- What is service class?
 - Anna: Anything that provides a service to a person.
- Then he's probably correct in expecting no increase in water or sewer, but if it was to become a restaurant, there would be an increase in use – not that the facilities can't handle it, but you just might be eligible to participate in more impact fees. Can you provide anything on the flows – we know the existing, here is the proposed?
 - Kevin: I don't really have anything other than the number of employees. The previous business had about 25 employees. He's proposed it at 10-20.
- I'm good with that. There is a change in use permit, but there is no charge for that. If they're not doing any exterior plumbing – if it's all interior – they may want to come and look at it just to make sure there are no floor drains tied into it.

John Foster (Town Engineer):

- Not clear what's happening. Is there one tenant and now there will be multiple tenants? Are there changes going on to the interior of the building or reallocation of space?
 - Kevin: There could be. He has not been able to lease the property as an industrial use. It is currently vacant. Harbor Tech got purchased by another composite company and moved their operations. Allied Composite Center owns the lot next door as well. They had an application to build at one time, and have a larger composite campus, but that did not come to fruition.
- If he finds one tenant that will occupy the existing space as is – as far as office/warehouse – everything's good to go – but if he finds a tenant like a call center and they want a bunch more cubbies or something, then he comes back?

- Kevin: If we had to add more parking. At this point, the anticipated employees exceed the number of parking spaces.
- Anna: A call center would not be a service business. It's another change of use.
- No more questions.

Jared Woolston:

- Just to be clear – it's a change of use and we have no defined tenant? We don't know exactly what they're doing, but we have some idea. A change of use that requires major development review – there are none on record. This is brand new. They have to be here because it's a change of use in a building of greater than 10,000 square feet.

Jeff Emerson (Deputy Fire Chief):

- When they decide what's going to be done with it, we'll have some building code issues. He has nothing for this meeting.

Jeff Hutchinson (CEO):

- These plantings were in the original development. You might want to check those – I don't know if they all survived. They're supposed to be conifer-type trees, whether they be spruce, fir, pine. There is a home there.
 - Kevin: Okay. We'll check those trees.
- The most recent use for parking was industry, which was 2 spaces per thousand. Service doubles that, so for 28,000 square feet, 4 spaces per thousand turns out to be 112 parking spaces, and you have 28. You need to make a good case to the Planning Board that 28 spaces will be sufficient.
 - Jeff, responding to John Foster's comment that they don't comply due to parking spaces: There is a provision in the ordinance where they can make a case before the Planning Board that the proposed parking will be sufficient for their use.

Anna Breinich:

- No further questions.

Public Comments:

- None

2. **Case # 16-050, Environmental Studies Academic Center:** The Committee will review and provide a recommendation to the Planning Board on a **Sketch Plan Major Development Review** application submitted by authorized representative Don Borkowski for Bowdoin College, to construct a new academic building with various site modifications including new landscaping, a bio-swale, and pedestrian walkways. The proposed development is located at 38 Harpswell Road in the **CU3 (College Street) Zoning District (Map U-09, Lot 47)**.

The Committee spent a few minutes explaining what needed to be contained in the site plan for their review, such as building height, footprint dimensions, actual setbacks, impervious coverage and parking spaces – everything that is included in the table. If they'd like to see what it takes to get a complete application, they can look in the zoning ordinance. It's not a sketch plan requirement, but it does make things clear.

Jeff Hutchinson (CEO):

- Before you even get to that, I think it's important for sketch plan to provide a new combined deed, because this was 2 separate lots; now we're into one lot. I assume that has been combined and a new deed has been created and recorded? The actual copy of the deed is what we usually want.
 - Don: He references the deed, but does not provide it.

- Jared: There again, it's not required, but it does help demonstrate the right title and interest.
- Jeff: It also helps to know the portions of the lot into each of the zoning districts so they can confirm that more than 50% of the lot is in the CU3, because that will be the district governing the whole lot.
- Jared: So, the CU table, the deed, and show the zoning districts on the site plan/sketch plan.

Anna Breinich:

- She can't go any further until she has that.

Jeff Hutchinson:

- Parking is existing. He saw in their letter that the Environmental Studies program will move from other buildings to this one, so you're not increasing the number of students or professors because of this. What is happening with the spaces that they are moving from?
 - Don: There's a real need for lab space in Druckenmiller, so a big impetus behind this is to get those faculty members out of their offices in Druckenmiller and over to the new building, along with some of their activities, so we can then create those labs.
- Those labs are not going to create more students, so it is just to relieve the density. Overall campus is not going to require new parking because of this.
 - Don: No, and on the plan the lines for the parking spaces are backwards. The plan is still to enter from the existing driveway that's adjacent to the building, then there would be the angled parking heading in this direction - there's a small garage here that's filled with lumber for our carpentry shop - and then there will be a one way, 14-16 foot driveway to connect to the Coffin lot if there's any type of overflow. The Coffin lot is never more than half filled.

Anna Breinich:

- 204.3 *Additional Requirements to the Dimensional and Density Tables* – they will need to address G – *Height Restrictions in the CU3 district – height may be increased by an additional 5 feet for every 10 additional feet of setback from any non-college-owned residential property line, up to a maximum of 55 feet.* If you can show the distance between the first non-college-owned property, to prove that you meet that requirement.
 - Don: The building is only 40 feet high.
- The other questions are more relative to final plan – landscaping, keeping existing trees....
 - Don: We're going to add some additional evergreens to the buffer.

Jeff Emerson (Deputy Fire Chief):

- When are you planning on submitting the final plans?
 - Don: January-February
- It looks like you've used the current adopted versions of the fire code to develop these plans, and the last I heard the 2015 version may be adopted in November, so you may want to do a comparative analysis in case you need to use the 2015 codes so you won't run into problems. When they submit their plans, the current adopted version will of the fire code will be used, and it is likely to be the 2015 version.
 - Don: We met with Dick Mason (State Fire Marshall's Office) last week, and he was comfortable that we were working under the current code.
- By statute, they have to adopt it within 2 code cycles, and it's now 2 code cycles. If he's comfortable with that, it's fine. They're the ones we're worried about. I'm surprised they didn't push you to the 2015.

Jared Woolston:

- Would he write that up for you? That would be good to have on the record.
 - Don: We sent a set of minutes from the meeting to him.
 - Jeff E.: That could be a real problem if they change their mind at the last minute.

Rob Pontau (Sewer District):

- Typical permit process – we’ll be looking at anticipated flows. For us, we do it by property, so it doesn’t matter that you’re vacating other buildings, but you get credited for the buildings that were on the site previous – those flows – but we can look that up. I’ll need that anticipated flow.
 - Don: Would that include that house that was at 38 Harpswell as well as 42?
- Yes, both lots that are being combined into one. You’ll get some credit there, and I’ll have to use that to calculate your entrance charge. The proposed tie-in looks fine to us – you’re going to drop a manhole in; no issues. We’ll want to inspect it. That’s part of the permit process.

John Foster:

- At this point, there’s no connection to storm drainage?
 - Don: No, everything’s going to be contained on the site. We’re looking at using gray water to flush the toilets in the building. We tied it to sanitary. We’re looking at creating a bioswale on the Harpswell Road side of the site to collect any additional roof water and storm water. That plan hasn’t been finalized yet, but we’re looking to introduce no stormwater into the town storm system. It goes through a tank in the basement where it’s treated and then pumped through the building – separate lines just for the toilets.
 - Rob Pontau: Going back to the gray water, you’ve got this on one of your other buildings – you’ll need another meter because the gray water is unmetered. It has worked out in the past and they can handle it – they won’t stand in the way of Bowdoin’s effort to improve the environment, although it is more difficult to do.
 - Jeff H.: Will there be any laundry facilities?
 - Don: No.
- Is this a new manhole on College Street? You’ve labeled it SMH – 2.
 - Rob: It looks like it’s sewer and it’s not one that we have now.
- You’re going to dig up the road that the Town and Bowdoin just paved?
 - Don: It’s either there or Harpswell.
- There are special requirements if you’re going to excavate a road, because there’s a moratorium for five years after paving.
 - Don: We can look at coming off of Harpswell.
- If it’s already stubbed down. You’re also showing new water.
 - Don: That’s not accurate.
- Underground electric – is that correct?
 - Don: I’m meeting with CMP about this tomorrow, because it impacts Birgitta’s house. They’ve got kind of a strange set-up here – they’ve set a pole basically on private property. It shouldn’t really be there. I’m going to meet with the CMP engineers to see what it would mean for the other properties if that pole were removed. The people on the south side of Harpswell Place are fed off a transformer and a pole that goes over one structure and feeds the other.
 - Birgitta Peterson: The other pole that Don’s referring to is right in the middle back corner of her and her husband’s (Kevin Cashman) property (8 Harpswell Place), right next to the back edge of their house. They are the most directly impacted by the building location.
 - Don: This pole goes right down to the meter on Bartlett’s house, which is like 6 feet off the ground.
- Our issue would be that Harpswell Road was paved last year
 - Don: Do you have a preference, whether we go Harpswell or College?
- Harpswell is a more heavily trafficked road, so we’d prefer not to have cuts in Harpswell Road, but it’s also going to make you do a more expensive restoration.
- You’re reversing the direction of traffic flow in this driveway from what’s there now?
 - Don: Correct.

Public Comment:

Birgitta Peterson, 8 Harpswell Place:

- Doesn't have anything in particular to add, but wanted to stay engaged in the process. She was invited to a presentation by Bowdoin last week, and the communication has been great during this process. She would like to stay informed.

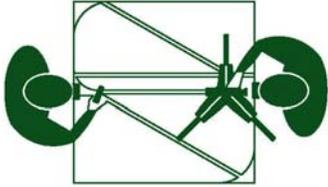
Jared Woolston:

- This will need to come back for a final review; this is just sketch plan.

Anna Breinich:

- How are you going to be heating? Any condensers?
 - Don: They are not sure yet, but have a conference call Friday to discuss options. There is a mechanical well up on the third floor on the west side of the building. It's in a well with a roof and it is acoustically treated, because there are going to be solar panels on the roof.
 -

The meeting was adjourned.



October 21, 2016

1111-7

Jared Wolston, Town Planner
Town of Brunswick
85 Union Street
Brunswick, Maine 04011

**Re: Major Development Review & Change of Use Application
ALLIED COMPOSITE CENTER, LLC
8 Business Parkway, Brunswick
Map 17, Lot 68**

Dear Jared,

Enclosed please find copies of the Major Development Review Final Plan Application Form, Site Plan, and supporting documentation for proposed Change of Use for 8 Business Parkway, also known as Lot 4 of the Brunswick Industrial Park Expansion. Allied Composite Center, LLC is proposed to change the approved use for the existing 28,000 sq. ft. facility from Industry, Class 2 to Service Business, Class 2. Please note that this change of use is intended for the building and not a specific tenant.

The parcel is located at 8 Business Parkway and is shown as Map 17, Lot 68 of the Town of Brunswick Tax Maps. The site is located within the I2 (Church Road Industrial Park) Zone and the Site Plan was originally approved in October 2004, with an expansion of the office area approved in the fall of 2007. The facility consists of a 28,000 sq. ft. commercial building, paved loading areas and a paved parking lot with 28 spaces (including 2 handicap accessible spaces). In accordance to MDEP Site location permit # L-6773-39-L-A a 50' buffer to Greenwood Road and a 100' vegetated buffer to the pond have been maintained.

To facilitate your review of our proposal, the following issues are summarized in accordance with *CHAPTER 5: DEVELOPMENT REVIEW PLAN STANDARDS* of the Ordinance.

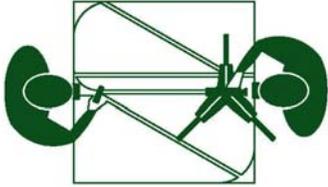
501 PRESERVATION OF NATURAL FEATURES AND NET SITE AREA:

A 100-foot setback is being maintained from the upland edge of the wetlands associated with the stream and man-made ponds near the southeasterly boundary of the lot. Additionally, a 50' buffer to Greenwood Road is provided and white pine plantings were added to enhance screening.

The area of the parcel is 2.95 acres, with 0.43 acres of wetlands and pond, resulting in a Net Site Area of 2.52 acres.

502 FLOOD HAZARD AREA:

The property is not located within a FEMA Special Flood Hazard Area.



503 STEEP SLOPES AND ENBANKMENTS:

There are no steep slopes greater than 25% located on the property.

504 STORM WATER MANAGEMENT:

As part of the MDEP Site Location of Development Application for the Brunswick Industrial Park Expansion prepared by Christopher Belanger, PE previously of Sitelines, P.A., an impervious area of 1.18 acres was accommodated in the stormwater quality and quality treatments and was approved for this parcel. The design proposes 1.00 acres of impervious area after the completion of Phase 3 build-out.

505 GROUNDWATER:

The project served by Public Sewer and Water facilities. No adverse impact to groundwater is expected from this development.

506 EROSION AND SEDIMENTATION:

The Erosion Control Plan & Notes were followed during construction and the completed site is vegetated and stable.

507 SEWAGE DISPOSAL:

The facility is currently connected to the 8" sewer main in Business Parkway. No additional discharge of wastewater above the use of the previous tenant is anticipated from the project.

508 WATER SYSTEM:

The facility is currently connected to the 8" water main in Business Parkway via a 2" domestic service line and a 6" fire service line. No additional water demand above the use of the previous tenants is anticipated from the project.

509 COMMUNITY FACILITIES IMPACT ANALYSIS:

The project is appropriately located within an industrial park and the proposed use is an allowed use within the I2 Zone. Schools and recreational services would have no impact from this project.

510 DEVELOPMENT IMPACT FEES:

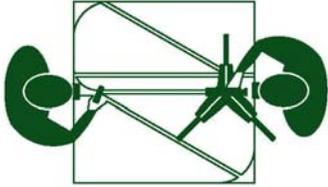
The facility was previously assessed impact fees based on 13 tons per year of solid waste (\$3,361.28) and 300 GPD for a sewage entrance charge (\$2,019.43). No additional impacts above those of the previous tenants is anticipated from the project.

511 DEVELOPMENT OF NEW STREETS:

There are no new streets proposed as part of this project.

512 OFF STREET PARKING:

The facility provides a total of 26 parking spaces, including 2 handicap accessible spaces. The existing building consists of an office area of 2,800 sq. ft. and industrial/warehouse space of 25,200 sq. ft. The ordinance requirement of 4 spaces per 1,000 sq. ft. for the proposed service business use



would indicate that the project would require 112 spaces. The previous manufacturing use of the building require was adequately serviced by this number of spaces based on the number of employees needed due to automation and the large size of the product. Potential tenant's would have fewer than 20 employees and would not involve the visitation of the public. Therefore, the existing parking is adequate. In the future, should additional parking be required, the applicant controls the adjacent property and could accommodate a parking expansion, if necessary. The adjacent parcel has been approved for up 1.34 acres of impervious area, which would accommodate well over 100 parking spaces.

The existing stall dimensions and traffic lanes meet acceptable design standards.

513 CURB CUTS:

The facility currently utilizes one curb cut on the Business Parkway. No new curb cuts are proposed.

514 OFF STREET LOADING:

The facility provides for delivery and loading at two locations on the existing building and the central entrance is designed to allow tractor trailer vehicles to back into the building via the central overhead doors.

515 APPEARANCE ASSESSMENT:

There is no new exterior construction proposed as part of this project; therefore this section is not applicable.

516 BUILDING CONFIGURATION:

There is no new exterior construction proposed as part of this project; therefore this section is not applicable.

517 PRESERVATION OF HISTORIC RESOURCES:

There is no new exterior construction proposed as part of this project; therefore this section is not applicable.

518 ACCESS FOR PERSONS WITH DISABILITIES:

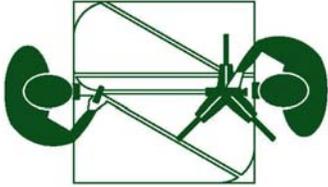
The facility accommodates the requirements of the ADA in regard to parking and access.

519 RECREATIONAL REQUIREMENTS FOR RESIDENTIAL DEVELOPMENTS:

This is not a residential project; therefore this section is not applicable.

520 FISCAL CAPACITY:

The proposed internal improvements to the facility, estimated at approximately \$250,000, will be completed by Allied Composite Center, LLC and Priority Real Estate Group, LLC utilizing operating capital. Should a lease be signed with a tenant this fall, work would begin immediately and continue throughout the fall and winter.



521 PERFORMANCE GUARANTEE:

There is no new exterior construction proposed as part of this project; therefore a performance guarantee is not required.

522 HOME OWNERS/PROPERTY OWNERS ASSOCIATION:

There is no homeowner or property owner association.

523 PROTECTED CONSERVATION LAND:

This is not a residential project; therefore this section is not applicable.

524 NOISE AND DUST:

There is no new exterior construction proposed as part of this project. Any proposed uses will be internal to the building. Noise and traffic associated with the project will be consistent with other industrial park uses.

We look forward to meeting with you and the Planning Board at their October 25, 2016 meeting to review the project and gain their approval. Should you have any questions, please call.

Very truly yours,

Kevin P. Clark

Kevin P. Clark, PLS

Enclosures

cc: Jim Howard

**MAJOR DEVELOPMENT REVIEW
FINAL PLAN APPLICATION**

1. Project Name: 8 Business Parkway

2. Project Applicant

Name: Allied Composite Center LLC

Address: 2 Main Street

Topsham, ME 04086

Phone Number: _____

3. Authorized Representative

Name: Sitelines, PA. Attn: Kevin Clark, PLS

Address: 8 Cumberland Street

Brunswick, ME 04011

Phone Number: 207-725-1200 xt. 18

4. List of Design Consultants. Indicate the registration number, address and phone number of any engineer, surveyor, architect, landscape architect or planner used:

1. Surveyor: Kevin P. Clark, PLS #2245, Sitelines, P.A., 207-725-1200 xt. 14

2. Engineer: Curtis Y. Neufeld, P.E. #9779, Sitelines, P.A., 207-725-1200 xt. 18

3. _____

5. Physical location of property being affected: 8 Business Parkway

6. Lot Size: 2.95 Acres

7. Zoning District: I-2

8. Indicate the interest of the applicant in the property and abutting property. For example, is the applicant the owner of the property and abutting property? If not, who owns the property subject to this application? Change of Use from Industry, Class 2 to Service Business, Class 2
Please refer to Cover Letter for more details.

9. Assessor's Tax Map 17 Lot Number 68 of subject property.

10. Brief Description of proposed: Refer to Cover Letter

11. Describe Specific Physical Improvements to be Done: Refer to Cover Letter

Owner Signature: _____

Applicant Signature (*if different*): _____

Required Attachments (by Applicant):

- Final Plan Check List
- Final Plan Requirements for Open Space Developments (if applicable)
- Request for Waivers (if applicable)
- Required Copies of Final Plan

Required Attachment (by Planning and Development Department):

- Listing of all owners of property within 200-foot radius of property under review.

FINAL PLAN REQUIREMENTS

Key: "O" = omit; "S"=submit; "NA"=not applicable; "W" = waiver P=pending

Item	O	S	NA	W	P	Comments
Name of Development		X				
Scale, date, north point, area, number of lots (if subdivision)		X				
Boundaries of all lots and tracts with accurate distances and bearings, locations of all permanent monuments property identified as existing or proposed.		X				
Certification by a professional land surveyor that the land has been surveyed and the boundaries established in accordance with the State of Maine Board of Licensure for Professional Surveyors standards for Category 1 (Standard Boundary Survey), conditions 1, 2, or 3.		X				
Existing zoning district and overlay designation.		X				
Names of engineer and surveyor; and professional registration numbers of those who prepared the plan.		X				
Names of current owner(s) of subject parcel and abutting parcels.		X				
Name, location, width of paving and rights-of-way, profile, cross-section dimensions, curve radii of existing and proposed streets; profiles of center-lines of proposed streets, at a horizontal scale of 1" equals 50' and vertical scale of 1 inch equals 5 feet, with all elevations referred to in U.S.G.S. datum.			X			
A general road plan noting circulation, direction, traffic control devices, street lighting and type of lighting proposed.			X			No new lighting is proposed
Existing and proposed easements associated with the development.		X				
Kind, location, profile and cross-section of all proposed drainage facilities, both within the development and outside of it, and a storm-water management plan which includes the submission requirements listed in the storm-water management checklist available in the Planning Department.						
Location of features, natural and artificial, such as water bodies, wetlands, streams, vegetation, railroads, ditches and buildings.		X				

Location of existing and proposed utilities; water, sewer, electrical lines, and profiles of underground facilities. Tentative locations of any private wells.		X			
Existing and proposed location, size, profile and cross section of sanitary sewers; description, plan and location of other means of sewage disposal with evidence of soil suitability.			X		
Topography with counter intervals of not more than 2 feet.		X			
A Class A (high intensity) Soil Survey prepared in accordance with the standards of the Maine Association of Professional Soil Scientists.			X		No new construction proposed
Location of all existing trees over 10 inches in diameter, locations of tree stands, and a plan showing all trees to be removed as a result of the development proposal.			X		No new clearing proposed
Lighting plan showing details of all proposed lighting and the location of that lighting in relation to the site.			X		No new lighting proposed
Existing locations and proposed locations, widths and profiles of sidewalks.		X			
Location map.		X			
Approximate locations and dimensions of proposed parking areas.			X		No new parking proposed
Proposed ownership and approximate location and dimensions of open spaces for conservation and recreation.			X		
Grading, erosion control, and landscaping plan; proposed finished grades, slopes, swells, and ground cover or other means of stabilization.			X		
Reference to special conditions stipulated by the Planning Board, with conditions either set forth in full or on the plan or identified as specific documents filed with the Board.			X		
A wetlands map drawn by a specialist delineating wetland boundaries in accordance with the methods prescribed by the US Army Corps of Engineers.			X		
Dedicated public open spaces, areas protected by conservation easements, and existing and proposed open spaces or recreation areas.		X			

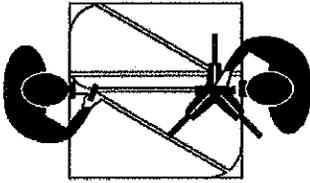
For Open Space Development, a note indicating the total permitted lot count of the entire land tract based upon the destiny standards in this Ordinance, the number of lots created by the Plan, and the number of lots permitted to be subdivided in the future, as well as a table showing setback requirements and impervious surface coverage limits for each lot.			X			
Building envelopes showing acceptable locations for principal and accessory structures.			X			

FINAL PLAN/SUPPORTING DOCUMENTS

Key: "O" = omit; "S"=submit; "NA"=not applicable; "W" = waiver P=pending

Item	O	S	NA	W	P	Comments
Documentation of Ownership or contract.		X				
Drafts of legal documents appropriate to the application, including: deeds, easements, conservation easements, deed restrictions or covenants, home/property owners association declarations and by-laws, and such other agreements or documents as are necessary to show the manner in which conservation land will be owned, maintained, and protected.			X			
Draft performance guarantee or conditional agreement.			X			
Disclosure of any required permits from the Department of Environmental Protection, Marine Resources, US Army Corps of Engineers, Department of Inland Fisheries and Wildlife, or other agencies, as applicable; or, if a permit has already been granted, a copy of that permit.		X				
Any additional studies required by the Planning Board, which are deemed necessary in accordance with this Ordinance.			X			None Anticipated
Storm water management program for the proposed project prepared by a professional engineer.			X			
A storm water management checklist prepared by the Cumberland County Soil and Water Conservation District made available at the Brunswick Department of Planning and Development.	X					

An erosion and sedimentation control checklist prepared by the Cumberland County Soil and Water Conservation District.	X					
A statement from the Brunswick-Topsham Water District of conditions under which water will be provided.					X	
A statement from the Brunswick-Topsham Water District of its review and comments on the proposed use if the project involves development within the Aquifer Protection Zone.			X			
A Statement from the Fire Chief recommending the number, size, and location of hydrants, available pressure levels, road layout and street and project name, and any other fire protection measures to be taken.			X			No new construction proposed
A statement from the Superintendent of the Brunswick Sewer District of the conditions under which the Sewer District will provide sewerage disposal service and approval of the sanitary sewers proposed within the development.					X	
Where a septic system is to be used, evidence of soil suitability.			X			
All applicable materials necessary for the reviewing entity to review the proposal in accordance with the Criteria of Section 411.		X				
A plan of all buildings with new construction or expansion of an existing facility, including type, size, and footprint, floor layout, setback, elevation of first floor slab, storage, and loading areas.			X			
An elevation view of all sides of each building proposed indicating height, color, bulk, surface treatment, and signage.			X			No New Buildings Proposed
A circulation plan describing all pedestrian and vehicle traffic flow on surrounding road systems.			X			
The size and proposed location of water supply and sewage disposal systems.			X			
A site landscaping plan indicating grade change, vegetation to be preserved, new plantings used to stabilize areas of cut and fill, screening, the size, location and purpose and type of vegetation.			X			



October 3, 2016

1111.01

Mr. James Howard
Allied Composite Center LLC
2 Main Street
Topsham, ME 04086

**Re: Designation of Agent Authorization
Allied Composite Center LLC
8 Business Parkway, Brunswick, Maine**

Dear Jim:

As required by various approval agencies, please indicate by signing below that Sitelines, PA is authorized to act as agent for Allied Composite Center LLC, for the specific purpose of preparation and submission of local and state permitting applications on your behalf for the proposed Change of Use Application for the existing 28,000 sq.ft. facility located at 8 Business Parkway in Brunswick, Maine.

Sincerely,

Kevin P. Clark

Kevin P. Clark, PLS
President

The undersigned hereby gives Sitelines, PA the authority to act as agent for Allied Composite Center LLC for the specific purpose of preparation and submission of local and state permitting applications for the project specifically identified above.

10-2-16

James Howard, Allied Composite Center LLC

Date



MAINE

Department of the Secretary of State
Bureau of Corporations, Elections and Commissions

Corporate Name Search

Information Summary

[Subscriber activity report](#)

This record contains information from the CEC database and is accurate as of: Tue Oct 04 2016 10:41:27. Please print or save for your records.

Legal Name	Charter Number	Filing Type	Status
ALLIED COMPOSITE CENTER, LLC	20070883DC	LIMITED LIABILITY COMPANY (DOMESTIC)	GOOD STANDING

Filing Date	Expiration Date	Jurisdiction
10/02/2006	N/A	MAINE

Other Names (A=Assumed ; F=Former)

NONE

Clerk/Registered Agent

JOHN MONCURE
9 BOWDOIN MILL ISLAND

TOPSHAM, ME 04086

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Click on a link to obtain additional information.

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[Plain Copy](#)

[Certified copy](#)

Certificate of Existence [\(more info\)](#)

[Short Form without
amendments
\(\\$30.00\)](#)

[Long Form with
amendments
\(\\$30.00\)](#)

QUITCLAIM DEED

BRUNSWICK ECONOMIC DEVELOPMENT CORPORATION, a nonprofit corporation organized and existing under the laws of the State of Maine with a mailing address of 28 Federal Street, Brunswick ME 04011, for consideration paid, the receipt of which is hereby acknowledged, does hereby remise, release, bargain, sell, convey and further quitclaim unto **ALLIED COMPOSITE CENTER, LLC**, a limited liability company existing under the laws of the State of Maine and located in Brunswick, Cumberland County, Maine, with a mailing address of Border Trust Business Center, 2 Main Street, Brunswick ME 04011, the land in Brunswick, Cumberland County, Maine, described as follows:

MAINE REAL ESTATE TAX PAID

A certain lot or parcel of land with any buildings and improvements located therein situated on the southerly side of Business Parkway in the Town of Brunswick, County of Cumberland and State of Maine and being Lot 4 on a plan entitled "Subdivision Plan Industrial Park Expansion, Brunswick Industrial Park (Phase IV), Record Owner: Town of Brunswick, Business Parkway, Brunswick, Maine" prepared by Sitelines P.A. dated March 28, 2003, last revised on June 23, 2003, approved by the Town of Brunswick Planning Board on June 24, 2003 and recorded in the Cumberland County Registry of Deeds on September 9, 2003 in Plan Book 203, Page 520 as modified by a plan entitled "Subdivision Plan - Amendment, Industrial Park Expansion, Brunswick Industrial Park (Phase IV), Record Owner: Town of Brunswick, Business Parkway, Brunswick, Maine" prepared by Sitelines P.A. dated March 28, 2003, last revised on August 29, 2003, approved by the Town of Brunswick Planning Board on September 3, 2003 and recorded in the Cumberland County Registry of Deeds on September 22, 2003 in Plan Book 203, Page 530.

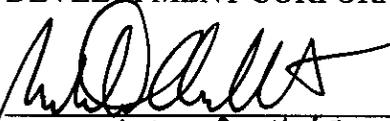
This conveyance is subject to:

1. The Declaration of Covenants and Restrictions of the Brunswick Industrial Park dated February 12, 1981 and recorded in the Cumberland County Registry of Deeds in Book 4739, Page 302, as amended from time to time, which grantee and its successors and assigns forever hereby agree to observe.
2. A 20-foot pedestrian easement as shown on a plan entitled "Final Plan of Harbor Technologies" prepared by Sitelines P.A. and dated 08-02-04.
3. Any utility easements.
4. The requirements of the federal, state and local approval processes.

To have and to hold the same, together with all the privileges and appurtenances thereunto belonging to it, the said Allied Composite Center, LLC, its successors and assigns forever.

Brunswick Economic Development Corporation has caused this instrument to be signed in its corporate name by Mike Ouellet, its Vice - Chair, duly authorized, this 18th day of January 2008.

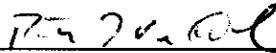
BRUNSWICK ECONOMIC
DEVELOPMENT CORPORATION

By: 
Mike Ouellet
Its Vice - Chair

STATE OF MAINE
COUNTY OF CUMBERLAND, ss.

January 18, 2008

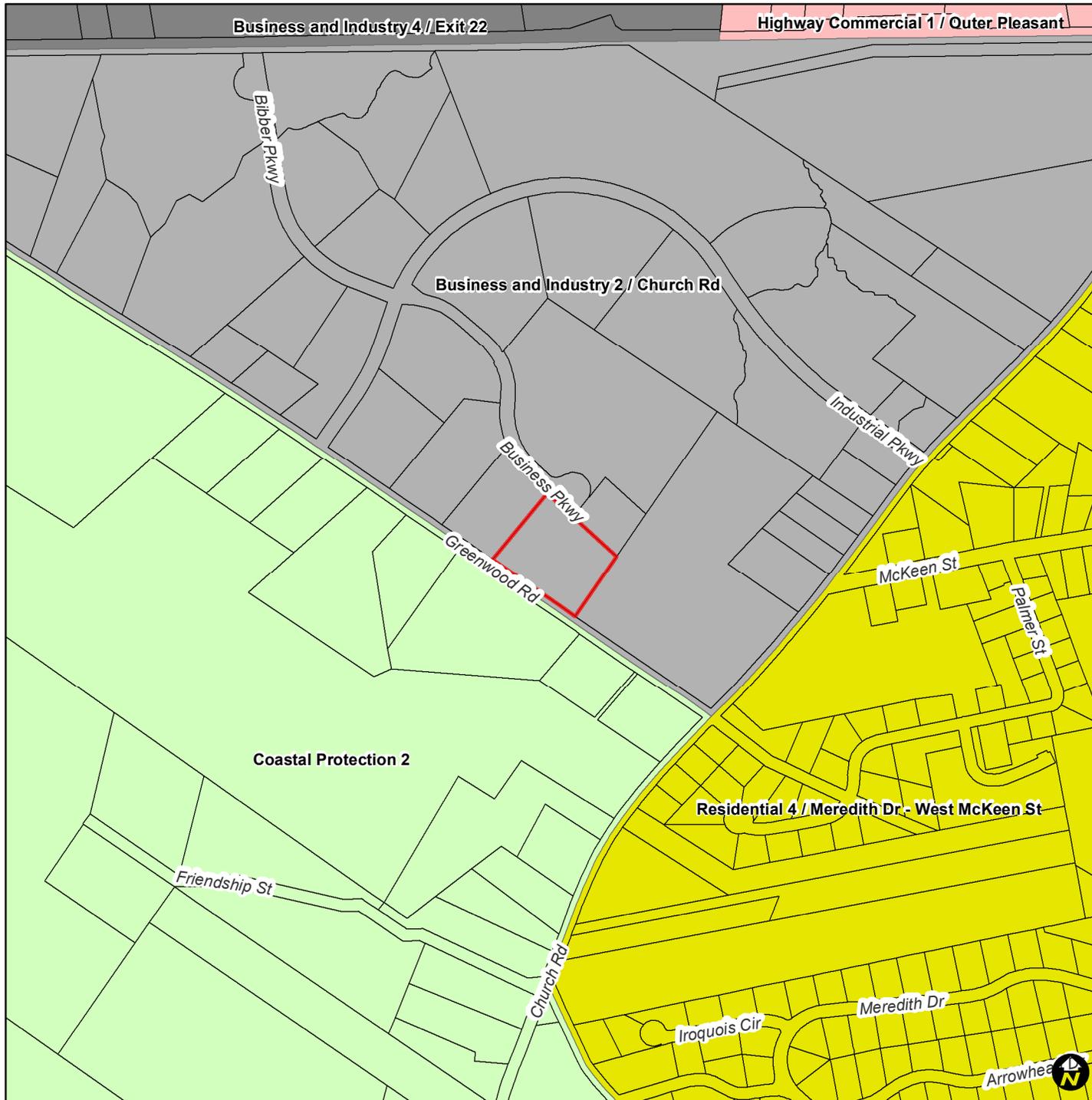
PERSONALLY APPEARED the above-named Michael Ouellet in his/her capacity as Vice - Chair of Brunswick Economic Development Corporation and acknowledged the foregoing instrument to be his/her free act and deed in his/her said capacity and the free act and deed of Brunswick Economic Development Corporation.


~~Notary Public~~ Attorney at Law
Peter Van Heme

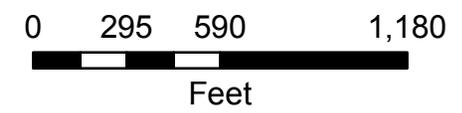
Received
Recorded Register of Deeds
Jan 25, 2008 11:44:21A
Cumberland County
Pamela E. Lovley

Brunswick Maine

8 Business Parkway



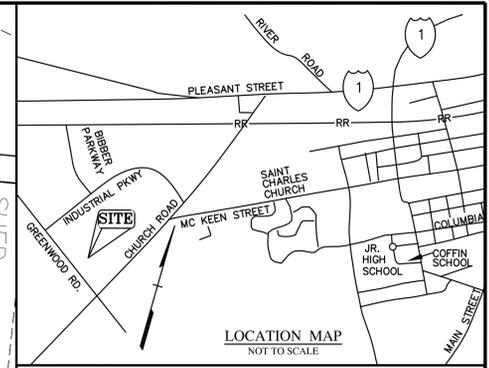
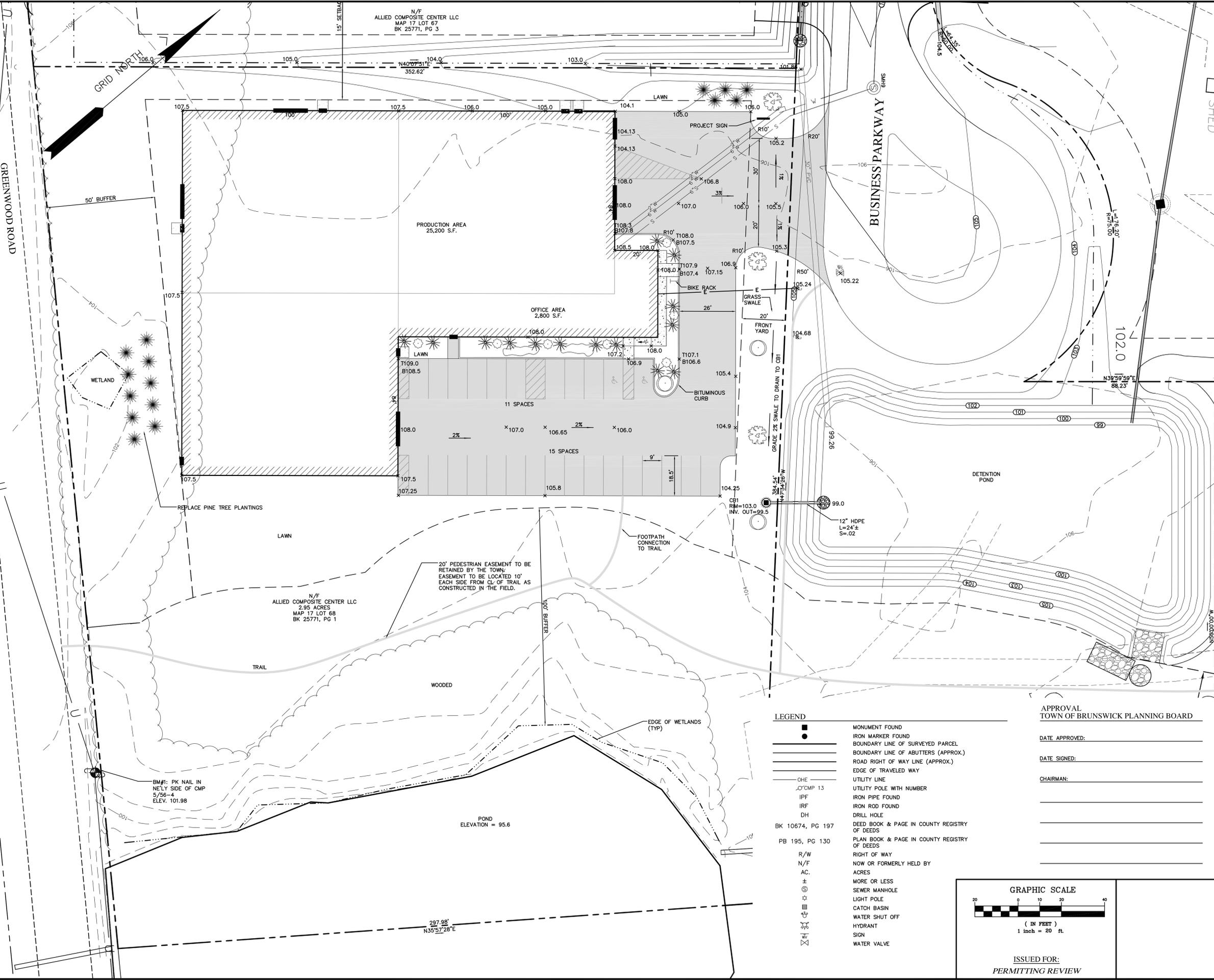
- Legend**
- Selected Parcels
 - Parcels
 - Town Boundary
 - Town Center 1 / Maine Street
 - BNAS Conservation District
 - Town Center 2 / Fort Andross
 - Town Center 3 / Lower Park Row
 - Town Residential 1 / Inner Pleasant St
 - Town Residential 2 / Federal St
 - Town Residential 3 / Water St
 - Town Residential 4 / Jordan Acres
 - Town Residential 5 / Columbia Ave - Spring St
 - Residential 1 / Longfellow St
 - Residential 2 / Meadowbrook - Parkview
 - Residential 3 / Maquott Rd
 - Residential 4 / Meredith Dr - West McKeen St
 - Residential 5 / River Rd
 - Residential 6 / Cook's Corner
 - Residential 7 / McLellan-Garrison St
 - Residential 8 / College Park
 - College Use/Town Conservation District
 - College Use 1 / Campus Center
 - College Use 2 / Pickard Field
 - College Use 3 / College St
 - College Use 4 / Bowdoin Pines
 - College Use 5 / Brunswick Apartments
 - College Use 6 / Cleaveland St - Bath Rd
 - College Use 7 / Longfellow Ave - South St
 - Commercial / Cook's Corner
 - Highway Commercial 1 / Outer Pleasant
 - Highway Commercial 2 / Inner Bath Rd
 - Mixed Use 2 / Intown Railroad Corridor
 - Mixed Use 3 / Upper Harpswell Rd
 - Mixed Use 4 / Fox Run
 - Mixed Use 6 / Lower Harpswell Rd
 - BNAS Reuse District
 - Business and Industry 1 / Industry Rd
 - Business and Industry 2 / Church Rd
 - Business and Industry 3 / Bath Rd
 - Business and Industry 4 / Exit 22
 - Farm Forest 1 / Durham-Hacker Road Area
 - Farm Forest 3 / New Meadows River Area
 - Country Residential 1 / Northwest Brunswick
 - Country Residential 2 / Old Bath Rd
 - Rural Mixed Use 1 / Lower Old Bath Rd
 - Rural Mixed Use 5 / Portland Road Area
 - Coastal Protection 1
 - Coastal Protection 2



This map was generated by the Town of Brunswick's online GIS. This information has been compiled from various public and private sources. While every attempt has been made to provide accurate information, neither the municipality nor the service host guarantee the accuracy of information provided herein.



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 X:\LAND PROJECTS\1111\DWG\1111.01 SITE COU-2016-1000.DWG, SITE PLAN, 10/03/2016 9:02:46 AM, KEVIN C.



- PLAN REFERENCE:
- "SUBDIVISION PLAN - AMENDMENT 1 INDUSTRIAL PARK EXPANSION" PREPARED FOR THE TOWN OF BRUNSWICK DATED 03-28-03 LAST REVISION OF 05-08-03. PREPARED BY SITESLINES, P.A. OF BRUNSWICK, MAINE
 - TOPOGRAPHIC AND BOUNDARY SURVEY COMPLETED BY ROBERT M. SPIVEY PLS # 1335 (P.O. BOX 901, BRUNSWICK, MAINE 04011 TEL. (207) 721-0511) PORTION OF WARRNER LUMBER CO. LOT FOR TOWN OF BRUNSWICK, MAINE. DATED OCT. MAY 29, 2002.
- GENERAL NOTES:
- THE SITE IS ON MAP 17 LOT 68.
 - AREA OF EXISTING LOT = 2.95 ACRES
 - ORDINANCE STANDARDS:
 ZONE: I2 (CHURCH ROAD INDUSTRIAL PARK)
 MINIMUM LOT SIZE: 20,000 S.F.
 DIMENSION REQUIREMENTS:
 1.) MINIMUM LOT WIDTH: 100'
 2.) YARD DEPTHS
 A) FRONT = 20'
 B) REAR = 20'
 C) SIDE = 15'
 3.) MAXIMUM BUILDING HEIGHT = 60'
 MAXIMUM FOOTPRINT FACTOR = N/A
 MAXIMUM IMPERVIOUS SURFACE COVERAGE = 80%
 - WETLANDS WERE LOCATED FROM FLAGS SET BY STEVE WALKER, TOWN OF BRUNSWICK.
 - ELEVATIONS ARE IN FEET AND ARE IN THE SAME DATUM AS PLAN REFERENCE 3 ABOVE.
 - THE EXISTENCE AND LOCATIONS OF UNDERGROUND UTILITIES HAVE NOT BEEN CONFIRMED.
 - MAINE DEP HAS APPROVED THE FOLLOWING IMPERVIOUS AREAS ON LOTS 1, 2, 3 & 4 AS SPECIFIED IN DEPARTMENT ORDER #L-6773-39-L-A:
 LOT 1 - 3.08 ACRES
 LOT 2 - 1.70 ACRES
 LOT 3 - 1.34 ACRES
 LOT 4 - 1.18 ACRES
 DEVELOPMENT WHICH EXCEEDS THE IMPERVIOUS AREA RESERVED FOR EACH LOT REQUIRES ADDITIONAL APPROVAL FROM MAINE DEP. 2,085 S.F. OF WETLAND WILL BE FILLED AS PART OF THE ROAD CONSTRUCTION. WETLANDS ON LOT 2 MAY BE FILLED UP TO 2,215 S.F. WITHOUT A PERMIT. IMPACTS ON LOT 2 GREATER THAN 2,215 S.F. SHALL REQUIRE A PERMIT FROM MAINE DEP. ANY WETLAND IMPACT ON LOT 1 & 3 SHALL REQUIRE A PERMIT FROM MAINE DEP.
 - A 50' UNDISTURBED BUFFER SHALL BE PRESERVED. THE BUFFER SHALL BE ENHANCED WITH ADDITIONAL EVERGREEN TREES TO PROVIDE A SOLID VISUAL BARRIER FROM THE RESIDENCE LOCATED AT TAX MAP 17 LOT 26.
 - EXISTING IMPERVIOUS AREA:
 PAVING & SIDEWALKS 15,417 S.F.
 BUILDING 28,000 S.F.
 TOTAL APPROVED PROJECT 43,417 S.F. 1.00 ACRES (10-11-07)
 TOTAL APPROVED BY DEP 51,400 S.F. 1.18 ACRES
 - THE PURPOSE OF THIS PLAN IS TO DEPICT THE APPROVED AND CONSTRUCTED PROJECT, TO FACILITATE A CHANGE OF USE FROM INDUSTRY, CLASS 2 TO SERVICE BUSINESS, CLASS 2.

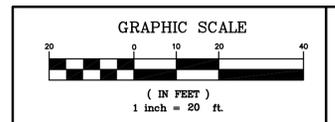
- LEGEND**
- MONUMENT FOUND
 - IRON MARKER FOUND
 - BOUNDARY LINE OF SURVEYED PARCEL
 - - - BOUNDARY LINE OF ABUTTERS (APPROX.)
 - ROAD RIGHT OF WAY LINE (APPROX.)
 - - - EDGE OF TRAVELED WAY
 - UTILITY LINE
 - /CMP 13 UTILITY POLE WITH NUMBER
 - IPF IRON PIPE FOUND
 - IRF IRON ROD FOUND
 - DH DRILL HOLE
 - BK 10674, PG 197 DEED BOOK & PAGE IN COUNTY REGISTRY OF DEEDS
 - PB 195, PG 130 PLAN BOOK & PAGE IN COUNTY REGISTRY OF DEEDS
 - R/W RIGHT OF WAY
 - N/F NOW OR FORMERLY HELD BY
 - AC. ACRES
 - MORE OR LESS
 - SEWER MANHOLE
 - LIGHT POLE
 - CATCH BASIN
 - WATER SHUT OFF
 - HYDRANT
 - SIGN
 - WATER VALVE

APPROVAL
TOWN OF BRUNSWICK PLANNING BOARD

DATE APPROVED: _____

DATE SIGNED: _____

CHAIRMAN: _____



ISSUED FOR:
PERMITTING REVIEW

SITE PLAN

CHANGE OF USE APPLICATION
ALLIED COMPOSITE CENTER, LLC

8 BUSINESS PARKWAY
BRUNSWICK, MAINE

SITESLINES, PA
ENGINEERS • PLANNERS • SURVEYORS
8 CUMBERLAND STREET, BRUNSWICK, ME 04011
207.725.1200 www.siteslinespa.com

FIELD WK: SPIVEY	SCALE: 1"=20'	SHEET:
DRN BY: PRL	JOB #: 1111.01	1 OF 1
CH'D BY: KPC	MAP/PLOT:	
DATE: 10-03-2016	FILE: 1111.01 SITE	

**BRUNSWICK PLANNING BOARD
MEETING MINUTES
JULY 26, 2016**

MEMBERS PRESENT: Chair Charlie Frizzle, Vice Chair Margaret Wilson, Jane Arbuckle, Bill Dana, Jeremy Evans, Sande Updegraph, and Richard Visser

MEMBERS ABSENT: No members were absent at this meeting.

STAFF PRESENT: Anna Breinich, Director of Planning and Development; Jared Woolston, Town Planner

A meeting of the Brunswick Planning Board was held on Tuesday, July 26, 2016, in Town Council Chambers, 85 Union Street. Chair Charlie Frizzle called the meeting to order at 7:00 P.M.

1. Case #16-025 Crystal Springs Farm Community Solar Farm: The Planning Board will hold a Public Hearing then review and take action on a Special Permit application submitted by Revision Energy for a proposed community solar farm which is an omitted use. The proposed use is located in the Coastal Protection 1 (CP1) Zoning District; Natural Resource Protection Zone (NRPZ) (Map 21, Lot 16).

Margaret Wilson recused herself from the meeting and left the room as she is a member of the Board for the Brunswick Topsham Land Trust who is the fee owner for Crystal Springs Farm.

Jared Woolston introduced the application for a solar array in an agricultural portion of Crystal Springs Farm and reviewed the project summary dated July 26, 2016. Jared provided a brief background of the project and said that this project has been in the works for a while. When the application was originally submitted, the neighborhood came out with some concerns at the Staff Review meeting. At that time, the applicant withdrew the application. In discussions with the neighborhood, the applicant decided to move the site to another location and agreed to buffer along the main street.

Applicant representative, Steve Weemes, said that this project is part of a collaborative effort including Crystal Springs Farms, Brunswick Topsham Land Trust and a group of Brunswick families who cannot support solar arrays where they live; this application is also part of Solarize Brunswick. Steve said that this project will support local agriculture by reducing the energy costs for Crystal Springs Farm and provide a model of how a farm can go green. In addition, this project will create electricity security, encourage open space, and demonstrate how individuals can band together to solarize. Steve pointed out that this project is similar to the Freeport solar project along the highway. Steve noted that the environmental impact is near zero and pointed out that there will be no access road, traffic, no impervious surface, employees, odor, loss to farmland, and no light pollution. Steve said that Seth Kroeck will maintain and install the buffer along the Pleasant Hill Road side. This buffer will consist of various evergreens and greenery to give a “natural” look.

MOTION BY JEREMY EVANS THAT THE SPECIAL PERMIT APPLICATION IS DEEMED COMPLETE. MOTION SECONDED BY BILL DANA, APPROVED UNANIMOUSLY.

Jane Arbuckle asked what they plan to use for the buffer. Seth Kroeck replied that the plan is to use Western firs for year round buffering with viburnum and dogwood which will add additional buffering over the summer months. Sande Updegraph asked about how they plan to mow and how often. Seth replied that the panels will be set up in rows so that machinery can go between the arrays and that they plan to mow as needed; this land will still be used as grazing pasture.

Chair Charlie Frizzle opened the meeting to public comment. Hearing none, Charlie closed the public hearing.

Charlie Frizzle stated that he understands why they don't have a fence around the arrays, but asked if the applicant had given any thoughts to vandalism. Seth Kroeck replied that the area is grazing pasture with an electric fence that is charged when the animals are there; there is a physical barrier.

MOTION BY BILL DANA THAT THE SPECIAL PERMIT IS APPROVED WITH THE FOLLOWING CONDITION:

1. That the Board's review and approval does hereby refer to these findings of fact, the plans and materials submitted by the applicant and the written and oral comments of the applicant, his representatives, reviewing officials, and members of the public as reflected in the public record. Any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require a review and approval in accordance with the Brunswick Zoning Ordinance.
2. That prior to issuance of an electrical permit, a planting plan detailing the size and type of plants shall be submitted to the satisfaction of the Director of Planning and Development.

MOTION SECONDED BY SANDE UPDEGRAPH, APPROVED UNANIMOUSLY.

Margaret Wilson returned to the meeting as a voting member.

2. Case #16-026 Tao Yuan Greenhouse and Café: The Planning Board will review and take action on a Major Development Review Amendment application for a new two-story building for greenhouse, café and administrative space. The proposed development is located in the Town Center 1 (TC1) Maine Street Zoning District; and Village Review Zone (Map U13, Lots 52 & 53, and Map U-13, Lot 95).

Jared Woolston introduced the application for a two-story building and greenhouse and reviewed the Site Plan Amendment Memo to the Planning Board dated July 26, 2016. Jared said that the new proposal is to add a retail space / bakery with other minor site changes.

Kate Holcomb presented a PowerPoint presentation regarding the greenhouse / aquaponics and symbiotic work forces as well as the addition of the bakery, layout, lighting, siding and potential parking concerns.

MOTION BY RICHARD VISSER THAT THE AMENDED SITE PLAN APPLICATION IS DEEMED COMPLETE. SECONDED BY SANDE UPDEGRAPH, APPROVED UNANIMOUSLY.

Richard Visser asked if the applicant had given any more thought to snow removal as this was a concern at the last meeting. Kate Holcomb replied that to combat snow building up on the greenhouse, they will have the heat on to melt it. Cecile Stadler added that they have an agreement with Terrace Place to share snow removal costs for the driveway going into the garage and the restaurant. Margaret Wilson asked what times the light will be on in the greenhouse. Kate replied they plan to light similar to summer daylight hours but will not be lighting through the night. Kate added that there is also an automatic blackout curtain. Margaret asked what the life expectancy is for the material the greenhouse is made of. Kate replied that she would have to ask the manufacturer for the specifics, but said that polycarbonate has come quite a way in terms of yellowing and clouding up. Margaret said that she is happy that they have decided to install siding all the way up on the side that faces the townhouses. Anna Breinich replied that the life expectancy concern came up during Village Review a year ago and the polycarbonate was approved. Anna said that she does have the polycarbonate information if Margaret would like to see it. Margaret was comfortable knowing that Village Review did discuss this issue. Margaret asked where the collected water from the greenhouse snow runoff will go. Kate replied that Sitelines did do a stormwater management plan and they have impervious surface. In addition, they expanded the greenhouse to the edge of the building so that they could have a gutter system. Anna Breinich pointed out that one of the changes from the original application is that they removed the deck and changed this to a grated system so that there will be no snow buildup; the snow will just melt down. Anna said that the Town Engineer was at the Staff Review meeting and this was not a concern. Margaret asked where the nearest town catch basin was. Jared Woolston replied that there is one on Pleasant Street, but he does not know if there is one on Abbey Lane. Margaret stated that her concern is that they cannot have water from the greenhouse traveling to Pleasant Street for collection. Jared suggested that they add this as a condition to the Conditions for Approval. Charlie Frizzle said that there is a statement from Sitelines that they have found the existing drains are suitable, but that it does not address the “what if it freezes on the way down”. Kate said that they are willing to look into this further. With respects to the amendment, Charlie noted that the café / bakery will be open in the morning until six in the evening and that the restaurant will be open at five in the evening; there will only be an overlap of an hour and he does not anticipate parking to be a big concern.

Chair Charlie Frizzle opened the meeting to public comment. Hearing none, the public comment period was closed.

MOTION BY BILL DANA THAT ALL APPLICABLE PRIOR CONDITIONS RELATING TO THIS AMENDMENT REMAIN IN EFFECT, IN ADDITION TO ANY NEW CONDITIONS CONTAINED HEREIN. MOTION SECONDED BY SANDE UPDEGRAPH, APPROVED UNANIMOUSLY.

MOTION BY MARGARET WILSON THAT THE AMENDED SITE PLAN APPLICATION IS APPROVED WITH THE FOLLOWING CONDITIONS ADDED TO PRIOR CONDITIONS CURRENTLY IN PLACE:

1. That the Board's review and approval does hereby refer to the plans and materials submitted by the applicant and the written and oral comments of the applicant's representatives, reviewing officials and members of the public as reflected in the public record and that any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require review and approval in accordance with the Brunswick Zoning Ordinance.
2. Prior to issuance of a building permit, a photometric plan for the proposed development shall be submitted for the review and approval of the Directors of Public Works and Planning and Development.
3. Prior to issuance of a building permit, a winter stormwater plan that addresses winter thaw and freeze conditions from the greenhouse roof shall be submitted for the review and approval of the Town Engineer.

MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

3. Case # 16-031 – 35-39 Pleasant Street (St. John’s Church) – The Planning Board will review and take action on a Common Development Plan for the two (2) phased construction of a new 14,685 square foot events center, and a 13,200 square foot two-story school building and various site alterations at 35-39 Pleasant Street (Map U16, Lots 47-48).

Jared Woolston introduced the Common Development Plan for St. John’s All Saints Parish and reviewed the project summary dated July 26, 2016.

The applicant representative, Joe Marden, presented a PowerPoint presentation reviewing the process that they must go through for the Common Development Plan, current zoning, project overview, phased development, proposed lot layout, proposed uses and floor plan for the multi-use events center, and different contextual views of the proposed building and abutting buildings.

MOTION BY BILL DANA THAT THE COMBINED SKETCH/FINAL MAJOR DEVELOPMENT REVIEW COMMON DEVELOPMENT PLAN APPLICATION IS DEEMED COMPLETE. MOTION SECONDED BY JEREMY EVANS, APPROVED UNANIMOUSLY.

Bill Dana pointed out that there is a discrepancy over the lot size and asked what the actual lot size was. Joe Marden replied that the lot is 4.68 acres. Bill asked if the parking lot would accommodate a school bus. Joe replied that the parking lot has to accommodate a fire truck so a school bus should have no issues. Margaret Wilson asked how they plan to heat and cool the events center as she does not see any HVAC equipment. Charlie Wiercinski replied that they do not have an answer to this yet as this is still preliminary. Sande Updegraph asked why the architect said that it would be difficult to move the events center over. Charlie Wiercinski

replied that the issue is that they would like to fence off the construction and still be able to pass between the convent building and the events center. In addition, they would like to build the center along the setback line. Anna Breinich added that the reason why they were asked to move the building back was because of concerns from the neighbors. Anna added that mass and scale are concerns of the Village Review Board. Anna said that the way the two proposed buildings are connected may be an issue with the new ordinance as they move forward and said that right now they do not allow for a connection of a breezeway / covered walkway between two buildings as it is considered a structure and would be part of a single footprint. The center and school would then well exceed what is allowed. Charlie Wiercinski asked if the canopy doesn't touch the building, would it still be considered one structure. Anna replied "no".

Chair Charlie Frizzle opened the meeting to public comment.

Pollyann Melton, resident of 8 Cedar Street, said that she is happy with all the landscaping that this project has proposed, but is saddened that nearly all the historic trees will have to be removed. Pollyann said that Cedar Street enjoys the very leafy environment respective of the old trees and that with nearly all new trees, the environment will feel a little like a suburban mall. Pollyann asked how the revised site plan will affect the operation of the fair; will it be closer to the Cedar Street homes. Charlie Wiercinski replied that they will not be planting all the same types of trees and in terms of the fair, there have been discussions about moving some of the booths that are currently located outside, inside the building or possibly moving the fair to another location such as Brunswick Landing.

Stew Russell, Topsham Physician and father of two children who graduated from St. Johns, said that the current school has been inadequate for some time and the locations, such as the former Knights of Columbus, where events were held, have been sold. Joe said that while he likes the trees, he would choose the education of the young people who would be able to attend the new facility over the trees as the children will be the future of this community.

Pollyann Melton said that they currently have some protection from Pleasant Street by virtue of the school and the convent building and the plan will open the four lots abutting the neighborhood. Pollyann asked that heavy evergreen and vegetation buffer be placed along the edge of the parking lot so as to mitigate any noise and view from Pleasant Street. Charlie Frizzle pointed out that the applicant has stated that they are willing to work with the neighbors on buffers as this application moves forward. Jane Arbuckle asked if there was any way to save the trees. Charlie Weircinski replied that even if they keep the all the trees, they will be impacted to the point where they will eventually pass.

**MOTION BILL DANA THAT THE BOARD APPROVES THE FOLLOWING
WAIVERS AS CONDITIONED:**

1. Sec. 413.3 (D) – showing locations of all signs is waived provided specific sign locations shall be addressed at the time of Final Site Plan review.

2. Sec. 413.3 (E.2 & 4) – showing photometric site plan of lighting treatment is waived provided existing site lighting is maintained until the construction of Phase II is approved and the locations of fixtures shall be addressed during Phase II Site Plan review.

3. Sec. 413.3 (F) – a master landscaping plan with complete plant list and sizes is waived provided the final plan includes plans that acceptably demonstrate the theme of accenting the buildings, screening parking areas, as well as providing shade, color and interest in parking areas through the use of ornamental trees and shrubs.

MOTION SECONDED BY RICHARD VISSER, APPROVED UNANIMOUSLY.

MOTION BY MARGARET WILSON THAT THE MAJOR DEVELOPMENT REVIEW FINAL COMMON DEVELOPMENT PLAN IS APPROVED WITH THE FOLLOWING CONDITIONS:

1. That the Board’s review and approval does hereby refer to these findings of fact, the plans and materials submitted by the applicant and the written and oral comments of the applicant, its representatives, reviewing officials, and members of the public as reflected in the public record. Any changes to the approved plan not called for in these conditions of approval or otherwise approved by the Director of Planning and Development as a minor modification shall require a review and approval in accordance with the Brunswick Zoning Ordinance.
2. That the dimensional and density standards as approved shall be placed on the common development plan.
3. That the size and materials for business identification signs shall be determined at the time of Final Site Plan review.
4. That existing site lighting shall be maintained until the construction of Phase II is approved and the locations of fixtures shall be addressed during Phase II Site Plan review.
5. That a master landscaping plan with complete plant list and sizes is provided that demonstrates the theme of accenting the buildings, screening parking areas, as well as providing shade, color and interest in parking areas through the use of native trees and shrubs at the time of Final Site Plan review.
6. That the proposed development is comprehensively reviewed for traffic and access of vehicles, bicycles, and pedestrians at the time of Final Site Plan review.

MOTION SECONDED BY SANDE UPDEGRAPH, APPROVED UNANIMOUSLY.

4. 946 Mere Point Road: At the request of Town Council, the Planning Board will offer input and recommendation regarding possible public use of the property.

Charlie Frizzle pointed out that the Planning Board has been copied on all correspondence regarding this parcel and also have the comments made by the Recreation Commission at their last meeting and are aware of the former owner's request that they be allowed to somehow restore their ownership of the property. Charlie said that for discussion tonight, he would like to avoid discussing re-ownership as this is outside the PB purview. Charlie said that the interest of the Board are pretty narrow as they pertain to planning and they should look at what the Comprehensive Plan offers for guidance.

Per Jane Arbuckle's request, Charlie Frizzle provided a summary of what the Recreation Commission recommended for this property. Jane pointed out the key actions of the Comprehensive Plan. Margaret Wilson said that although this is a beautiful site for a small pocket park, it would be a very passive use. Sande Updegraph agreed that this site is not really suitable and would require a major investment to make it suitable. Richard Visser asked about the gravesite. Anna Breinich said that there are gravesites throughout the Town that are mapped and are for the most part protected by easements.

Chair Charlie Frizzle opened the meeting to public comment.

Steve Walker, District 2 Town Councilor and resident of 14 Bowdoin Street, said that the Board is spot on in terms of review. Steve said that if the Town did choose to retain this parcel for public use, any design for use would certainly be seen by the Planning Board for their input. In terms of how to approach this, Steve said that the Comprehensive Plan is a key document as is the Brunswick Parks and Recreation Open Space Plan which the PB assisted in developing. Steve said that in terms of water access, water access shouldn't be limited to being able to put a boat in the water. Steve said that there are rare species and significant wildlife present on this site as well as the historic gravesite and encouraged the Board to think through all the potential uses and Comprehensive Plan goals before making a recommendation.

Heather Osterfeld, resident of Oster Lane, presented a petition for the six abutting neighbors of this property and reviewed her letter to the Board that was handed to Anna Breinich for the records and distribution.

Mr Grey, resident of 19 Eider Cove Road and abutter, pointed out that the view shed is limited to the cove. Mr. Grey said that there are an Eider duck community, Eagles and Blue Herron that can be found in this area. Mr. Grey said that anyone owning waterfront property like the quiet and any density of recreational activity in the area would drive up sound in this area. Mr. Grey stated that water use is ambiguous and limited to the tide, mud flats and the very steep slope. In terms of a view shed, any activity would be more than a single family residence would generate.

Richard Knox, resident of Simpsons Point Road, urged the Board to consider the facts. Richard agrees with a number of the points that Steve Walker made and said that there are very few access points in Brunswick with access for swimmers. Rich said that this property is unique and from a quiet passive recreation point of view, he does not think that you would see the neighbors

or that they would see you. Rich thinks that there is a real opportunity to create public access and public viewing without any negativity to the neighbors.

Mr. Osterfeld pointed out that 946 Mere Point is 100 feet from his residence and is very easily viewable. Mr. Osterfeld reiterated that swimming is ambiguous and that you could only swim at this location two to three hours per day two months out of the year.

Charlie Frizzle provided examples of what the Board would review if the Town decided to retain this property such as habitat, entrance to the parcel and etc. Anna Breinich to draft a letter for consideration for the Board to review / edit at the next meeting.

Decision among Board members to table discussion to the next meeting.

5. Other Business: No other business.

6. Approval of Minutes

No minutes were reviewed at this meeting.

Adjourn

This meeting was adjourned at 9:18 P.M.

Respectfully Submitted,

Tonya Jenusaitis

Recording Secretary

Revised 7/15/16 to add Item #3